



**APPENDIX A**

**Raw Traffic Data  
Existing Traffic Signal Timing Data  
Peak Hour Traffic Volume and Crash Location Diagram**

# Peggy Malone & Associates, Inc.

## (888) 247-8602

File Name : TM 1-Pepper's Ferry and US 11 AM  
 Site Code :  
 Start Date : 2/4/2015  
 Page No : 1

Groups Printed- Car

| Start Time         | Pepper's Ferry Blvd.<br>Southbound |            |          |             | US 11<br>Westbound |            |          |          |            | US 11<br>Eastbound |            |          |             | Int. Total  |
|--------------------|------------------------------------|------------|----------|-------------|--------------------|------------|----------|----------|------------|--------------------|------------|----------|-------------|-------------|
|                    | Right                              | Left       | Peds     | App. Total  | Right              | Thru       | Left     | Peds     | App. Total | Thru               | Left       | Peds     | App. Total  |             |
| 07:00 AM           | 33                                 | 56         | 0        | 89          | 47                 | 37         | 1        | 0        | 85         | 67                 | 68         | 0        | 135         | 309         |
| 07:15 AM           | 43                                 | 104        | 0        | 147         | 70                 | 39         | 0        | 0        | 109        | 88                 | 92         | 0        | 180         | 436         |
| 07:30 AM           | 73                                 | 110        | 0        | 183         | 72                 | 54         | 0        | 0        | 126        | 113                | 104        | 0        | 217         | 526         |
| 07:45 AM           | 56                                 | 154        | 0        | 210         | 64                 | 54         | 0        | 0        | 118        | 109                | 86         | 0        | 195         | 523         |
| <b>Total</b>       | <b>205</b>                         | <b>424</b> | <b>0</b> | <b>629</b>  | <b>253</b>         | <b>184</b> | <b>1</b> | <b>0</b> | <b>438</b> | <b>377</b>         | <b>350</b> | <b>0</b> | <b>727</b>  | <b>1794</b> |
| 08:00 AM           | 69                                 | 101        | 0        | 170         | 64                 | 39         | 0        | 0        | 103        | 95                 | 87         | 0        | 182         | 455         |
| 08:15 AM           | 65                                 | 116        | 0        | 181         | 47                 | 57         | 0        | 0        | 104        | 86                 | 70         | 0        | 156         | 441         |
| 08:30 AM           | 52                                 | 106        | 0        | 158         | 47                 | 56         | 0        | 0        | 103        | 63                 | 64         | 0        | 127         | 388         |
| 08:45 AM           | 33                                 | 113        | 0        | 146         | 51                 | 56         | 0        | 0        | 107        | 73                 | 42         | 0        | 115         | 368         |
| <b>Total</b>       | <b>219</b>                         | <b>436</b> | <b>0</b> | <b>655</b>  | <b>209</b>         | <b>208</b> | <b>0</b> | <b>0</b> | <b>417</b> | <b>317</b>         | <b>263</b> | <b>0</b> | <b>580</b>  | <b>1652</b> |
| <b>Grand Total</b> | <b>424</b>                         | <b>860</b> | <b>0</b> | <b>1284</b> | <b>462</b>         | <b>392</b> | <b>1</b> | <b>0</b> | <b>855</b> | <b>694</b>         | <b>613</b> | <b>0</b> | <b>1307</b> | <b>3446</b> |
| Apprch %           | 33                                 | 67         | 0        |             | 54                 | 45.8       | 0.1      | 0        |            | 53.1               | 46.9       | 0        |             |             |
| Total %            | 12.3                               | 25         | 0        | 37.3        | 13.4               | 11.4       | 0        | 0        | 24.8       | 20.1               | 17.8       | 0        | 37.9        |             |

| Start Time   | Pepper's Ferry Blvd.<br>Southbound |            |            | US 11<br>Westbound |            |          |            | US 11<br>Eastbound |            |            | Int. Total  |
|--|------------------------------------|------------|------------|--------------------|------------|----------|------------|--------------------|------------|------------|-------------|
|  | Right                              | Left       | App. Total | Right              | Thru       | Left     | App. Total | Thru               | Left       | App. Total |             |
| Peak Hour Analysis From 07:00 AM to 08:45 AM - Peak 1 of 1 |                                    |            |            |                    |            |          |            |                    |            |            |             |
| Peak Hour for Entire Intersection Begins at 07:30 AM       |                                    |            |            |                    |            |          |            |                    |            |            |             |
| 07:30 AM   | 73                                 | 110        | 183        | 72                 | 54         | 0        | 126        | 113                | 104        | 217        | 526         |
| 07:45 AM   | 56                                 | 154        | 210        | 64                 | 54         | 0        | 118        | 109                | 86         | 195        | 523         |
| 08:00 AM   | 69                                 | 101        | 170        | 64                 | 39         | 0        | 103        | 95                 | 87         | 182        | 455         |
| 08:15 AM   | 65                                 | 116        | 181        | 47                 | 57         | 0        | 104        | 86                 | 70         | 156        | 441         |
| <b>Total Volume</b>  | <b>263</b>                         | <b>481</b> | <b>744</b> | <b>247</b>         | <b>204</b> | <b>0</b> | <b>451</b> | <b>403</b>         | <b>347</b> | <b>750</b> | <b>1945</b> |
| % App. Total   | 35.3                               | 64.7       |            | 54.8               | 45.2       | 0        |            | 53.7               | 46.3       |            |             |
| PHF  | .901                               | .781       | .886       | .858               | .895       | .000     | .895       | .892               | .834       | .864       | .924        |

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File Name : TM 1-Pepper's Ferry and US 11 AM  
 Site Code :  
 Start Date : 2/4/2015  
 Page No : 1

### Groups Printed- Truck

| Start Time         | Pepper's Ferry Blvd.<br>Southbound |           |          |            | US 11<br>Westbound |          |          |          |            | US 11<br>Eastbound |           |          |            | Int. Total |
|--------------------|------------------------------------|-----------|----------|------------|--------------------|----------|----------|----------|------------|--------------------|-----------|----------|------------|------------|
|                    | Right                              | Left      | Peds     | App. Total | Right              | Thru     | Left     | Peds     | App. Total | Thru               | Left      | Peds     | App. Total |            |
| 07:00 AM           | 1                                  | 2         | 0        | 3          | 0                  | 0        | 0        | 0        | 0          | 0                  | 3         | 0        | 3          | 6          |
| 07:15 AM           | 0                                  | 1         | 0        | 1          | 2                  | 0        | 0        | 0        | 2          | 2                  | 3         | 0        | 5          | 8          |
| 07:30 AM           | 6                                  | 2         | 0        | 8          | 0                  | 1        | 0        | 0        | 1          | 0                  | 8         | 0        | 8          | 17         |
| 07:45 AM           | 6                                  | 3         | 0        | 9          | 3                  | 2        | 0        | 0        | 5          | 2                  | 5         | 0        | 7          | 21         |
| <b>Total</b>       | <b>13</b>                          | <b>8</b>  | <b>0</b> | <b>21</b>  | <b>5</b>           | <b>3</b> | <b>0</b> | <b>0</b> | <b>8</b>   | <b>4</b>           | <b>19</b> | <b>0</b> | <b>23</b>  | <b>52</b>  |
| 08:00 AM           | 2                                  | 2         | 0        | 4          | 3                  | 1        | 0        | 0        | 4          | 0                  | 2         | 0        | 2          | 10         |
| 08:15 AM           | 4                                  | 0         | 0        | 4          | 3                  | 1        | 0        | 0        | 4          | 2                  | 2         | 0        | 4          | 12         |
| 08:30 AM           | 1                                  | 0         | 0        | 1          | 2                  | 1        | 0        | 0        | 3          | 1                  | 1         | 0        | 2          | 6          |
| 08:45 AM           | 5                                  | 4         | 0        | 9          | 4                  | 2        | 0        | 0        | 6          | 2                  | 3         | 0        | 5          | 20         |
| <b>Total</b>       | <b>12</b>                          | <b>6</b>  | <b>0</b> | <b>18</b>  | <b>12</b>          | <b>5</b> | <b>0</b> | <b>0</b> | <b>17</b>  | <b>5</b>           | <b>8</b>  | <b>0</b> | <b>13</b>  | <b>48</b>  |
| <b>Grand Total</b> | <b>25</b>                          | <b>14</b> | <b>0</b> | <b>39</b>  | <b>17</b>          | <b>8</b> | <b>0</b> | <b>0</b> | <b>25</b>  | <b>9</b>           | <b>27</b> | <b>0</b> | <b>36</b>  | <b>100</b> |
| Apprch %           | 64.1                               | 35.9      | 0        |            | 68                 | 32       | 0        | 0        |            | 25                 | 75        | 0        |            |            |
| Total %            | 25                                 | 14        | 0        | 39         | 17                 | 8        | 0        | 0        | 25         | 9                  | 27        | 0        | 36         |            |

| Start Time   | Pepper's Ferry Blvd.<br>Southbound |           |            | US 11<br>Westbound |             |          |            | US 11<br>Eastbound |           |            | Int. Total |
|--|------------------------------------|-----------|------------|--------------------|-------------|----------|------------|--------------------|-----------|------------|------------|
|  | Right                              | Left      | App. Total | Right              | Thru        | Left     | App. Total | Thru               | Left      | App. Total |            |
| Peak Hour Analysis From 07:00 AM to 08:45 AM - Peak 1 of 1 |                                    |           |            |                    |             |          |            |                    |           |            |            |
| Peak Hour for Entire Intersection Begins at 07:30 AM       |                                    |           |            |                    |             |          |            |                    |           |            |            |
| 07:30 AM   | 6                                  | 2         | 8          | 0                  | 1           | 0        | 1          | 0                  | 8         | 8          | 17         |
| 07:45 AM   | 6                                  | 3         | 9          | 3                  | 2           | 0        | 5          | 2                  | 5         | 7          | 21         |
| 08:00 AM   | 2                                  | 2         | 4          | 3                  | 1           | 0        | 4          | 0                  | 2         | 2          | 10         |
| 08:15 AM   | 4                                  | 0         | 4          | 3                  | 1           | 0        | 4          | 2                  | 2         | 4          | 12         |
| <b>Total Volume</b>  | <b>18</b>                          | <b>7</b>  | <b>25</b>  | <b>9</b>           | <b>5</b>    | <b>0</b> | <b>14</b>  | <b>4</b>           | <b>17</b> | <b>21</b>  | <b>60</b>  |
| <b>% App. Total</b>  | <b>72</b>                          | <b>28</b> |            | <b>64.3</b>        | <b>35.7</b> | <b>0</b> |            | <b>19</b>          | <b>81</b> |            |            |
| PHF  | .750                               | .583      | .694       | .750               | .625        | .000     | .700       | .500               | .531      | .656       | .714       |

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File Name : TM 1-Pepper's Ferry and US 11 AM  
 Site Code :  
 Start Date : 2/4/2015  
 Page No : 1

Groups Printed- Combined

| Start Time         | Pepper's Ferry Blvd.<br>Southbound |            |          |             | US 11<br>Westbound |            |          |          |            | US 11<br>Eastbound |            |          |             | Int. Total  |
|--------------------|------------------------------------|------------|----------|-------------|--------------------|------------|----------|----------|------------|--------------------|------------|----------|-------------|-------------|
|                    | Right                              | Left       | Peds     | App. Total  | Right              | Thru       | Left     | Peds     | App. Total | Thru               | Left       | Peds     | App. Total  |             |
| 07:00 AM           | 34                                 | 58         | 0        | 92          | 47                 | 37         | 1        | 0        | 85         | 67                 | 71         | 0        | 138         | 315         |
| 07:15 AM           | 43                                 | 105        | 0        | 148         | 72                 | 39         | 0        | 0        | 111        | 90                 | 95         | 0        | 185         | 444         |
| 07:30 AM           | 79                                 | 112        | 0        | 191         | 72                 | 55         | 0        | 0        | 127        | 113                | 112        | 0        | 225         | 543         |
| 07:45 AM           | 62                                 | 157        | 0        | 219         | 67                 | 56         | 0        | 0        | 123        | 111                | 91         | 0        | 202         | 544         |
| <b>Total</b>       | <b>218</b>                         | <b>432</b> | <b>0</b> | <b>650</b>  | <b>258</b>         | <b>187</b> | <b>1</b> | <b>0</b> | <b>446</b> | <b>381</b>         | <b>369</b> | <b>0</b> | <b>750</b>  | <b>1846</b> |
| 08:00 AM           | 71                                 | 103        | 0        | 174         | 67                 | 40         | 0        | 0        | 107        | 95                 | 89         | 0        | 184         | 465         |
| 08:15 AM           | 69                                 | 116        | 0        | 185         | 50                 | 58         | 0        | 0        | 108        | 88                 | 72         | 0        | 160         | 453         |
| 08:30 AM           | 53                                 | 106        | 0        | 159         | 49                 | 57         | 0        | 0        | 106        | 64                 | 65         | 0        | 129         | 394         |
| 08:45 AM           | 38                                 | 117        | 0        | 155         | 55                 | 58         | 0        | 0        | 113        | 75                 | 45         | 0        | 120         | 388         |
| <b>Total</b>       | <b>231</b>                         | <b>442</b> | <b>0</b> | <b>673</b>  | <b>221</b>         | <b>213</b> | <b>0</b> | <b>0</b> | <b>434</b> | <b>322</b>         | <b>271</b> | <b>0</b> | <b>593</b>  | <b>1700</b> |
| <b>Grand Total</b> | <b>449</b>                         | <b>874</b> | <b>0</b> | <b>1323</b> | <b>479</b>         | <b>400</b> | <b>1</b> | <b>0</b> | <b>880</b> | <b>703</b>         | <b>640</b> | <b>0</b> | <b>1343</b> | <b>3546</b> |
| Apprch %           | 33.9                               | 66.1       | 0        |             | 54.4               | 45.5       | 0.1      | 0        |            | 52.3               | 47.7       | 0        |             |             |
| Total %            | 12.7                               | 24.6       | 0        | 37.3        | 13.5               | 11.3       | 0        | 0        | 24.8       | 19.8               | 18         | 0        | 37.9        |             |

| Start Time   | Pepper's Ferry Blvd.<br>Southbound |            |            | US 11<br>Westbound |            |          |            | US 11<br>Eastbound |            |            | Int. Total  |
|--|------------------------------------|------------|------------|--------------------|------------|----------|------------|--------------------|------------|------------|-------------|
|  | Right                              | Left       | App. Total | Right              | Thru       | Left     | App. Total | Thru               | Left       | App. Total |             |
| Peak Hour Analysis From 07:00 AM to 08:45 AM - Peak 1 of 1 |                                    |            |            |                    |            |          |            |                    |            |            |             |
| Peak Hour for Entire Intersection Begins at 07:30 AM       |                                    |            |            |                    |            |          |            |                    |            |            |             |
| 07:30 AM   | 79                                 | 112        | 191        | 72                 | 55         | 0        | 127        | 113                | 112        | 225        | 543         |
| 07:45 AM   | 62                                 | 157        | 219        | 67                 | 56         | 0        | 123        | 111                | 91         | 202        | 544         |
| 08:00 AM   | 71                                 | 103        | 174        | 67                 | 40         | 0        | 107        | 95                 | 89         | 184        | 465         |
| 08:15 AM   | 69                                 | 116        | 185        | 50                 | 58         | 0        | 108        | 88                 | 72         | 160        | 453         |
| <b>Total Volume</b>  | <b>281</b>                         | <b>488</b> | <b>769</b> | <b>256</b>         | <b>209</b> | <b>0</b> | <b>465</b> | <b>407</b>         | <b>364</b> | <b>771</b> | <b>2005</b> |
| % App. Total   | 36.5                               | 63.5       |            | 55.1               | 44.9       | 0        |            | 52.8               | 47.2       |            |             |
| PHF  | .889                               | .777       | .878       | .889               | .901       | .000     | .915       | .900               | .813       | .857       | .921        |

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### Groups Printed- Car

| Start Time         | Pepper's Ferry Blvd.<br>Southbound |             |          |             | US 11<br>Westbound |            |          |          |             | US 11<br>Eastbound |            |          |            | Int. Total  |
|--------------------|------------------------------------|-------------|----------|-------------|--------------------|------------|----------|----------|-------------|--------------------|------------|----------|------------|-------------|
|                    | Right                              | Left        | Peds     | App. Total  | Right              | Thru       | Left     | Peds     | App. Total  | Thru               | Left       | Peds     | App. Total |             |
| 11:00 AM           | 45                                 | 120         | 0        | 165         | 93                 | 50         | 0        | 0        | 143         | 52                 | 49         | 1        | 102        | 410         |
| 11:15 AM           | 38                                 | 111         | 0        | 149         | 98                 | 64         | 0        | 0        | 162         | 72                 | 57         | 0        | 129        | 440         |
| 11:30 AM           | 48                                 | 140         | 0        | 188         | 100                | 57         | 0        | 0        | 157         | 65                 | 48         | 0        | 113        | 458         |
| 11:45 AM           | 47                                 | 121         | 0        | 168         | 105                | 75         | 0        | 0        | 180         | 55                 | 43         | 0        | 98         | 446         |
| <b>Total</b>       | <b>178</b>                         | <b>492</b>  | <b>0</b> | <b>670</b>  | <b>396</b>         | <b>246</b> | <b>0</b> | <b>0</b> | <b>642</b>  | <b>244</b>         | <b>197</b> | <b>1</b> | <b>442</b> | <b>1754</b> |
| 12:00 PM           | 35                                 | 155         | 0        | 190         | 117                | 69         | 1        | 0        | 187         | 67                 | 43         | 0        | 110        | 487         |
| 12:15 PM           | 52                                 | 149         | 0        | 201         | 129                | 65         | 0        | 0        | 194         | 79                 | 72         | 0        | 151        | 546         |
| 12:30 PM           | 48                                 | 187         | 0        | 235         | 106                | 55         | 0        | 0        | 161         | 64                 | 46         | 0        | 110        | 506         |
| 12:45 PM           | 31                                 | 157         | 0        | 188         | 127                | 60         | 0        | 0        | 187         | 73                 | 43         | 0        | 116        | 491         |
| <b>Total</b>       | <b>166</b>                         | <b>648</b>  | <b>0</b> | <b>814</b>  | <b>479</b>         | <b>249</b> | <b>1</b> | <b>0</b> | <b>729</b>  | <b>283</b>         | <b>204</b> | <b>0</b> | <b>487</b> | <b>2030</b> |
| <b>Grand Total</b> | <b>344</b>                         | <b>1140</b> | <b>0</b> | <b>1484</b> | <b>875</b>         | <b>495</b> | <b>1</b> | <b>0</b> | <b>1371</b> | <b>527</b>         | <b>401</b> | <b>1</b> | <b>929</b> | <b>3784</b> |
| Apprch %           | 23.2                               | 76.8        | 0        |             | 63.8               | 36.1       | 0.1      | 0        |             | 56.7               | 43.2       | 0.1      |            |             |
| Total %            | 9.1                                | 30.1        | 0        | 39.2        | 23.1               | 13.1       | 0        | 0        | 36.2        | 13.9               | 10.6       | 0        | 24.6       |             |

| Start Time   | Pepper's Ferry Blvd.<br>Southbound |            |            | US 11<br>Westbound |           |          |            | US 11<br>Eastbound |           |            | Int. Total |
|--|------------------------------------|------------|------------|--------------------|-----------|----------|------------|--------------------|-----------|------------|------------|
|  | Right                              | Left       | App. Total | Right              | Thru      | Left     | App. Total | Thru               | Left      | App. Total |            |
| Peak Hour Analysis From 11:00 AM to 12:45 PM - Peak 1 of 1 |                                    |            |            |                    |           |          |            |                    |           |            |            |
| Peak Hour for Entire Intersection Begins at 12:00 PM       |                                    |            |            |                    |           |          |            |                    |           |            |            |
| 12:00 PM   | 35                                 | 155        | 190        | 117                | <b>69</b> | <b>1</b> | 187        | 67                 | 43        | 110        | 487        |
| 12:15 PM   | <b>52</b>                          | 149        | 201        | <b>129</b>         | 65        | 0        | <b>194</b> | <b>79</b>          | <b>72</b> | <b>151</b> | <b>546</b> |
| 12:30 PM   | 48                                 | <b>187</b> | <b>235</b> | 106                | 55        | 0        | 161        | 64                 | 46        | 110        | 506        |
| 12:45 PM   | 31                                 | 157        | 188        | 127                | 60        | 0        | 187        | 73                 | 43        | 116        | 491        |
| Total Volume   | 166                                | 648        | 814        | 479                | 249       | 1        | 729        | 283                | 204       | 487        | 2030       |
| % App. Total   | 20.4                               | 79.6       |            | 65.7               | 34.2      | 0.1      |            | 58.1               | 41.9      |            |            |
| PHF  | .798                               | .866       | .866       | .928               | .902      | .250     | .939       | .896               | .708      | .806       | .929       |

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Groups Printed- Truck

| Start Time         | Pepper's Ferry Blvd.<br>Southbound |      |      |            | US 11<br>Westbound |      |      |      |            | US 11<br>Eastbound |      |      |            | Int. Total |
|--------------------|------------------------------------|------|------|------------|--------------------|------|------|------|------------|--------------------|------|------|------------|------------|
|                    | Right                              | Left | Peds | App. Total | Right              | Thru | Left | Peds | App. Total | Thru               | Left | Peds | App. Total |            |
| 11:00 AM           | 4                                  | 4    | 0    | 8          | 2                  | 5    | 0    | 0    | 7          | 3                  | 3    | 0    | 6          | 21         |
| 11:15 AM           | 2                                  | 2    | 0    | 4          | 2                  | 0    | 0    | 0    | 2          | 1                  | 3    | 0    | 4          | 10         |
| 11:30 AM           | 8                                  | 2    | 0    | 10         | 3                  | 5    | 0    | 0    | 8          | 3                  | 1    | 0    | 4          | 22         |
| 11:45 AM           | 3                                  | 6    | 0    | 9          | 2                  | 1    | 0    | 0    | 3          | 2                  | 5    | 0    | 7          | 19         |
| <b>Total</b>       | 17                                 | 14   | 0    | 31         | 9                  | 11   | 0    | 0    | 20         | 9                  | 12   | 0    | 21         | 72         |
| 12:00 PM           | 4                                  | 3    | 0    | 7          | 2                  | 2    | 0    | 0    | 4          | 1                  | 2    | 0    | 3          | 14         |
| 12:15 PM           | 3                                  | 4    | 0    | 7          | 1                  | 1    | 0    | 0    | 2          | 2                  | 3    | 0    | 5          | 14         |
| 12:30 PM           | 6                                  | 2    | 0    | 8          | 3                  | 1    | 0    | 0    | 4          | 2                  | 3    | 0    | 5          | 17         |
| 12:45 PM           | 0                                  | 3    | 0    | 3          | 3                  | 3    | 0    | 0    | 6          | 3                  | 2    | 0    | 5          | 14         |
| <b>Total</b>       | 13                                 | 12   | 0    | 25         | 9                  | 7    | 0    | 0    | 16         | 8                  | 10   | 0    | 18         | 59         |
| <b>Grand Total</b> | 30                                 | 26   | 0    | 56         | 18                 | 18   | 0    | 0    | 36         | 17                 | 22   | 0    | 39         | 131        |
| <b>Apprch %</b>    | 53.6                               | 46.4 | 0    |            | 50                 | 50   | 0    | 0    |            | 43.6               | 56.4 | 0    |            |            |
| <b>Total %</b>     | 22.9                               | 19.8 | 0    | 42.7       | 13.7               | 13.7 | 0    | 0    | 27.5       | 13                 | 16.8 | 0    | 29.8       |            |

| Start Time   | Pepper's Ferry Blvd.<br>Southbound |      |            | US 11<br>Westbound |      |      |            | US 11<br>Eastbound |      |            | Int. Total |
|--|------------------------------------|------|------------|--------------------|------|------|------------|--------------------|------|------------|------------|
|  | Right                              | Left | App. Total | Right              | Thru | Left | App. Total | Thru               | Left | App. Total |            |
| Peak Hour Analysis From 11:00 AM to 12:45 PM - Peak 1 of 1 |                                    |      |            |                    |      |      |            |                    |      |            |            |
| Peak Hour for Entire Intersection Begins at 11:00 AM       |                                    |      |            |                    |      |      |            |                    |      |            |            |
| 11:00 AM   | 4                                  | 4    | 8          | 2                  | 5    | 0    | 7          | 3                  | 3    | 6          | 21         |
| 11:15 AM   | 2                                  | 2    | 4          | 2                  | 0    | 0    | 2          | 1                  | 3    | 4          | 10         |
| 11:30 AM   | 8                                  | 2    | 10         | 3                  | 5    | 0    | 8          | 3                  | 1    | 4          | 22         |
| 11:45 AM   | 3                                  | 6    | 9          | 2                  | 1    | 0    | 3          | 2                  | 5    | 7          | 19         |
| <b>Total Volume</b>  | 17                                 | 14   | 31         | 9                  | 11   | 0    | 20         | 9                  | 12   | 21         | 72         |
| <b>% App. Total</b>  | 54.8                               | 45.2 |            | 45                 | 55   | 0    |            | 42.9               | 57.1 |            |            |
| <b>PHF</b>   | .531                               | .583 | .775       | .750               | .550 | .000 | .625       | .750               | .600 | .750       | .818       |

# Peggy Malone & Associates, Inc.

## (888) 247-8602

File Name : TM 1-Pepper's Ferry and US 11 MID  
 Site Code :  
 Start Date : 2/4/2015  
 Page No : 1

Groups Printed- Combined

| Start Time         | Pepper's Ferry Blvd.<br>Southbound |      |      |            | US 11<br>Westbound |      |      |      |            | US 11<br>Eastbound |      |      |            | Int. Total |
|--------------------|------------------------------------|------|------|------------|--------------------|------|------|------|------------|--------------------|------|------|------------|------------|
|                    | Right                              | Left | Peds | App. Total | Right              | Thru | Left | Peds | App. Total | Thru               | Left | Peds | App. Total |            |
| 11:00 AM           | 49                                 | 124  | 0    | 173        | 95                 | 55   | 0    | 0    | 150        | 55                 | 52   | 1    | 108        | 431        |
| 11:15 AM           | 40                                 | 113  | 0    | 153        | 100                | 64   | 0    | 0    | 164        | 73                 | 60   | 0    | 133        | 450        |
| 11:30 AM           | 56                                 | 142  | 0    | 198        | 103                | 62   | 0    | 0    | 165        | 68                 | 49   | 0    | 117        | 480        |
| 11:45 AM           | 50                                 | 127  | 0    | 177        | 107                | 76   | 0    | 0    | 183        | 57                 | 48   | 0    | 105        | 465        |
| <b>Total</b>       | 195                                | 506  | 0    | 701        | 405                | 257  | 0    | 0    | 662        | 253                | 209  | 1    | 463        | 1826       |
| 12:00 PM           | 39                                 | 158  | 0    | 197        | 119                | 71   | 1    | 0    | 191        | 68                 | 45   | 0    | 113        | 501        |
| 12:15 PM           | 55                                 | 153  | 0    | 208        | 130                | 66   | 0    | 0    | 196        | 81                 | 75   | 0    | 156        | 560        |
| 12:30 PM           | 54                                 | 189  | 0    | 243        | 109                | 56   | 0    | 0    | 165        | 66                 | 49   | 0    | 115        | 523        |
| 12:45 PM           | 31                                 | 160  | 0    | 191        | 130                | 63   | 0    | 0    | 193        | 76                 | 45   | 0    | 121        | 505        |
| <b>Total</b>       | 179                                | 660  | 0    | 839        | 488                | 256  | 1    | 0    | 745        | 291                | 214  | 0    | 505        | 2089       |
| <b>Grand Total</b> | 374                                | 1166 | 0    | 1540       | 893                | 513  | 1    | 0    | 1407       | 544                | 423  | 1    | 968        | 3915       |
| Apprch %           | 24.3                               | 75.7 | 0    |            | 63.5               | 36.5 | 0.1  | 0    |            | 56.2               | 43.7 | 0.1  |            |            |
| Total %            | 9.6                                | 29.8 | 0    | 39.3       | 22.8               | 13.1 | 0    | 0    | 35.9       | 13.9               | 10.8 | 0    | 24.7       |            |

| Start Time   | Pepper's Ferry Blvd.<br>Southbound |            |            | US 11<br>Westbound |           |          |            | US 11<br>Eastbound |           |            | Int. Total |
|--|------------------------------------|------------|------------|--------------------|-----------|----------|------------|--------------------|-----------|------------|------------|
|  | Right                              | Left       | App. Total | Right              | Thru      | Left     | App. Total | Thru               | Left      | App. Total |            |
| Peak Hour Analysis From 11:00 AM to 12:45 PM - Peak 1 of 1 |                                    |            |            |                    |           |          |            |                    |           |            |            |
| Peak Hour for Entire Intersection Begins at 12:00 PM       |                                    |            |            |                    |           |          |            |                    |           |            |            |
| 12:00 PM   | 39                                 | 158        | 197        | 119                | <b>71</b> | <b>1</b> | 191        | 68                 | 45        | 113        | 501        |
| 12:15 PM   | <b>55</b>                          | 153        | 208        | <b>130</b>         | 66        | 0        | <b>196</b> | <b>81</b>          | <b>75</b> | <b>156</b> | <b>560</b> |
| 12:30 PM   | 54                                 | <b>189</b> | <b>243</b> | 109                | 56        | 0        | 165        | 66                 | 49        | 115        | 523        |
| 12:45 PM   | 31                                 | 160        | 191        | 130                | 63        | 0        | 193        | 76                 | 45        | 121        | 505        |
| <b>Total Volume</b>  | 179                                | 660        | 839        | 488                | 256       | 1        | 745        | 291                | 214       | 505        | 2089       |
| % App. Total   | 21.3                               | 78.7       |            | 65.5               | 34.4      | 0.1      |            | 57.6               | 42.4      |            |            |
| PHF  | .814                               | .873       | .863       | .938               | .901      | .250     | .950       | .898               | .713      | .809       | .933       |

# Peggy Malone & Associates, Inc.

## (888) 247-8602

File Name : TM 1-Pepper's Ferry and US 11 PM  
 Site Code :  
 Start Date : 2/4/2015  
 Page No : 1

### Groups Printed- Car

| Start Time         | Pepper's Ferry Blvd.<br>Southbound |             |          |             | US 11<br>Westbound |             |          |          |             | US 11<br>Eastbound |            |          |             | Int. Total  |
|--------------------|------------------------------------|-------------|----------|-------------|--------------------|-------------|----------|----------|-------------|--------------------|------------|----------|-------------|-------------|
|                    | Right                              | Left        | Peds     | App. Total  | Right              | Thru        | Left     | Peds     | App. Total  | Thru               | Left       | Peds     | App. Total  |             |
| 03:00 PM           | 81                                 | 151         | 0        | 232         | 113                | 72          | 0        | 0        | 185         | 56                 | 74         | 0        | 130         | 547         |
| 03:15 PM           | 99                                 | 187         | 1        | 287         | 111                | 83          | 1        | 0        | 195         | 78                 | 90         | 0        | 168         | 650         |
| 03:30 PM           | 95                                 | 171         | 0        | 266         | 134                | 121         | 0        | 0        | 255         | 82                 | 75         | 0        | 157         | 678         |
| 03:45 PM           | 93                                 | 168         | 0        | 261         | 120                | 105         | 0        | 0        | 225         | 96                 | 81         | 0        | 177         | 663         |
| <b>Total</b>       | <b>368</b>                         | <b>677</b>  | <b>1</b> | <b>1046</b> | <b>478</b>         | <b>381</b>  | <b>1</b> | <b>0</b> | <b>860</b>  | <b>312</b>         | <b>320</b> | <b>0</b> | <b>632</b>  | <b>2538</b> |
| 04:00 PM           | 104                                | 196         | 0        | 300         | 123                | 119         | 0        | 0        | 242         | 65                 | 69         | 0        | 134         | 676         |
| 04:15 PM           | 120                                | 163         | 0        | 283         | 121                | 76          | 0        | 0        | 197         | 94                 | 75         | 0        | 169         | 649         |
| 04:30 PM           | 87                                 | 212         | 0        | 299         | 143                | 108         | 0        | 0        | 251         | 76                 | 58         | 0        | 134         | 684         |
| 04:45 PM           | 91                                 | 213         | 0        | 304         | 140                | 101         | 0        | 0        | 241         | 74                 | 79         | 0        | 153         | 698         |
| <b>Total</b>       | <b>402</b>                         | <b>784</b>  | <b>0</b> | <b>1186</b> | <b>527</b>         | <b>404</b>  | <b>0</b> | <b>0</b> | <b>931</b>  | <b>309</b>         | <b>281</b> | <b>0</b> | <b>590</b>  | <b>2707</b> |
| 05:00 PM           | 89                                 | 187         | 0        | 276         | 145                | 134         | 0        | 0        | 279         | 81                 | 74         | 0        | 155         | 710         |
| 05:15 PM           | 111                                | 190         | 0        | 301         | 149                | 121         | 0        | 0        | 270         | 62                 | 99         | 0        | 161         | 732         |
| 05:30 PM           | 110                                | 230         | 0        | 340         | 111                | 103         | 0        | 0        | 214         | 63                 | 80         | 0        | 143         | 697         |
| 05:45 PM           | 95                                 | 188         | 0        | 283         | 105                | 75          | 0        | 0        | 180         | 67                 | 64         | 0        | 131         | 594         |
| <b>Total</b>       | <b>405</b>                         | <b>795</b>  | <b>0</b> | <b>1200</b> | <b>510</b>         | <b>433</b>  | <b>0</b> | <b>0</b> | <b>943</b>  | <b>273</b>         | <b>317</b> | <b>0</b> | <b>590</b>  | <b>2733</b> |
| <b>Grand Total</b> | <b>1175</b>                        | <b>2256</b> | <b>1</b> | <b>3432</b> | <b>1515</b>        | <b>1218</b> | <b>1</b> | <b>0</b> | <b>2734</b> | <b>894</b>         | <b>918</b> | <b>0</b> | <b>1812</b> | <b>7978</b> |
| Apprch %           | 34.2                               | 65.7        | 0        |             | 55.4               | 44.6        | 0        | 0        |             | 49.3               | 50.7       | 0        |             |             |
| Total %            | 14.7                               | 28.3        | 0        | 43          | 19                 | 15.3        | 0        | 0        | 34.3        | 11.2               | 11.5       | 0        | 22.7        |             |

| Start Time   | Pepper's Ferry Blvd.<br>Southbound |            |            | US 11<br>Westbound |            |      |            | US 11<br>Eastbound |           |            | Int. Total |
|--|------------------------------------|------------|------------|--------------------|------------|------|------------|--------------------|-----------|------------|------------|
|  | Right                              | Left       | App. Total | Right              | Thru       | Left | App. Total | Thru               | Left      | App. Total |            |
| Peak Hour Analysis From 03:00 PM to 05:45 PM - Peak 1 of 1 |                                    |            |            |                    |            |      |            |                    |           |            |            |
| Peak Hour for Entire Intersection Begins at 04:45 PM       |                                    |            |            |                    |            |      |            |                    |           |            |            |
| 04:45 PM   | 91                                 | 213        | 304        | 140                | 101        | 0    | 241        | 74                 | 79        | 153        | 698        |
| 05:00 PM   | 89                                 | 187        | 276        | 145                | <b>134</b> | 0    | <b>279</b> | <b>81</b>          | 74        | 155        | 710        |
| 05:15 PM   | <b>111</b>                         | 190        | 301        | <b>149</b>         | 121        | 0    | 270        | 62                 | <b>99</b> | <b>161</b> | <b>732</b> |
| 05:30 PM   | 110                                | <b>230</b> | <b>340</b> | 111                | 103        | 0    | 214        | 63                 | 80        | 143        | 697        |
| Total Volume   | 401                                | 820        | 1221       | 545                | 459        | 0    | 1004       | 280                | 332       | 612        | 2837       |
| % App. Total   | 32.8                               | 67.2       |            | 54.3               | 45.7       | 0    |            | 45.8               | 54.2      |            |            |
| PHF  | .903                               | .891       | .898       | .914               | .856       | .000 | .900       | .864               | .838      | .950       | .969       |



# Peggy Malone & Associates, Inc.

## (888) 247-8602

File Name : TM 1-Pepper's Ferry and US 11 PM  
 Site Code :  
 Start Date : 2/4/2015  
 Page No : 1

### Groups Printed- Truck

| Start Time         | Pepper's Ferry Blvd.<br>Southbound |      |      |            | US 11<br>Westbound |      |      |      |            | US 11<br>Eastbound |      |      |            | Int. Total |
|--------------------|------------------------------------|------|------|------------|--------------------|------|------|------|------------|--------------------|------|------|------------|------------|
|                    | Right                              | Left | Peds | App. Total | Right              | Thru | Left | Peds | App. Total | Thru               | Left | Peds | App. Total |            |
| 03:00 PM           | 8                                  | 2    | 0    | 10         | 1                  | 5    | 0    | 0    | 6          | 3                  | 1    | 0    | 4          | 20         |
| 03:15 PM           | 2                                  | 1    | 0    | 3          | 1                  | 2    | 0    | 0    | 3          | 5                  | 3    | 0    | 8          | 14         |
| 03:30 PM           | 3                                  | 2    | 0    | 5          | 2                  | 4    | 0    | 0    | 6          | 3                  | 3    | 0    | 6          | 17         |
| 03:45 PM           | 2                                  | 1    | 0    | 3          | 3                  | 2    | 0    | 0    | 5          | 3                  | 1    | 0    | 4          | 12         |
| <b>Total</b>       | 15                                 | 6    | 0    | 21         | 7                  | 13   | 0    | 0    | 20         | 14                 | 8    | 0    | 22         | 63         |
| 04:00 PM           | 2                                  | 6    | 0    | 8          | 1                  | 1    | 0    | 0    | 2          | 1                  | 9    | 0    | 10         | 20         |
| 04:15 PM           | 1                                  | 5    | 0    | 6          | 0                  | 2    | 0    | 0    | 2          | 3                  | 1    | 0    | 4          | 12         |
| 04:30 PM           | 1                                  | 3    | 0    | 4          | 1                  | 2    | 0    | 0    | 3          | 3                  | 0    | 0    | 3          | 10         |
| 04:45 PM           | 4                                  | 0    | 0    | 4          | 1                  | 2    | 0    | 0    | 3          | 0                  | 1    | 0    | 1          | 8          |
| <b>Total</b>       | 8                                  | 14   | 0    | 22         | 3                  | 7    | 0    | 0    | 10         | 7                  | 11   | 0    | 18         | 50         |
| 05:00 PM           | 3                                  | 2    | 0    | 5          | 1                  | 2    | 0    | 0    | 3          | 3                  | 2    | 0    | 5          | 13         |
| 05:15 PM           | 0                                  | 2    | 0    | 2          | 1                  | 0    | 0    | 0    | 1          | 0                  | 0    | 0    | 0          | 3          |
| 05:30 PM           | 2                                  | 0    | 0    | 2          | 0                  | 0    | 0    | 0    | 0          | 1                  | 0    | 0    | 1          | 3          |
| 05:45 PM           | 0                                  | 0    | 0    | 0          | 2                  | 3    | 0    | 0    | 5          | 0                  | 2    | 0    | 2          | 7          |
| <b>Total</b>       | 5                                  | 4    | 0    | 9          | 4                  | 5    | 0    | 0    | 9          | 4                  | 4    | 0    | 8          | 26         |
| <b>Grand Total</b> | 28                                 | 24   | 0    | 52         | 14                 | 25   | 0    | 0    | 39         | 25                 | 23   | 0    | 48         | 139        |
| Apprch %           | 53.8                               | 46.2 | 0    |            | 35.9               | 64.1 | 0    | 0    |            | 52.1               | 47.9 | 0    |            |            |
| Total %            | 20.1                               | 17.3 | 0    | 37.4       | 10.1               | 18   | 0    | 0    | 28.1       | 18                 | 16.5 | 0    | 34.5       |            |

| Start Time   | Pepper's Ferry Blvd.<br>Southbound |      |            | US 11<br>Westbound |      |      |            | US 11<br>Eastbound |      |            | Int. Total |
|--|------------------------------------|------|------------|--------------------|------|------|------------|--------------------|------|------------|------------|
|  | Right                              | Left | App. Total | Right              | Thru | Left | App. Total | Thru               | Left | App. Total |            |
| Peak Hour Analysis From 03:00 PM to 05:45 PM - Peak 1 of 1 |                                    |      |            |                    |      |      |            |                    |      |            |            |
| Peak Hour for Entire Intersection Begins at 03:00 PM       |                                    |      |            |                    |      |      |            |                    |      |            |            |
| 03:00 PM   | 8                                  | 2    | 10         | 1                  | 5    | 0    | 6          | 3                  | 1    | 4          | 20         |
| 03:15 PM   | 2                                  | 1    | 3          | 1                  | 2    | 0    | 3          | 5                  | 3    | 8          | 14         |
| 03:30 PM   | 3                                  | 2    | 5          | 2                  | 4    | 0    | 6          | 3                  | 3    | 6          | 17         |
| 03:45 PM   | 2                                  | 1    | 3          | 3                  | 2    | 0    | 5          | 3                  | 1    | 4          | 12         |
| <b>Total Volume</b>  | 15                                 | 6    | 21         | 7                  | 13   | 0    | 20         | 14                 | 8    | 22         | 63         |
| % App. Total   | 71.4                               | 28.6 |            | 35                 | 65   | 0    |            | 63.6               | 36.4 |            |            |
| PHF  | .469                               | .750 | .525       | .583               | .650 | .000 | .833       | .700               | .667 | .688       | .788       |

# Peggy Malone & Associates, Inc.

## (888) 247-8602

File Name : TM 1-Pepper's Ferry and US 11 PM  
 Site Code :  
 Start Date : 2/4/2015  
 Page No : 1

### Groups Printed- Combined

| Start Time         | Pepper's Ferry Blvd.<br>Southbound |             |          |             | US 11<br>Westbound |             |          |          |             | US 11<br>Eastbound |            |          |             | Int. Total  |
|--------------------|------------------------------------|-------------|----------|-------------|--------------------|-------------|----------|----------|-------------|--------------------|------------|----------|-------------|-------------|
|                    | Right                              | Left        | Peds     | App. Total  | Right              | Thru        | Left     | Peds     | App. Total  | Thru               | Left       | Peds     | App. Total  |             |
| 03:00 PM           | 89                                 | 153         | 0        | 242         | 114                | 77          | 0        | 0        | 191         | 59                 | 75         | 0        | 134         | 567         |
| 03:15 PM           | 101                                | 188         | 1        | 290         | 112                | 85          | 1        | 0        | 198         | 83                 | 93         | 0        | 176         | 664         |
| 03:30 PM           | 98                                 | 173         | 0        | 271         | 136                | 125         | 0        | 0        | 261         | 85                 | 78         | 0        | 163         | 695         |
| 03:45 PM           | 95                                 | 169         | 0        | 264         | 123                | 107         | 0        | 0        | 230         | 99                 | 82         | 0        | 181         | 675         |
| <b>Total</b>       | <b>383</b>                         | <b>683</b>  | <b>1</b> | <b>1067</b> | <b>485</b>         | <b>394</b>  | <b>1</b> | <b>0</b> | <b>880</b>  | <b>326</b>         | <b>328</b> | <b>0</b> | <b>654</b>  | <b>2601</b> |
| 04:00 PM           | 106                                | 202         | 0        | 308         | 124                | 120         | 0        | 0        | 244         | 66                 | 78         | 0        | 144         | 696         |
| 04:15 PM           | 121                                | 168         | 0        | 289         | 121                | 78          | 0        | 0        | 199         | 97                 | 76         | 0        | 173         | 661         |
| 04:30 PM           | 88                                 | 215         | 0        | 303         | 144                | 110         | 0        | 0        | 254         | 79                 | 58         | 0        | 137         | 694         |
| 04:45 PM           | 95                                 | 213         | 0        | 308         | 141                | 103         | 0        | 0        | 244         | 74                 | 80         | 0        | 154         | 706         |
| <b>Total</b>       | <b>410</b>                         | <b>798</b>  | <b>0</b> | <b>1208</b> | <b>530</b>         | <b>411</b>  | <b>0</b> | <b>0</b> | <b>941</b>  | <b>316</b>         | <b>292</b> | <b>0</b> | <b>608</b>  | <b>2757</b> |
| 05:00 PM           | 92                                 | 189         | 0        | 281         | 146                | 136         | 0        | 0        | 282         | 84                 | 76         | 0        | 160         | 723         |
| 05:15 PM           | 111                                | 192         | 0        | 303         | 150                | 121         | 0        | 0        | 271         | 62                 | 99         | 0        | 161         | 735         |
| 05:30 PM           | 112                                | 230         | 0        | 342         | 111                | 103         | 0        | 0        | 214         | 64                 | 80         | 0        | 144         | 700         |
| 05:45 PM           | 95                                 | 188         | 0        | 283         | 107                | 78          | 0        | 0        | 185         | 67                 | 66         | 0        | 133         | 601         |
| <b>Total</b>       | <b>410</b>                         | <b>799</b>  | <b>0</b> | <b>1209</b> | <b>514</b>         | <b>438</b>  | <b>0</b> | <b>0</b> | <b>952</b>  | <b>277</b>         | <b>321</b> | <b>0</b> | <b>598</b>  | <b>2759</b> |
| <b>Grand Total</b> | <b>1203</b>                        | <b>2280</b> | <b>1</b> | <b>3484</b> | <b>1529</b>        | <b>1243</b> | <b>1</b> | <b>0</b> | <b>2773</b> | <b>919</b>         | <b>941</b> | <b>0</b> | <b>1860</b> | <b>8117</b> |
| Apprch %           | 34.5                               | 65.4        | 0        |             | 55.1               | 44.8        | 0        | 0        |             | 49.4               | 50.6       | 0        |             |             |
| Total %            | 14.8                               | 28.1        | 0        | 42.9        | 18.8               | 15.3        | 0        | 0        | 34.2        | 11.3               | 11.6       | 0        | 22.9        |             |

| Start Time   | Pepper's Ferry Blvd.<br>Southbound |            |            | US 11<br>Westbound |            |      |            | US 11<br>Eastbound |           |            | Int. Total |
|--|------------------------------------|------------|------------|--------------------|------------|------|------------|--------------------|-----------|------------|------------|
|  | Right                              | Left       | App. Total | Right              | Thru       | Left | App. Total | Thru               | Left      | App. Total |            |
| Peak Hour Analysis From 03:00 PM to 05:45 PM - Peak 1 of 1 |                                    |            |            |                    |            |      |            |                    |           |            |            |
| Peak Hour for Entire Intersection Begins at 04:45 PM       |                                    |            |            |                    |            |      |            |                    |           |            |            |
| 04:45 PM   | 95                                 | 213        | 308        | 141                | 103        | 0    | 244        | 74                 | 80        | 154        | 706        |
| 05:00 PM   | 92                                 | 189        | 281        | 146                | <b>136</b> | 0    | <b>282</b> | <b>84</b>          | 76        | 160        | 723        |
| 05:15 PM   | 111                                | 192        | 303        | <b>150</b>         | 121        | 0    | 271        | 62                 | <b>99</b> | <b>161</b> | <b>735</b> |
| 05:30 PM   | <b>112</b>                         | <b>230</b> | <b>342</b> | 111                | 103        | 0    | 214        | 64                 | 80        | 144        | 700        |
| Total Volume   | 410                                | 824        | 1234       | 548                | 463        | 0    | 1011       | 284                | 335       | 619        | 2864       |
| % App. Total   | 33.2                               | 66.8       |            | 54.2               | 45.8       | 0    |            | 45.9               | 54.1      |            |            |
| PHF  | .915                               | .896       | .902       | .913               | .851       | .000 | .896       | .845               | .846      | .961       | .974       |

# Peggy Malone & Associates, Inc.

## (888) 247-8602

File Name : TM 2-Cherry Ct and US 11 AM  
 Site Code :  
 Start Date : 2/4/2015  
 Page No : 1

Groups Printed- Car

| Start Time         | Business Driveway Southbound |          |          |          |            | US 11 Westbound |          |          |          |            | Cherry Ct. Northbound |          |          |          |            | US 11 Eastbound |          |          |          |            | Int. Total |
|--------------------|------------------------------|----------|----------|----------|------------|-----------------|----------|----------|----------|------------|-----------------------|----------|----------|----------|------------|-----------------|----------|----------|----------|------------|------------|
|                    | Right                        | Thru     | Left     | Peds     | App. Total | Right           | Thru     | Left     | Peds     | App. Total | Right                 | Thru     | Left     | Peds     | App. Total | Right           | Thru     | Left     | Peds     | App. Total |            |
| 07:00 AM           | 1                            | 0        | 0        | 0        | 1          | 0               | 0        | 0        | 0        | 0          | 0                     | 0        | 1        | 0        | 1          | 0               | 0        | 0        | 0        | 0          | 0          |
| 07:15 AM           | 2                            | 0        | 0        | 0        | 2          | 0               | 0        | 0        | 0        | 0          | 0                     | 0        | 0        | 0        | 0          | 0               | 0        | 1        | 0        | 1          | 1          |
| 07:30 AM           | 0                            | 0        | 0        | 0        | 0          | 0               | 0        | 0        | 0        | 0          | 1                     | 0        | 0        | 0        | 1          | 0               | 0        | 1        | 0        | 1          | 2          |
| 07:45 AM           | 0                            | 0        | 0        | 0        | 0          | 0               | 0        | 0        | 0        | 0          | 0                     | 0        | 1        | 0        | 1          | 1               | 0        | 1        | 0        | 2          | 3          |
| <b>Total</b>       | <b>3</b>                     | <b>0</b> | <b>0</b> | <b>0</b> | <b>3</b>   | <b>0</b>        | <b>0</b> | <b>0</b> | <b>0</b> | <b>0</b>   | <b>1</b>              | <b>0</b> | <b>2</b> | <b>0</b> | <b>3</b>   | <b>1</b>        | <b>0</b> | <b>3</b> | <b>0</b> | <b>4</b>   | <b>10</b>  |
| 08:00 AM           | 1                            | 0        | 0        | 0        | 1          | 0               | 0        | 0        | 0        | 0          | 2                     | 0        | 3        | 0        | 5          | 0               | 0        | 0        | 0        | 0          | 6          |
| 08:15 AM           | 0                            | 0        | 0        | 0        | 0          | 0               | 0        | 0        | 0        | 0          | 0                     | 0        | 0        | 0        | 0          | 0               | 0        | 0        | 0        | 0          | 0          |
| 08:30 AM           | 0                            | 0        | 0        | 0        | 0          | 0               | 0        | 0        | 0        | 0          | 0                     | 0        | 0        | 0        | 0          | 0               | 0        | 0        | 0        | 0          | 0          |
| 08:45 AM           | 0                            | 0        | 0        | 0        | 0          | 1               | 0        | 0        | 0        | 1          | 0                     | 0        | 0        | 1        | 1          | 0               | 0        | 2        | 0        | 2          | 4          |
| <b>Total</b>       | <b>1</b>                     | <b>0</b> | <b>0</b> | <b>0</b> | <b>1</b>   | <b>1</b>        | <b>0</b> | <b>0</b> | <b>0</b> | <b>1</b>   | <b>2</b>              | <b>0</b> | <b>3</b> | <b>1</b> | <b>6</b>   | <b>0</b>        | <b>0</b> | <b>2</b> | <b>0</b> | <b>2</b>   | <b>10</b>  |
| <b>Grand Total</b> | <b>4</b>                     | <b>0</b> | <b>0</b> | <b>0</b> | <b>4</b>   | <b>1</b>        | <b>0</b> | <b>0</b> | <b>0</b> | <b>1</b>   | <b>3</b>              | <b>0</b> | <b>5</b> | <b>1</b> | <b>9</b>   | <b>1</b>        | <b>0</b> | <b>5</b> | <b>0</b> | <b>6</b>   | <b>20</b>  |
| Apprch %           | 100                          | 0        | 0        | 0        |            | 100             | 0        | 0        | 0        |            | 33.3                  | 0        | 55.6     | 11.1     |            | 16.7            | 0        | 83.3     | 0        |            |            |
| Total %            | 20                           | 0        | 0        | 0        | 20         | 5               | 0        | 0        | 0        | 5          | 15                    | 0        | 25       | 5        | 45         | 5               | 0        | 25       | 0        | 30         |            |

| Start Time   | Business Driveway Southbound |          |          |            | US 11 Westbound |          |          |            | Cherry Ct. Northbound |          |          |            | US 11 Eastbound |          |          |            | Int. Total |
|--|------------------------------|----------|----------|------------|-----------------|----------|----------|------------|-----------------------|----------|----------|------------|-----------------|----------|----------|------------|------------|
|  | Right                        | Thru     | Left     | App. Total | Right           | Thru     | Left     | App. Total | Right                 | Thru     | Left     | App. Total | Right           | Thru     | Left     | App. Total |            |
| Peak Hour Analysis From 07:00 AM to 08:45 AM - Peak 1 of 1 |                              |          |          |            |                 |          |          |            |                       |          |          |            |                 |          |          |            |            |
| Peak Hour for Entire Intersection Begins at 07:15 AM       |                              |          |          |            |                 |          |          |            |                       |          |          |            |                 |          |          |            |            |
| 07:15 AM   | 2                            | 0        | 0        | 2          | 0               | 0        | 0        | 0          | 0                     | 0        | 0        | 0          | 0               | 0        | 1        | 1          | 3          |
| 07:30 AM   | 0                            | 0        | 0        | 0          | 0               | 0        | 0        | 0          | 1                     | 0        | 0        | 1          | 0               | 0        | 1        | 1          | 2          |
| 07:45 AM   | 0                            | 0        | 0        | 0          | 0               | 0        | 0        | 0          | 0                     | 0        | 1        | 1          | 1               | 0        | 1        | 2          | 3          |
| 08:00 AM   | 1                            | 0        | 0        | 1          | 0               | 0        | 0        | 0          | 2                     | 0        | 3        | 5          | 0               | 0        | 0        | 0          | 6          |
| <b>Total Volume</b>  | <b>3</b>                     | <b>0</b> | <b>0</b> | <b>3</b>   | <b>0</b>        | <b>0</b> | <b>0</b> | <b>0</b>   | <b>3</b>              | <b>0</b> | <b>4</b> | <b>7</b>   | <b>1</b>        | <b>0</b> | <b>3</b> | <b>4</b>   | <b>14</b>  |
| % App. Total   | 100                          | 0        | 0        |            | 0               | 0        | 0        |            | 42.9                  | 0        | 57.1     |            | 25              | 0        | 75       |            |            |
| PHF  | .375                         | .000     | .000     | .375       | .000            | .000     | .000     | .000       | .375                  | .000     | .333     | .350       | .250            | .000     | .750     | .500       | .583       |

# Peggy Malone & Associates, Inc.

## (888) 247-8602

File Name : TM 2-Cherry Ct and US 11 AM  
 Site Code :  
 Start Date : 2/4/2015  
 Page No : 1

Groups Printed- Truck

| Start Time  | Business Driveway Southbound |      |      |      |            | US 11 Westbound |      |      |      |            | Cherry Ct. Northbound |      |      |      |            | US 11 Eastbound |      |      |      |            | Int. Total |
|-------------|------------------------------|------|------|------|------------|-----------------|------|------|------|------------|-----------------------|------|------|------|------------|-----------------|------|------|------|------------|------------|
|             | Right                        | Thru | Left | Peds | App. Total | Right           | Thru | Left | Peds | App. Total | Right                 | Thru | Left | Peds | App. Total | Right           | Thru | Left | Peds | App. Total |            |
| 07:00 AM    | 0                            | 0    | 0    | 0    | 0          | 0               | 0    | 0    | 0    | 0          | 0                     | 0    | 0    | 0    | 0          | 0               | 0    | 0    | 0    | 0          | 0          |
| 07:15 AM    | 0                            | 0    | 0    | 0    | 0          | 0               | 0    | 0    | 0    | 0          | 0                     | 0    | 0    | 0    | 0          | 0               | 0    | 0    | 0    | 0          | 0          |
| 07:30 AM    | 0                            | 0    | 0    | 0    | 0          | 0               | 0    | 0    | 0    | 0          | 0                     | 0    | 0    | 0    | 0          | 0               | 0    | 0    | 0    | 0          | 0          |
| 07:45 AM    | 0                            | 0    | 0    | 0    | 0          | 0               | 0    | 0    | 0    | 0          | 0                     | 0    | 0    | 0    | 0          | 0               | 0    | 0    | 0    | 0          | 0          |
| Total       | 0                            | 0    | 0    | 0    | 0          | 0               | 0    | 0    | 0    | 0          | 0                     | 0    | 0    | 0    | 0          | 0               | 0    | 0    | 0    | 0          | 0          |
| 08:00 AM    | 0                            | 0    | 0    | 0    | 0          | 0               | 0    | 0    | 0    | 0          | 0                     | 0    | 0    | 0    | 0          | 0               | 0    | 0    | 0    | 0          | 0          |
| 08:15 AM    | 0                            | 0    | 0    | 0    | 0          | 0               | 0    | 0    | 0    | 0          | 0                     | 0    | 0    | 0    | 0          | 0               | 0    | 0    | 0    | 0          | 0          |
| 08:30 AM    | 0                            | 0    | 0    | 0    | 0          | 0               | 0    | 0    | 0    | 0          | 0                     | 0    | 0    | 0    | 0          | 0               | 0    | 0    | 0    | 0          | 0          |
| 08:45 AM    | 0                            | 0    | 0    | 0    | 0          | 0               | 0    | 0    | 0    | 0          | 0                     | 0    | 0    | 0    | 0          | 0               | 0    | 0    | 0    | 0          | 0          |
| Total       | 0                            | 0    | 0    | 0    | 0          | 0               | 0    | 0    | 0    | 0          | 0                     | 0    | 0    | 0    | 0          | 0               | 0    | 0    | 0    | 0          | 0          |
| Grand Total | 0                            | 0    | 0    | 0    | 0          | 0               | 0    | 0    | 0    | 0          | 0                     | 0    | 0    | 0    | 0          | 0               | 0    | 0    | 0    | 0          | 0          |
| Apprch %    | 0                            | 0    | 0    | 0    |            | 0               | 0    | 0    | 0    |            | 0                     | 0    | 0    | 0    |            | 0               | 0    | 0    | 0    |            |            |
| Total %     |                              |      |      |      |            |                 |      |      |      |            |                       |      |      |      |            |                 |      |      |      |            |            |

| Start Time   | Business Driveway Southbound |      |      |            | US 11 Westbound |      |      |            | Cherry Ct. Northbound |      |      |            | US 11 Eastbound |      |      |            | Int. Total |
|--|------------------------------|------|------|------------|-----------------|------|------|------------|-----------------------|------|------|------------|-----------------|------|------|------------|------------|
|  | Right                        | Thru | Left | App. Total | Right           | Thru | Left | App. Total | Right                 | Thru | Left | App. Total | Right           | Thru | Left | App. Total |            |
| Peak Hour Analysis From 07:00 AM to 08:45 AM - Peak 1 of 1 |                              |      |      |            |                 |      |      |            |                       |      |      |            |                 |      |      |            |            |
| Peak Hour for Entire Intersection Begins at 07:00 AM       |                              |      |      |            |                 |      |      |            |                       |      |      |            |                 |      |      |            |            |
| 07:00 AM   | 0                            | 0    | 0    | 0          | 0               | 0    | 0    | 0          | 0                     | 0    | 0    | 0          | 0               | 0    | 0    | 0          |            |
| 07:15 AM   | 0                            | 0    | 0    | 0          | 0               | 0    | 0    | 0          | 0                     | 0    | 0    | 0          | 0               | 0    | 0    | 0          |            |
| 07:30 AM   | 0                            | 0    | 0    | 0          | 0               | 0    | 0    | 0          | 0                     | 0    | 0    | 0          | 0               | 0    | 0    | 0          |            |
| 07:45 AM   | 0                            | 0    | 0    | 0          | 0               | 0    | 0    | 0          | 0                     | 0    | 0    | 0          | 0               | 0    | 0    | 0          |            |
| Total Volume   | 0                            | 0    | 0    | 0          | 0               | 0    | 0    | 0          | 0                     | 0    | 0    | 0          | 0               | 0    | 0    | 0          |            |
| % App. Total   | 0                            | 0    | 0    |            | 0               | 0    | 0    |            | 0                     | 0    | 0    |            | 0               | 0    | 0    |            |            |
| PHF  | .000                         | .000 | .000 | .000       | .000            | .000 | .000 | .000       | .000                  | .000 | .000 | .000       | .000            | .000 | .000 | .000       |            |

# Peggy Malone & Associates, Inc.

## (888) 247-8602

File Name : TM 2-Cherry Ct and US 11 AM  
 Site Code :  
 Start Date : 2/4/2015  
 Page No : 1

Groups Printed- Combined

| Start Time         | Business Driveway Southbound |          |          |          |            | US 11 Westbound |          |          |          |            | Cherry Ct. Northbound |          |          |          |            | US 11 Eastbound |          |          |          |            | Int. Total |
|--------------------|------------------------------|----------|----------|----------|------------|-----------------|----------|----------|----------|------------|-----------------------|----------|----------|----------|------------|-----------------|----------|----------|----------|------------|------------|
|                    | Right                        | Thru     | Left     | Peds     | App. Total | Right           | Thru     | Left     | Peds     | App. Total | Right                 | Thru     | Left     | Peds     | App. Total | Right           | Thru     | Left     | Peds     | App. Total |            |
| 07:00 AM           | 1                            | 0        | 0        | 0        | 1          | 0               | 0        | 0        | 0        | 0          | 0                     | 0        | 1        | 0        | 1          | 0               | 0        | 0        | 0        | 0          | 0          |
| 07:15 AM           | 2                            | 0        | 0        | 0        | 2          | 0               | 0        | 0        | 0        | 0          | 0                     | 0        | 0        | 0        | 0          | 0               | 0        | 1        | 0        | 1          | 1          |
| 07:30 AM           | 0                            | 0        | 0        | 0        | 0          | 0               | 0        | 0        | 0        | 0          | 1                     | 0        | 0        | 0        | 1          | 0               | 0        | 1        | 0        | 1          | 2          |
| 07:45 AM           | 0                            | 0        | 0        | 0        | 0          | 0               | 0        | 0        | 0        | 0          | 0                     | 0        | 1        | 0        | 1          | 1               | 0        | 1        | 0        | 2          | 3          |
| <b>Total</b>       | <b>3</b>                     | <b>0</b> | <b>0</b> | <b>0</b> | <b>3</b>   | <b>0</b>        | <b>0</b> | <b>0</b> | <b>0</b> | <b>0</b>   | <b>1</b>              | <b>0</b> | <b>2</b> | <b>0</b> | <b>3</b>   | <b>1</b>        | <b>0</b> | <b>3</b> | <b>0</b> | <b>4</b>   | <b>10</b>  |
| 08:00 AM           | 1                            | 0        | 0        | 0        | 1          | 0               | 0        | 0        | 0        | 0          | 2                     | 0        | 3        | 0        | 5          | 0               | 0        | 0        | 0        | 0          | 6          |
| 08:15 AM           | 0                            | 0        | 0        | 0        | 0          | 0               | 0        | 0        | 0        | 0          | 0                     | 0        | 0        | 0        | 0          | 0               | 0        | 0        | 0        | 0          | 0          |
| 08:30 AM           | 0                            | 0        | 0        | 0        | 0          | 0               | 0        | 0        | 0        | 0          | 0                     | 0        | 0        | 0        | 0          | 0               | 0        | 0        | 0        | 0          | 0          |
| 08:45 AM           | 0                            | 0        | 0        | 0        | 0          | 1               | 0        | 0        | 0        | 1          | 0                     | 0        | 0        | 1        | 1          | 0               | 0        | 2        | 0        | 2          | 4          |
| <b>Total</b>       | <b>1</b>                     | <b>0</b> | <b>0</b> | <b>0</b> | <b>1</b>   | <b>1</b>        | <b>0</b> | <b>0</b> | <b>0</b> | <b>1</b>   | <b>2</b>              | <b>0</b> | <b>3</b> | <b>1</b> | <b>6</b>   | <b>0</b>        | <b>0</b> | <b>2</b> | <b>0</b> | <b>2</b>   | <b>10</b>  |
| <b>Grand Total</b> | <b>4</b>                     | <b>0</b> | <b>0</b> | <b>0</b> | <b>4</b>   | <b>1</b>        | <b>0</b> | <b>0</b> | <b>0</b> | <b>1</b>   | <b>3</b>              | <b>0</b> | <b>5</b> | <b>1</b> | <b>9</b>   | <b>1</b>        | <b>0</b> | <b>5</b> | <b>0</b> | <b>6</b>   | <b>20</b>  |
| Apprch %           | 100                          | 0        | 0        | 0        |            | 100             | 0        | 0        | 0        |            | 33.3                  | 0        | 55.6     | 11.1     |            | 16.7            | 0        | 83.3     | 0        |            |            |
| Total %            | 20                           | 0        | 0        | 0        | 20         | 5               | 0        | 0        | 0        | 5          | 15                    | 0        | 25       | 5        | 45         | 5               | 0        | 25       | 0        | 30         |            |

| Start Time   | Business Driveway Southbound |          |          |            | US 11 Westbound |          |          |            | Cherry Ct. Northbound |          |          |            | US 11 Eastbound |          |          |            | Int. Total |
|--|------------------------------|----------|----------|------------|-----------------|----------|----------|------------|-----------------------|----------|----------|------------|-----------------|----------|----------|------------|------------|
|  | Right                        | Thru     | Left     | App. Total | Right           | Thru     | Left     | App. Total | Right                 | Thru     | Left     | App. Total | Right           | Thru     | Left     | App. Total |            |
| Peak Hour Analysis From 07:00 AM to 08:45 AM - Peak 1 of 1 |                              |          |          |            |                 |          |          |            |                       |          |          |            |                 |          |          |            |            |
| Peak Hour for Entire Intersection Begins at 07:15 AM       |                              |          |          |            |                 |          |          |            |                       |          |          |            |                 |          |          |            |            |
| 07:15 AM   | 2                            | 0        | 0        | 2          | 0               | 0        | 0        | 0          | 0                     | 0        | 0        | 0          | 0               | 0        | 1        | 1          | 3          |
| 07:30 AM   | 0                            | 0        | 0        | 0          | 0               | 0        | 0        | 0          | 1                     | 0        | 0        | 1          | 0               | 0        | 1        | 1          | 2          |
| 07:45 AM   | 0                            | 0        | 0        | 0          | 0               | 0        | 0        | 0          | 0                     | 0        | 1        | 1          | 1               | 0        | 1        | 2          | 3          |
| 08:00 AM   | 1                            | 0        | 0        | 1          | 0               | 0        | 0        | 0          | 2                     | 0        | 3        | 5          | 0               | 0        | 0        | 0          | 6          |
| <b>Total Volume</b>  | <b>3</b>                     | <b>0</b> | <b>0</b> | <b>3</b>   | <b>0</b>        | <b>0</b> | <b>0</b> | <b>0</b>   | <b>3</b>              | <b>0</b> | <b>4</b> | <b>7</b>   | <b>1</b>        | <b>0</b> | <b>3</b> | <b>4</b>   | <b>14</b>  |
| % App. Total   | 100                          | 0        | 0        |            | 0               | 0        | 0        |            | 42.9                  | 0        | 57.1     |            | 25              | 0        | 75       |            |            |
| PHF  | .375                         | .000     | .000     | .375       | .000            | .000     | .000     | .000       | .375                  | .000     | .333     | .350       | .250            | .000     | .750     | .500       | .583       |

# Peggy Malone & Associates, Inc.

## (888) 247-8602

File Name : TM 2-Cherry Ct and US 11 MID  
 Site Code :  
 Start Date : 2/4/2015  
 Page No : 1

Groups Printed- Car

| Start Time         | Business Driveway Southbound |      |      |      |            | US 11 Westbound |      |      |      |            | Cherry Ct. Northbound |      |      |      |            | US 11 Eastbound |      |      |      |            | Int. Total |
|--------------------|------------------------------|------|------|------|------------|-----------------|------|------|------|------------|-----------------------|------|------|------|------------|-----------------|------|------|------|------------|------------|
|                    | Right                        | Thru | Left | Peds | App. Total | Right           | Thru | Left | Peds | App. Total | Right                 | Thru | Left | Peds | App. Total | Right           | Thru | Left | Peds | App. Total |            |
| 11:00 AM           | 3                            | 0    | 1    | 0    | 4          | 1               | 0    | 0    | 0    | 1          | 1                     | 0    | 0    | 0    | 1          | 1               | 0    | 3    | 0    | 4          | 10         |
| 11:15 AM           | 8                            | 0    | 0    | 0    | 8          | 0               | 0    | 0    | 0    | 0          | 0                     | 0    | 0    | 0    | 0          | 0               | 0    | 1    | 0    | 1          | 9          |
| 11:30 AM           | 1                            | 0    | 0    | 0    | 1          | 0               | 0    | 0    | 0    | 0          | 0                     | 0    | 1    | 0    | 1          | 1               | 0    | 1    | 0    | 2          | 4          |
| 11:45 AM           | 4                            | 0    | 1    | 0    | 5          | 0               | 0    | 1    | 0    | 1          | 0                     | 0    | 2    | 1    | 3          | 1               | 0    | 0    | 0    | 1          | 10         |
| <b>Total</b>       | 16                           | 0    | 2    | 0    | 18         | 1               | 0    | 1    | 0    | 2          | 1                     | 0    | 3    | 1    | 5          | 3               | 0    | 5    | 0    | 8          | 33         |
| 12:00 PM           | 3                            | 0    | 2    | 0    | 5          | 1               | 0    | 1    | 0    | 2          | 1                     | 0    | 0    | 0    | 1          | 0               | 0    | 2    | 0    | 2          | 10         |
| 12:15 PM           | 3                            | 0    | 0    | 0    | 3          | 0               | 0    | 1    | 0    | 1          | 1                     | 0    | 0    | 0    | 1          | 0               | 0    | 1    | 0    | 1          | 6          |
| 12:30 PM           | 3                            | 0    | 1    | 0    | 4          | 0               | 0    | 0    | 0    | 0          | 0                     | 0    | 0    | 0    | 0          | 0               | 0    | 1    | 0    | 1          | 5          |
| 12:45 PM           | 2                            | 0    | 1    | 0    | 3          | 0               | 0    | 0    | 0    | 0          | 0                     | 0    | 2    | 0    | 2          | 0               | 0    | 1    | 0    | 1          | 6          |
| <b>Total</b>       | 11                           | 0    | 4    | 0    | 15         | 1               | 0    | 2    | 0    | 3          | 2                     | 0    | 2    | 0    | 4          | 0               | 0    | 5    | 0    | 5          | 27         |
| <b>Grand Total</b> | 27                           | 0    | 6    | 0    | 33         | 2               | 0    | 3    | 0    | 5          | 3                     | 0    | 5    | 1    | 9          | 3               | 0    | 10   | 0    | 13         | 60         |
| Apprch %           | 81.8                         | 0    | 18.2 | 0    |            | 40              | 0    | 60   | 0    |            | 33.3                  | 0    | 55.6 | 11.1 |            | 23.1            | 0    | 76.9 | 0    |            |            |
| Total %            | 45                           | 0    | 10   | 0    | 55         | 3.3             | 0    | 5    | 0    | 8.3        | 5                     | 0    | 8.3  | 1.7  | 15         | 5               | 0    | 16.7 | 0    | 21.7       |            |

| Start Time   | Business Driveway Southbound |      |      |            | US 11 Westbound |      |      |            | Cherry Ct. Northbound |      |      |            | US 11 Eastbound |      |      |            | Int. Total |
|--|------------------------------|------|------|------------|-----------------|------|------|------------|-----------------------|------|------|------------|-----------------|------|------|------------|------------|
|  | Right                        | Thru | Left | App. Total | Right           | Thru | Left | App. Total | Right                 | Thru | Left | App. Total | Right           | Thru | Left | App. Total |            |
| Peak Hour Analysis From 11:00 AM to 12:45 PM - Peak 1 of 1 |                              |      |      |            |                 |      |      |            |                       |      |      |            |                 |      |      |            |            |
| Peak Hour for Entire Intersection Begins at 11:00 AM       |                              |      |      |            |                 |      |      |            |                       |      |      |            |                 |      |      |            |            |
| 11:00 AM   | 3                            | 0    | 1    | 4          | 1               | 0    | 0    | 1          | 1                     | 0    | 0    | 1          | 1               | 0    | 3    | 4          | 10         |
| 11:15 AM   | 8                            | 0    | 0    | 8          | 0               | 0    | 0    | 0          | 0                     | 0    | 0    | 0          | 0               | 0    | 1    | 1          | 9          |
| 11:30 AM   | 1                            | 0    | 0    | 1          | 0               | 0    | 0    | 0          | 0                     | 0    | 1    | 1          | 1               | 0    | 1    | 2          | 4          |
| 11:45 AM   | 4                            | 0    | 1    | 5          | 0               | 0    | 1    | 1          | 0                     | 0    | 2    | 2          | 1               | 0    | 0    | 1          | 9          |
| <b>Total Volume</b>  | 16                           | 0    | 2    | 18         | 1               | 0    | 1    | 2          | 1                     | 0    | 3    | 4          | 3               | 0    | 5    | 8          | 32         |
| % App. Total   | 88.9                         | 0    | 11.1 |            | 50              | 0    | 50   |            | 25                    | 0    | 75   |            | 37.5            | 0    | 62.5 |            |            |
| PHF  | .500                         | .000 | .500 | .563       | .250            | .000 | .250 | .500       | .250                  | .000 | .375 | .500       | .750            | .000 | .417 | .500       | .800       |

# Peggy Malone & Associates, Inc.

## (888) 247-8602

File Name : TM 2-Cherry Ct and US 11 MID  
 Site Code :  
 Start Date : 2/4/2015  
 Page No : 1

Groups Printed- Truck

| Start Time         | Business Driveway Southbound |      |      |      |            | US 11 Westbound |      |      |      |            | Cherry Ct. Northbound |      |      |      |            | US 11 Eastbound |      |      |      |            | Int. Total |
|--------------------|------------------------------|------|------|------|------------|-----------------|------|------|------|------------|-----------------------|------|------|------|------------|-----------------|------|------|------|------------|------------|
|                    | Right                        | Thru | Left | Peds | App. Total | Right           | Thru | Left | Peds | App. Total | Right                 | Thru | Left | Peds | App. Total | Right           | Thru | Left | Peds | App. Total |            |
| 11:00 AM           | 0                            | 0    | 0    | 0    | 0          | 0               | 0    | 0    | 0    | 0          | 0                     | 0    | 0    | 0    | 0          | 0               | 0    | 0    | 0    | 0          | 0          |
| 11:15 AM           | 0                            | 0    | 0    | 0    | 0          | 0               | 0    | 0    | 0    | 0          | 0                     | 0    | 0    | 0    | 0          | 0               | 0    | 0    | 0    | 0          | 0          |
| 11:30 AM           | 1                            | 0    | 0    | 0    | 1          | 0               | 0    | 0    | 0    | 0          | 0                     | 0    | 0    | 0    | 0          | 0               | 0    | 0    | 0    | 0          | 1          |
| 11:45 AM           | 0                            | 0    | 0    | 0    | 0          | 0               | 0    | 0    | 0    | 0          | 0                     | 0    | 0    | 0    | 0          | 0               | 0    | 0    | 0    | 0          | 0          |
| <b>Total</b>       | 1                            | 0    | 0    | 0    | 1          | 0               | 0    | 0    | 0    | 0          | 0                     | 0    | 0    | 0    | 0          | 0               | 0    | 0    | 0    | 0          | 1          |
| 12:00 PM           | 0                            | 0    | 0    | 0    | 0          | 0               | 0    | 0    | 0    | 0          | 0                     | 0    | 0    | 0    | 0          | 0               | 0    | 0    | 0    | 0          | 0          |
| 12:15 PM           | 0                            | 0    | 0    | 0    | 0          | 0               | 0    | 0    | 0    | 0          | 0                     | 0    | 0    | 0    | 0          | 0               | 0    | 0    | 0    | 0          | 0          |
| 12:30 PM           | 0                            | 0    | 0    | 0    | 0          | 0               | 0    | 0    | 0    | 0          | 0                     | 0    | 0    | 0    | 0          | 0               | 0    | 0    | 0    | 0          | 0          |
| 12:45 PM           | 1                            | 0    | 0    | 0    | 1          | 0               | 0    | 0    | 0    | 0          | 0                     | 0    | 0    | 0    | 0          | 0               | 0    | 0    | 0    | 0          | 1          |
| <b>Total</b>       | 1                            | 0    | 0    | 0    | 1          | 0               | 0    | 0    | 0    | 0          | 0                     | 0    | 0    | 0    | 0          | 0               | 0    | 0    | 0    | 0          | 1          |
| <b>Grand Total</b> | 2                            | 0    | 0    | 0    | 2          | 0               | 0    | 0    | 0    | 0          | 0                     | 0    | 0    | 0    | 0          | 0               | 0    | 0    | 0    | 0          | 2          |
| Apprch %           | 100                          | 0    | 0    | 0    |            | 0               | 0    | 0    | 0    |            | 0                     | 0    | 0    | 0    |            | 0               | 0    | 0    | 0    |            |            |
| Total %            | 100                          | 0    | 0    | 0    | 100        | 0               | 0    | 0    | 0    | 0          | 0                     | 0    | 0    | 0    | 0          | 0               | 0    | 0    | 0    | 0          |            |

| Start Time   | Business Driveway Southbound |      |      |            | US 11 Westbound |      |      |            | Cherry Ct. Northbound |      |      |            | US 11 Eastbound |      |      |            | Int. Total |
|--|------------------------------|------|------|------------|-----------------|------|------|------------|-----------------------|------|------|------------|-----------------|------|------|------------|------------|
|  | Right                        | Thru | Left | App. Total | Right           | Thru | Left | App. Total | Right                 | Thru | Left | App. Total | Right           | Thru | Left | App. Total |            |
| Peak Hour Analysis From 11:00 AM to 12:45 PM - Peak 1 of 1 |                              |      |      |            |                 |      |      |            |                       |      |      |            |                 |      |      |            |            |
| Peak Hour for Entire Intersection Begins at 11:00 AM       |                              |      |      |            |                 |      |      |            |                       |      |      |            |                 |      |      |            |            |
| 11:00 AM   | 0                            | 0    | 0    | 0          | 0               | 0    | 0    | 0          | 0                     | 0    | 0    | 0          | 0               | 0    | 0    | 0          |            |
| 11:15 AM   | 0                            | 0    | 0    | 0          | 0               | 0    | 0    | 0          | 0                     | 0    | 0    | 0          | 0               | 0    | 0    | 0          |            |
| 11:30 AM   | 1                            | 0    | 0    | 1          | 0               | 0    | 0    | 0          | 0                     | 0    | 0    | 0          | 0               | 0    | 0    | 0          |            |
| 11:45 AM   | 0                            | 0    | 0    | 0          | 0               | 0    | 0    | 0          | 0                     | 0    | 0    | 0          | 0               | 0    | 0    | 0          |            |
| <b>Total Volume</b>  | 1                            | 0    | 0    | 1          | 0               | 0    | 0    | 0          | 0                     | 0    | 0    | 0          | 0               | 0    | 0    | 0          |            |
| % App. Total   | 100                          | 0    | 0    |            | 0               | 0    | 0    |            | 0                     | 0    | 0    |            | 0               | 0    | 0    |            |            |
| PHF  | .250                         | .000 | .000 | .250       | .000            | .000 | .000 | .000       | .000                  | .000 | .000 | .000       | .000            | .000 | .000 | .000       |            |

# Peggy Malone & Associates, Inc.

## (888) 247-8602

File Name : TM 2-Cherry Ct and US 11 MID  
 Site Code :  
 Start Date : 2/4/2015  
 Page No : 1

Groups Printed- Combined

| Start Time         | Business Driveway Southbound |      |      |      |            | US 11 Westbound |      |      |      |            | Cherry Ct. Northbound |      |      |      |            | US 11 Eastbound |      |      |      |            | Int. Total |
|--------------------|------------------------------|------|------|------|------------|-----------------|------|------|------|------------|-----------------------|------|------|------|------------|-----------------|------|------|------|------------|------------|
|                    | Right                        | Thru | Left | Peds | App. Total | Right           | Thru | Left | Peds | App. Total | Right                 | Thru | Left | Peds | App. Total | Right           | Thru | Left | Peds | App. Total |            |
| 11:00 AM           | 3                            | 0    | 1    | 0    | 4          | 1               | 0    | 0    | 0    | 1          | 1                     | 0    | 0    | 0    | 1          | 1               | 0    | 3    | 0    | 4          | 10         |
| 11:15 AM           | 8                            | 0    | 0    | 0    | 8          | 0               | 0    | 0    | 0    | 0          | 0                     | 0    | 0    | 0    | 0          | 0               | 0    | 1    | 0    | 1          | 9          |
| 11:30 AM           | 2                            | 0    | 0    | 0    | 2          | 0               | 0    | 0    | 0    | 0          | 0                     | 0    | 1    | 0    | 1          | 1               | 0    | 1    | 0    | 2          | 5          |
| 11:45 AM           | 4                            | 0    | 1    | 0    | 5          | 0               | 0    | 1    | 0    | 1          | 0                     | 0    | 2    | 1    | 3          | 1               | 0    | 0    | 0    | 1          | 10         |
| <b>Total</b>       | 17                           | 0    | 2    | 0    | 19         | 1               | 0    | 1    | 0    | 2          | 1                     | 0    | 3    | 1    | 5          | 3               | 0    | 5    | 0    | 8          | 34         |
| 12:00 PM           | 3                            | 0    | 2    | 0    | 5          | 1               | 0    | 1    | 0    | 2          | 1                     | 0    | 0    | 0    | 1          | 0               | 0    | 2    | 0    | 2          | 10         |
| 12:15 PM           | 3                            | 0    | 0    | 0    | 3          | 0               | 0    | 1    | 0    | 1          | 1                     | 0    | 0    | 0    | 1          | 0               | 0    | 1    | 0    | 1          | 6          |
| 12:30 PM           | 3                            | 0    | 1    | 0    | 4          | 0               | 0    | 0    | 0    | 0          | 0                     | 0    | 0    | 0    | 0          | 0               | 0    | 1    | 0    | 1          | 5          |
| 12:45 PM           | 3                            | 0    | 1    | 0    | 4          | 0               | 0    | 0    | 0    | 0          | 0                     | 0    | 2    | 0    | 2          | 0               | 0    | 1    | 0    | 1          | 7          |
| <b>Total</b>       | 12                           | 0    | 4    | 0    | 16         | 1               | 0    | 2    | 0    | 3          | 2                     | 0    | 2    | 0    | 4          | 0               | 0    | 5    | 0    | 5          | 28         |
| <b>Grand Total</b> | 29                           | 0    | 6    | 0    | 35         | 2               | 0    | 3    | 0    | 5          | 3                     | 0    | 5    | 1    | 9          | 3               | 0    | 10   | 0    | 13         | 62         |
| Apprch %           | 82.9                         | 0    | 17.1 | 0    |            | 40              | 0    | 60   | 0    |            | 33.3                  | 0    | 55.6 | 11.1 |            | 23.1            | 0    | 76.9 | 0    |            |            |
| Total %            | 46.8                         | 0    | 9.7  | 0    | 56.5       | 3.2             | 0    | 4.8  | 0    | 8.1        | 4.8                   | 0    | 8.1  | 1.6  | 14.5       | 4.8             | 0    | 16.1 | 0    | 21         |            |

| Start Time   | Business Driveway Southbound |      |      |            | US 11 Westbound |      |      |            | Cherry Ct. Northbound |      |      |            | US 11 Eastbound |      |      |            | Int. Total |
|--|------------------------------|------|------|------------|-----------------|------|------|------------|-----------------------|------|------|------------|-----------------|------|------|------------|------------|
|  | Right                        | Thru | Left | App. Total | Right           | Thru | Left | App. Total | Right                 | Thru | Left | App. Total | Right           | Thru | Left | App. Total |            |
| Peak Hour Analysis From 11:00 AM to 12:45 PM - Peak 1 of 1 |                              |      |      |            |                 |      |      |            |                       |      |      |            |                 |      |      |            |            |
| Peak Hour for Entire Intersection Begins at 11:00 AM       |                              |      |      |            |                 |      |      |            |                       |      |      |            |                 |      |      |            |            |
| 11:00 AM   | 3                            | 0    | 1    | 4          | 1               | 0    | 0    | 1          | 1                     | 0    | 0    | 1          | 1               | 0    | 3    | 4          | 10         |
| 11:15 AM   | 8                            | 0    | 0    | 8          | 0               | 0    | 0    | 0          | 0                     | 0    | 0    | 0          | 0               | 0    | 1    | 1          | 9          |
| 11:30 AM   | 2                            | 0    | 0    | 2          | 0               | 0    | 0    | 0          | 0                     | 0    | 1    | 1          | 1               | 0    | 1    | 2          | 5          |
| 11:45 AM   | 4                            | 0    | 1    | 5          | 0               | 0    | 1    | 1          | 0                     | 0    | 2    | 2          | 1               | 0    | 0    | 1          | 9          |
| <b>Total Volume</b>  | 17                           | 0    | 2    | 19         | 1               | 0    | 1    | 2          | 1                     | 0    | 3    | 4          | 3               | 0    | 5    | 8          | 33         |
| % App. Total   | 89.5                         | 0    | 10.5 |            | 50              | 0    | 50   |            | 25                    | 0    | 75   |            | 37.5            | 0    | 62.5 |            |            |
| PHF  | .531                         | .000 | .500 | .594       | .250            | .000 | .250 | .500       | .250                  | .000 | .375 | .500       | .750            | .000 | .417 | .500       | .825       |



# Peggy Malone & Associates, Inc.

## (888) 247-8602

File Name : TM 2-Cherry Ct and US 11 PM  
 Site Code :  
 Start Date : 2/4/2015  
 Page No : 1

Groups Printed- Car

| Start Time         | Business Driveway Southbound |          |           |          |            | US 11 Westbound |          |          |          |            | Cherry Ct. Northbound |          |          |          |            | US 11 Eastbound |          |           |          |            | Int. Total |
|--------------------|------------------------------|----------|-----------|----------|------------|-----------------|----------|----------|----------|------------|-----------------------|----------|----------|----------|------------|-----------------|----------|-----------|----------|------------|------------|
|                    | Right                        | Thru     | Left      | Peds     | App. Total | Right           | Thru     | Left     | Peds     | App. Total | Right                 | Thru     | Left     | Peds     | App. Total | Right           | Thru     | Left      | Peds     | App. Total |            |
| 03:00 PM           | 6                            | 0        | 2         | 0        | 8          | 0               | 0        | 1        | 0        | 1          | 0                     | 0        | 0        | 0        | 0          | 1               | 0        | 2         | 0        | 3          | 12         |
| 03:15 PM           | 4                            | 0        | 1         | 1        | 6          | 0               | 0        | 0        | 0        | 0          | 0                     | 0        | 0        | 0        | 0          | 0               | 0        | 2         | 0        | 2          | 8          |
| 03:30 PM           | 3                            | 0        | 1         | 0        | 4          | 0               | 0        | 0        | 0        | 0          | 1                     | 0        | 0        | 0        | 1          | 1               | 0        | 1         | 0        | 2          | 7          |
| 03:45 PM           | 3                            | 0        | 1         | 0        | 4          | 0               | 0        | 0        | 0        | 0          | 3                     | 0        | 0        | 0        | 3          | 0               | 0        | 5         | 0        | 5          | 12         |
| <b>Total</b>       | <b>16</b>                    | <b>0</b> | <b>5</b>  | <b>1</b> | <b>22</b>  | <b>0</b>        | <b>0</b> | <b>1</b> | <b>0</b> | <b>1</b>   | <b>4</b>              | <b>0</b> | <b>0</b> | <b>0</b> | <b>4</b>   | <b>2</b>        | <b>0</b> | <b>10</b> | <b>0</b> | <b>12</b>  | <b>39</b>  |
| 04:00 PM           | 4                            | 0        | 2         | 1        | 7          | 0               | 0        | 0        | 0        | 0          | 0                     | 0        | 0        | 0        | 0          | 2               | 0        | 5         | 0        | 7          | 14         |
| 04:15 PM           | 5                            | 0        | 0         | 0        | 5          | 0               | 0        | 0        | 0        | 0          | 0                     | 0        | 0        | 0        | 0          | 0               | 0        | 2         | 0        | 2          | 7          |
| 04:30 PM           | 9                            | 0        | 1         | 0        | 10         | 0               | 0        | 0        | 0        | 0          | 0                     | 0        | 3        | 0        | 3          | 0               | 0        | 4         | 0        | 4          | 17         |
| 04:45 PM           | 7                            | 0        | 2         | 0        | 9          | 0               | 0        | 1        | 0        | 1          | 0                     | 0        | 0        | 0        | 0          | 2               | 0        | 1         | 0        | 3          | 13         |
| <b>Total</b>       | <b>25</b>                    | <b>0</b> | <b>5</b>  | <b>1</b> | <b>31</b>  | <b>0</b>        | <b>0</b> | <b>1</b> | <b>0</b> | <b>1</b>   | <b>0</b>              | <b>0</b> | <b>3</b> | <b>0</b> | <b>3</b>   | <b>4</b>        | <b>0</b> | <b>12</b> | <b>0</b> | <b>16</b>  | <b>51</b>  |
| 05:00 PM           | 3                            | 0        | 0         | 1        | 4          | 0               | 0        | 0        | 0        | 0          | 0                     | 0        | 1        | 0        | 1          | 1               | 0        | 1         | 0        | 2          | 7          |
| 05:15 PM           | 4                            | 0        | 2         | 0        | 6          | 0               | 0        | 1        | 0        | 1          | 0                     | 0        | 0        | 1        | 1          | 1               | 0        | 0         | 0        | 1          | 9          |
| 05:30 PM           | 3                            | 0        | 2         | 0        | 5          | 0               | 0        | 0        | 0        | 0          | 0                     | 0        | 0        | 0        | 0          | 0               | 0        | 1         | 0        | 1          | 6          |
| 05:45 PM           | 2                            | 0        | 1         | 0        | 3          | 2               | 0        | 1        | 0        | 3          | 0                     | 0        | 0        | 0        | 0          | 0               | 0        | 2         | 0        | 2          | 8          |
| <b>Total</b>       | <b>12</b>                    | <b>0</b> | <b>5</b>  | <b>1</b> | <b>18</b>  | <b>2</b>        | <b>0</b> | <b>2</b> | <b>0</b> | <b>4</b>   | <b>0</b>              | <b>0</b> | <b>1</b> | <b>1</b> | <b>2</b>   | <b>2</b>        | <b>0</b> | <b>4</b>  | <b>0</b> | <b>6</b>   | <b>30</b>  |
| <b>Grand Total</b> | <b>53</b>                    | <b>0</b> | <b>15</b> | <b>3</b> | <b>71</b>  | <b>2</b>        | <b>0</b> | <b>4</b> | <b>0</b> | <b>6</b>   | <b>4</b>              | <b>0</b> | <b>4</b> | <b>1</b> | <b>9</b>   | <b>8</b>        | <b>0</b> | <b>26</b> | <b>0</b> | <b>34</b>  | <b>120</b> |
| Apprch %           | 74.6                         | 0        | 21.1      | 4.2      |            | 33.3            | 0        | 66.7     | 0        |            | 44.4                  | 0        | 44.4     | 11.1     |            | 23.5            | 0        | 76.5      | 0        |            |            |
| Total %            | 44.2                         | 0        | 12.5      | 2.5      | 59.2       | 1.7             | 0        | 3.3      | 0        | 5          | 3.3                   | 0        | 3.3      | 0.8      | 7.5        | 6.7             | 0        | 21.7      | 0        | 28.3       |            |

| Start Time   | Business Driveway Southbound |          |          |            | US 11 Westbound |          |          |            | Cherry Ct. Northbound |          |          |            | US 11 Eastbound |          |           |            | Int. Total |
|--|------------------------------|----------|----------|------------|-----------------|----------|----------|------------|-----------------------|----------|----------|------------|-----------------|----------|-----------|------------|------------|
|  | Right                        | Thru     | Left     | App. Total | Right           | Thru     | Left     | App. Total | Right                 | Thru     | Left     | App. Total | Right           | Thru     | Left      | App. Total |            |
| Peak Hour Analysis From 03:00 PM to 05:45 PM - Peak 1 of 1 |                              |          |          |            |                 |          |          |            |                       |          |          |            |                 |          |           |            |            |
| Peak Hour for Entire Intersection Begins at 04:00 PM       |                              |          |          |            |                 |          |          |            |                       |          |          |            |                 |          |           |            |            |
| 04:00 PM   | 4                            | 0        | 2        | 6          | 0               | 0        | 0        | 0          | 0                     | 0        | 0        | 0          | 2               | 0        | 5         | 7          | 13         |
| 04:15 PM   | 5                            | 0        | 0        | 5          | 0               | 0        | 0        | 0          | 0                     | 0        | 0        | 0          | 0               | 0        | 2         | 2          | 7          |
| 04:30 PM   | 9                            | 0        | 1        | 10         | 0               | 0        | 0        | 0          | 0                     | 0        | 3        | 3          | 0               | 0        | 4         | 4          | 17         |
| 04:45 PM   | 7                            | 0        | 2        | 9          | 0               | 0        | 1        | 1          | 0                     | 0        | 0        | 0          | 2               | 0        | 1         | 3          | 13         |
| <b>Total Volume</b>  | <b>25</b>                    | <b>0</b> | <b>5</b> | <b>30</b>  | <b>0</b>        | <b>0</b> | <b>1</b> | <b>1</b>   | <b>0</b>              | <b>0</b> | <b>3</b> | <b>3</b>   | <b>4</b>        | <b>0</b> | <b>12</b> | <b>16</b>  | <b>50</b>  |
| % App. Total   | 83.3                         | 0        | 16.7     |            | 0               | 0        | 100      |            | 0                     | 0        | 100      |            | 25              | 0        | 75        |            |            |
| PHF  | .694                         | .000     | .625     | .750       | .000            | .000     | .250     | .250       | .000                  | .000     | .250     | .250       | .500            | .000     | .600      | .571       | .735       |

# Peggy Malone & Associates, Inc.

## (888) 247-8602

File Name : TM 2-Cherry Ct and US 11 PM  
 Site Code :  
 Start Date : 2/4/2015  
 Page No : 1

Groups Printed- Truck

| Start Time  | Business Driveway Southbound |      |      |      |            | US 11 Westbound |      |      |      |            | Cherry Ct. Northbound |      |      |      |            | US 11 Eastbound |      |      |      |            | Int. Total |
|-------------|------------------------------|------|------|------|------------|-----------------|------|------|------|------------|-----------------------|------|------|------|------------|-----------------|------|------|------|------------|------------|
|             | Right                        | Thru | Left | Peds | App. Total | Right           | Thru | Left | Peds | App. Total | Right                 | Thru | Left | Peds | App. Total | Right           | Thru | Left | Peds | App. Total |            |
| 03:00 PM    | 0                            | 0    | 0    | 0    | 0          | 0               | 0    | 0    | 0    | 0          | 0                     | 0    | 0    | 0    | 0          | 0               | 0    | 0    | 0    | 0          | 0          |
| 03:15 PM    | 0                            | 0    | 0    | 0    | 0          | 0               | 0    | 0    | 0    | 0          | 0                     | 0    | 0    | 0    | 0          | 0               | 0    | 0    | 0    | 0          | 0          |
| 03:30 PM    | 0                            | 0    | 0    | 0    | 0          | 0               | 0    | 0    | 0    | 0          | 0                     | 0    | 0    | 0    | 0          | 0               | 0    | 0    | 0    | 0          | 0          |
| 03:45 PM    | 0                            | 0    | 0    | 0    | 0          | 0               | 0    | 0    | 0    | 0          | 0                     | 0    | 0    | 0    | 0          | 0               | 0    | 0    | 0    | 0          | 0          |
| Total       | 0                            | 0    | 0    | 0    | 0          | 0               | 0    | 0    | 0    | 0          | 0                     | 0    | 0    | 0    | 0          | 0               | 0    | 0    | 0    | 0          | 0          |
| 04:00 PM    | 0                            | 0    | 0    | 0    | 0          | 0               | 0    | 0    | 0    | 0          | 0                     | 0    | 0    | 0    | 0          | 0               | 0    | 0    | 0    | 0          | 0          |
| 04:15 PM    | 0                            | 0    | 0    | 0    | 0          | 0               | 0    | 0    | 0    | 0          | 0                     | 0    | 0    | 0    | 0          | 0               | 0    | 0    | 0    | 0          | 0          |
| 04:30 PM    | 0                            | 0    | 0    | 0    | 0          | 0               | 0    | 0    | 0    | 0          | 0                     | 0    | 0    | 0    | 0          | 0               | 0    | 0    | 0    | 0          | 0          |
| 04:45 PM    | 0                            | 0    | 0    | 0    | 0          | 0               | 0    | 0    | 0    | 0          | 0                     | 0    | 0    | 0    | 0          | 0               | 0    | 0    | 0    | 0          | 0          |
| Total       | 0                            | 0    | 0    | 0    | 0          | 0               | 0    | 0    | 0    | 0          | 0                     | 0    | 0    | 0    | 0          | 0               | 0    | 0    | 0    | 0          | 0          |
| 05:00 PM    | 0                            | 0    | 0    | 0    | 0          | 0               | 0    | 0    | 0    | 0          | 0                     | 0    | 0    | 0    | 0          | 0               | 0    | 0    | 0    | 0          | 0          |
| 05:15 PM    | 0                            | 0    | 0    | 0    | 0          | 0               | 0    | 0    | 0    | 0          | 0                     | 0    | 0    | 0    | 0          | 0               | 0    | 0    | 0    | 0          | 0          |
| 05:30 PM    | 0                            | 0    | 0    | 0    | 0          | 0               | 0    | 0    | 0    | 0          | 0                     | 0    | 0    | 0    | 0          | 0               | 0    | 0    | 0    | 0          | 0          |
| 05:45 PM    | 0                            | 0    | 0    | 0    | 0          | 0               | 0    | 0    | 0    | 0          | 0                     | 0    | 0    | 0    | 0          | 0               | 0    | 0    | 0    | 0          | 0          |
| Total       | 0                            | 0    | 0    | 0    | 0          | 0               | 0    | 0    | 0    | 0          | 0                     | 0    | 0    | 0    | 0          | 0               | 0    | 0    | 0    | 0          | 0          |
| Grand Total | 0                            | 0    | 0    | 0    | 0          | 0               | 0    | 0    | 0    | 0          | 0                     | 0    | 0    | 0    | 0          | 0               | 0    | 0    | 0    | 0          | 0          |
| Apprch %    | 0                            | 0    | 0    | 0    | 0          | 0               | 0    | 0    | 0    | 0          | 0                     | 0    | 0    | 0    | 0          | 0               | 0    | 0    | 0    | 0          | 0          |
| Total %     |                              |      |      |      |            |                 |      |      |      |            |                       |      |      |      |            |                 |      |      |      |            |            |

| Start Time   | Business Driveway Southbound |      |      |            | US 11 Westbound |      |      |            | Cherry Ct. Northbound |      |      |            | US 11 Eastbound |      |      |            | Int. Total |
|--|------------------------------|------|------|------------|-----------------|------|------|------------|-----------------------|------|------|------------|-----------------|------|------|------------|------------|
|  | Right                        | Thru | Left | App. Total | Right           | Thru | Left | App. Total | Right                 | Thru | Left | App. Total | Right           | Thru | Left | App. Total |            |
| Peak Hour Analysis From 03:00 PM to 05:45 PM - Peak 1 of 1 |                              |      |      |            |                 |      |      |            |                       |      |      |            |                 |      |      |            |            |
| Peak Hour for Entire Intersection Begins at 03:00 PM       |                              |      |      |            |                 |      |      |            |                       |      |      |            |                 |      |      |            |            |
| 03:00 PM   | 0                            | 0    | 0    | 0          | 0               | 0    | 0    | 0          | 0                     | 0    | 0    | 0          | 0               | 0    | 0    | 0          | 0          |
| 03:15 PM   | 0                            | 0    | 0    | 0          | 0               | 0    | 0    | 0          | 0                     | 0    | 0    | 0          | 0               | 0    | 0    | 0          | 0          |
| 03:30 PM   | 0                            | 0    | 0    | 0          | 0               | 0    | 0    | 0          | 0                     | 0    | 0    | 0          | 0               | 0    | 0    | 0          | 0          |
| 03:45 PM   | 0                            | 0    | 0    | 0          | 0               | 0    | 0    | 0          | 0                     | 0    | 0    | 0          | 0               | 0    | 0    | 0          | 0          |
| Total Volume   | 0                            | 0    | 0    | 0          | 0               | 0    | 0    | 0          | 0                     | 0    | 0    | 0          | 0               | 0    | 0    | 0          | 0          |
| % App. Total   | 0                            | 0    | 0    | 0          | 0               | 0    | 0    | 0          | 0                     | 0    | 0    | 0          | 0               | 0    | 0    | 0          | 0          |
| PHF  | .000                         | .000 | .000 | .000       | .000            | .000 | .000 | .000       | .000                  | .000 | .000 | .000       | .000            | .000 | .000 | .000       | .000       |

# Peggy Malone & Associates, Inc.

## (888) 247-8602

File Name : TM 2-Cherry Ct and US 11 PM  
 Site Code :  
 Start Date : 2/4/2015  
 Page No : 1

Groups Printed- Combined

| Start Time         | Business Driveway Southbound |          |           |          |            | US 11 Westbound |          |          |          |            | Cherry Ct. Northbound |          |          |          |            | US 11 Eastbound |          |           |          |            | Int. Total |
|--------------------|------------------------------|----------|-----------|----------|------------|-----------------|----------|----------|----------|------------|-----------------------|----------|----------|----------|------------|-----------------|----------|-----------|----------|------------|------------|
|                    | Right                        | Thru     | Left      | Peds     | App. Total | Right           | Thru     | Left     | Peds     | App. Total | Right                 | Thru     | Left     | Peds     | App. Total | Right           | Thru     | Left      | Peds     | App. Total |            |
| 03:00 PM           | 6                            | 0        | 2         | 0        | 8          | 0               | 0        | 1        | 0        | 1          | 0                     | 0        | 0        | 0        | 0          | 1               | 0        | 2         | 0        | 3          | 12         |
| 03:15 PM           | 4                            | 0        | 1         | 1        | 6          | 0               | 0        | 0        | 0        | 0          | 0                     | 0        | 0        | 0        | 0          | 0               | 0        | 2         | 0        | 2          | 8          |
| 03:30 PM           | 3                            | 0        | 1         | 0        | 4          | 0               | 0        | 0        | 0        | 0          | 1                     | 0        | 0        | 0        | 1          | 1               | 0        | 1         | 0        | 2          | 7          |
| 03:45 PM           | 3                            | 0        | 1         | 0        | 4          | 0               | 0        | 0        | 0        | 0          | 3                     | 0        | 0        | 0        | 3          | 0               | 0        | 5         | 0        | 5          | 12         |
| <b>Total</b>       | <b>16</b>                    | <b>0</b> | <b>5</b>  | <b>1</b> | <b>22</b>  | <b>0</b>        | <b>0</b> | <b>1</b> | <b>0</b> | <b>1</b>   | <b>4</b>              | <b>0</b> | <b>0</b> | <b>0</b> | <b>4</b>   | <b>2</b>        | <b>0</b> | <b>10</b> | <b>0</b> | <b>12</b>  | <b>39</b>  |
| 04:00 PM           | 4                            | 0        | 2         | 1        | 7          | 0               | 0        | 0        | 0        | 0          | 0                     | 0        | 0        | 0        | 0          | 2               | 0        | 5         | 0        | 7          | 14         |
| 04:15 PM           | 5                            | 0        | 0         | 0        | 5          | 0               | 0        | 0        | 0        | 0          | 0                     | 0        | 0        | 0        | 0          | 0               | 0        | 2         | 0        | 2          | 7          |
| 04:30 PM           | 9                            | 0        | 1         | 0        | 10         | 0               | 0        | 0        | 0        | 0          | 0                     | 0        | 3        | 0        | 3          | 0               | 0        | 4         | 0        | 4          | 17         |
| 04:45 PM           | 7                            | 0        | 2         | 0        | 9          | 0               | 0        | 1        | 0        | 1          | 0                     | 0        | 0        | 0        | 0          | 2               | 0        | 1         | 0        | 3          | 13         |
| <b>Total</b>       | <b>25</b>                    | <b>0</b> | <b>5</b>  | <b>1</b> | <b>31</b>  | <b>0</b>        | <b>0</b> | <b>1</b> | <b>0</b> | <b>1</b>   | <b>0</b>              | <b>0</b> | <b>3</b> | <b>0</b> | <b>3</b>   | <b>4</b>        | <b>0</b> | <b>12</b> | <b>0</b> | <b>16</b>  | <b>51</b>  |
| 05:00 PM           | 3                            | 0        | 0         | 1        | 4          | 0               | 0        | 0        | 0        | 0          | 0                     | 0        | 1        | 0        | 1          | 1               | 0        | 1         | 0        | 2          | 7          |
| 05:15 PM           | 4                            | 0        | 2         | 0        | 6          | 0               | 0        | 1        | 0        | 1          | 0                     | 0        | 0        | 1        | 1          | 1               | 0        | 0         | 0        | 1          | 9          |
| 05:30 PM           | 3                            | 0        | 2         | 0        | 5          | 0               | 0        | 0        | 0        | 0          | 0                     | 0        | 0        | 0        | 0          | 0               | 0        | 1         | 0        | 1          | 6          |
| 05:45 PM           | 2                            | 0        | 1         | 0        | 3          | 2               | 0        | 1        | 0        | 3          | 0                     | 0        | 0        | 0        | 0          | 0               | 0        | 2         | 0        | 2          | 8          |
| <b>Total</b>       | <b>12</b>                    | <b>0</b> | <b>5</b>  | <b>1</b> | <b>18</b>  | <b>2</b>        | <b>0</b> | <b>2</b> | <b>0</b> | <b>4</b>   | <b>0</b>              | <b>0</b> | <b>1</b> | <b>1</b> | <b>2</b>   | <b>2</b>        | <b>0</b> | <b>4</b>  | <b>0</b> | <b>6</b>   | <b>30</b>  |
| <b>Grand Total</b> | <b>53</b>                    | <b>0</b> | <b>15</b> | <b>3</b> | <b>71</b>  | <b>2</b>        | <b>0</b> | <b>4</b> | <b>0</b> | <b>6</b>   | <b>4</b>              | <b>0</b> | <b>4</b> | <b>1</b> | <b>9</b>   | <b>8</b>        | <b>0</b> | <b>26</b> | <b>0</b> | <b>34</b>  | <b>120</b> |
| Apprch %           | 74.6                         | 0        | 21.1      | 4.2      |            | 33.3            | 0        | 66.7     | 0        |            | 44.4                  | 0        | 44.4     | 11.1     |            | 23.5            | 0        | 76.5      | 0        |            |            |
| Total %            | 44.2                         | 0        | 12.5      | 2.5      | 59.2       | 1.7             | 0        | 3.3      | 0        | 5          | 3.3                   | 0        | 3.3      | 0.8      | 7.5        | 6.7             | 0        | 21.7      | 0        | 28.3       |            |

| Start Time   | Business Driveway Southbound |          |          |            | US 11 Westbound |          |          |            | Cherry Ct. Northbound |          |          |            | US 11 Eastbound |          |           |            | Int. Total |
|--|------------------------------|----------|----------|------------|-----------------|----------|----------|------------|-----------------------|----------|----------|------------|-----------------|----------|-----------|------------|------------|
|  | Right                        | Thru     | Left     | App. Total | Right           | Thru     | Left     | App. Total | Right                 | Thru     | Left     | App. Total | Right           | Thru     | Left      | App. Total |            |
| Peak Hour Analysis From 03:00 PM to 05:45 PM - Peak 1 of 1 |                              |          |          |            |                 |          |          |            |                       |          |          |            |                 |          |           |            |            |
| Peak Hour for Entire Intersection Begins at 04:00 PM       |                              |          |          |            |                 |          |          |            |                       |          |          |            |                 |          |           |            |            |
| 04:00 PM   | 4                            | 0        | 2        | 6          | 0               | 0        | 0        | 0          | 0                     | 0        | 0        | 0          | 2               | 0        | 5         | 7          | 13         |
| 04:15 PM   | 5                            | 0        | 0        | 5          | 0               | 0        | 0        | 0          | 0                     | 0        | 0        | 0          | 0               | 0        | 2         | 2          | 7          |
| 04:30 PM   | 9                            | 0        | 1        | 10         | 0               | 0        | 0        | 0          | 0                     | 0        | 3        | 3          | 0               | 0        | 4         | 4          | 17         |
| 04:45 PM   | 7                            | 0        | 2        | 9          | 0               | 0        | 1        | 1          | 0                     | 0        | 0        | 0          | 2               | 0        | 1         | 3          | 13         |
| <b>Total Volume</b>  | <b>25</b>                    | <b>0</b> | <b>5</b> | <b>30</b>  | <b>0</b>        | <b>0</b> | <b>1</b> | <b>1</b>   | <b>0</b>              | <b>0</b> | <b>3</b> | <b>3</b>   | <b>4</b>        | <b>0</b> | <b>12</b> | <b>16</b>  | <b>50</b>  |
| % App. Total   | 83.3                         | 0        | 16.7     |            | 0               | 0        | 100      |            | 0                     | 0        | 100      |            | 25              | 0        | 75        |            |            |
| PHF  | .694                         | .000     | .625     | .750       | .000            | .000     | .250     | .250       | .000                  | .000     | .250     | .250       | .500            | .000     | .600      | .571       | .735       |

# Peggy Malone & Associates, Inc.

## (888) 247-8602

File Name : TM 3-Bellspring and US 11 AM  
 Site Code :  
 Start Date : 2/4/2015  
 Page No : 1

Groups Printed- Car

| Start Time         | Bluespring Rd. Southbound |          |           |          |            | US 11 Westbound |            |          |          |             | Business Driveway Northbound |          |          |          |            | US 11 Eastbound |             |          |          |             | Int. Total  |
|--------------------|---------------------------|----------|-----------|----------|------------|-----------------|------------|----------|----------|-------------|------------------------------|----------|----------|----------|------------|-----------------|-------------|----------|----------|-------------|-------------|
|                    | Right                     | Thru     | Left      | Peds     | App. Total | Right           | Thru       | Left     | Peds     | App. Total  | Right                        | Thru     | Left     | Peds     | App. Total | Right           | Thru        | Left     | Peds     | App. Total  |             |
| 07:00 AM           | 1                         | 0        | 2         | 0        | 3          | 29              | 88         | 0        | 0        | 117         | 0                            | 0        | 0        | 0        | 0          | 0               | 129         | 0        | 0        | 129         | 249         |
| 07:15 AM           | 0                         | 0        | 1         | 0        | 1          | 27              | 117        | 0        | 0        | 144         | 0                            | 0        | 0        | 0        | 0          | 0               | 190         | 0        | 0        | 190         | 335         |
| 07:30 AM           | 1                         | 0        | 3         | 0        | 4          | 49              | 118        | 0        | 0        | 167         | 0                            | 0        | 0        | 0        | 0          | 0               | 239         | 0        | 0        | 239         | 410         |
| 07:45 AM           | 1                         | 0        | 4         | 0        | 5          | 40              | 123        | 0        | 0        | 163         | 0                            | 0        | 0        | 0        | 0          | 0               | 242         | 1        | 0        | 243         | 411         |
| <b>Total</b>       | <b>3</b>                  | <b>0</b> | <b>10</b> | <b>0</b> | <b>13</b>  | <b>145</b>      | <b>446</b> | <b>0</b> | <b>0</b> | <b>591</b>  | <b>0</b>                     | <b>0</b> | <b>0</b> | <b>0</b> | <b>0</b>   | <b>0</b>        | <b>800</b>  | <b>1</b> | <b>0</b> | <b>801</b>  | <b>1405</b> |
| 08:00 AM           | 0                         | 0        | 2         | 0        | 2          | 43              | 101        | 0        | 0        | 144         | 0                            | 0        | 0        | 0        | 0          | 0               | 193         | 0        | 0        | 193         | 339         |
| 08:15 AM           | 1                         | 0        | 3         | 0        | 4          | 51              | 105        | 0        | 0        | 156         | 0                            | 0        | 0        | 0        | 0          | 0               | 194         | 1        | 0        | 195         | 355         |
| 08:30 AM           | 1                         | 0        | 2         | 0        | 3          | 52              | 116        | 0        | 0        | 168         | 0                            | 0        | 0        | 0        | 0          | 0               | 167         | 0        | 0        | 167         | 338         |
| 08:45 AM           | 0                         | 0        | 1         | 0        | 1          | 33              | 120        | 0        | 0        | 153         | 0                            | 0        | 0        | 0        | 0          | 0               | 185         | 0        | 0        | 185         | 339         |
| <b>Total</b>       | <b>2</b>                  | <b>0</b> | <b>8</b>  | <b>0</b> | <b>10</b>  | <b>179</b>      | <b>442</b> | <b>0</b> | <b>0</b> | <b>621</b>  | <b>0</b>                     | <b>0</b> | <b>0</b> | <b>0</b> | <b>0</b>   | <b>0</b>        | <b>739</b>  | <b>1</b> | <b>0</b> | <b>740</b>  | <b>1371</b> |
| <b>Grand Total</b> | <b>5</b>                  | <b>0</b> | <b>18</b> | <b>0</b> | <b>23</b>  | <b>324</b>      | <b>888</b> | <b>0</b> | <b>0</b> | <b>1212</b> | <b>0</b>                     | <b>0</b> | <b>0</b> | <b>0</b> | <b>0</b>   | <b>0</b>        | <b>1539</b> | <b>2</b> | <b>0</b> | <b>1541</b> | <b>2776</b> |
| Apprch %           | 21.7                      | 0        | 78.3      | 0        |            | 26.7            | 73.3       | 0        | 0        |             | 0                            | 0        | 0        | 0        |            | 0               | 99.9        | 0.1      | 0        |             |             |
| Total %            | 0.2                       | 0        | 0.6       | 0        | 0.8        | 11.7            | 32         | 0        | 0        | 43.7        | 0                            | 0        | 0        | 0        | 0          | 0               | 55.4        | 0.1      | 0        | 55.5        |             |

| Start Time   | Bluespring Rd. Southbound |          |           |            | US 11 Westbound |            |          |            | Business Driveway Northbound |          |          |            | US 11 Eastbound |            |          |            | Int. Total  |
|--|---------------------------|----------|-----------|------------|-----------------|------------|----------|------------|------------------------------|----------|----------|------------|-----------------|------------|----------|------------|-------------|
|  | Right                     | Thru     | Left      | App. Total | Right           | Thru       | Left     | App. Total | Right                        | Thru     | Left     | App. Total | Right           | Thru       | Left     | App. Total |             |
| Peak Hour Analysis From 07:00 AM to 08:45 AM - Peak 1 of 1 |                           |          |           |            |                 |            |          |            |                              |          |          |            |                 |            |          |            |             |
| Peak Hour for Entire Intersection Begins at 07:30 AM       |                           |          |           |            |                 |            |          |            |                              |          |          |            |                 |            |          |            |             |
| 07:30 AM   | <b>1</b>                  | 0        | 3         | 4          | 49              | 118        | 0        | <b>167</b> | 0                            | 0        | 0        | 0          | 0               | 239        | 0        | 239        | 410         |
| 07:45 AM   | 1                         | 0        | <b>4</b>  | <b>5</b>   | 40              | <b>123</b> | 0        | 163        | 0                            | 0        | 0        | 0          | 0               | <b>242</b> | <b>1</b> | <b>243</b> | <b>411</b>  |
| 08:00 AM   | 0                         | 0        | 2         | 2          | 43              | 101        | 0        | 144        | 0                            | 0        | 0        | 0          | 0               | 193        | 0        | 193        | 339         |
| 08:15 AM   | 1                         | 0        | 3         | 4          | <b>51</b>       | 105        | 0        | 156        | 0                            | 0        | 0        | 0          | 0               | 194        | 1        | 195        | 355         |
| <b>Total Volume</b>  | <b>3</b>                  | <b>0</b> | <b>12</b> | <b>15</b>  | <b>183</b>      | <b>447</b> | <b>0</b> | <b>630</b> | <b>0</b>                     | <b>0</b> | <b>0</b> | <b>0</b>   | <b>0</b>        | <b>868</b> | <b>2</b> | <b>870</b> | <b>1515</b> |
| % App. Total   | 20                        | 0        | 80        |            | 29              | 71         | 0        |            | 0                            | 0        | 0        |            | 0               | 99.8       | 0.2      |            |             |
| PHF  | .750                      | .000     | .750      | .750       | .897            | .909       | .000     | .943       | .000                         | .000     | .000     | .000       | .000            | .897       | .500     | .895       | .922        |

# Peggy Malone & Associates, Inc.

## (888) 247-8602

File Name : TM 3-Bellspring and US 11 AM  
 Site Code :  
 Start Date : 2/4/2015  
 Page No : 1

Groups Printed- Truck

| Start Time         | Bluespring Rd. Southbound |      |      |      |            | US 11 Westbound |      |      |      |            | Business Driveway Northbound |      |      |      |            | US 11 Eastbound |      |      |      |            | Int. Total |
|--------------------|---------------------------|------|------|------|------------|-----------------|------|------|------|------------|------------------------------|------|------|------|------------|-----------------|------|------|------|------------|------------|
|                    | Right                     | Thru | Left | Peds | App. Total | Right           | Thru | Left | Peds | App. Total | Right                        | Thru | Left | Peds | App. Total | Right           | Thru | Left | Peds | App. Total |            |
| 07:00 AM           | 0                         | 0    | 0    | 0    | 0          | 0               | 0    | 0    | 0    | 0          | 0                            | 0    | 0    | 0    | 0          | 0               | 1    | 0    | 0    | 0          | 1          |
| 07:15 AM           | 0                         | 0    | 0    | 0    | 0          | 1               | 1    | 0    | 0    | 2          | 0                            | 0    | 0    | 0    | 0          | 0               | 3    | 0    | 0    | 0          | 3          |
| 07:30 AM           | 0                         | 0    | 0    | 0    | 0          | 0               | 1    | 0    | 0    | 1          | 0                            | 0    | 0    | 0    | 0          | 0               | 3    | 0    | 0    | 0          | 3          |
| 07:45 AM           | 0                         | 0    | 0    | 0    | 0          | 2               | 5    | 0    | 0    | 7          | 0                            | 0    | 0    | 0    | 0          | 0               | 4    | 0    | 0    | 0          | 4          |
| <b>Total</b>       | 0                         | 0    | 0    | 0    | 0          | 3               | 7    | 0    | 0    | 10         | 0                            | 0    | 0    | 0    | 0          | 0               | 11   | 0    | 0    | 0          | 11         |
| 08:00 AM           | 0                         | 0    | 0    | 0    | 0          | 2               | 4    | 0    | 0    | 6          | 0                            | 0    | 0    | 0    | 0          | 0               | 2    | 0    | 0    | 0          | 2          |
| 08:15 AM           | 0                         | 0    | 0    | 0    | 0          | 0               | 3    | 0    | 0    | 3          | 0                            | 0    | 0    | 0    | 0          | 0               | 6    | 0    | 0    | 0          | 6          |
| 08:30 AM           | 0                         | 0    | 0    | 0    | 0          | 0               | 1    | 0    | 0    | 1          | 0                            | 0    | 0    | 0    | 0          | 0               | 4    | 0    | 0    | 0          | 4          |
| 08:45 AM           | 0                         | 0    | 0    | 0    | 0          | 1               | 5    | 0    | 0    | 6          | 0                            | 0    | 0    | 0    | 0          | 0               | 8    | 0    | 0    | 0          | 8          |
| <b>Total</b>       | 0                         | 0    | 0    | 0    | 0          | 3               | 13   | 0    | 0    | 16         | 0                            | 0    | 0    | 0    | 0          | 0               | 20   | 0    | 0    | 0          | 20         |
| <b>Grand Total</b> | 0                         | 0    | 0    | 0    | 0          | 6               | 20   | 0    | 0    | 26         | 0                            | 0    | 0    | 0    | 0          | 0               | 31   | 0    | 0    | 0          | 31         |
| Apprch %           | 0                         | 0    | 0    | 0    | 0          | 23.1            | 76.9 | 0    | 0    |            | 0                            | 0    | 0    | 0    | 0          | 0               | 100  | 0    | 0    | 0          |            |
| Total %            | 0                         | 0    | 0    | 0    | 0          | 10.5            | 35.1 | 0    | 0    | 45.6       | 0                            | 0    | 0    | 0    | 0          | 0               | 54.4 | 0    | 0    | 0          | 54.4       |

| Start Time   | Bluespring Rd. Southbound |      |      |            | US 11 Westbound |      |      |            | Business Driveway Northbound |      |      |            | US 11 Eastbound |      |      |            | Int. Total |
|--|---------------------------|------|------|------------|-----------------|------|------|------------|------------------------------|------|------|------------|-----------------|------|------|------------|------------|
|  | Right                     | Thru | Left | App. Total | Right           | Thru | Left | App. Total | Right                        | Thru | Left | App. Total | Right           | Thru | Left | App. Total |            |
| Peak Hour Analysis From 07:00 AM to 08:45 AM - Peak 1 of 1 |                           |      |      |            |                 |      |      |            |                              |      |      |            |                 |      |      |            |            |
| Peak Hour for Entire Intersection Begins at 08:00 AM       |                           |      |      |            |                 |      |      |            |                              |      |      |            |                 |      |      |            |            |
| 08:00 AM   | 0                         | 0    | 0    | 0          | 2               | 4    | 0    | 6          | 0                            | 0    | 0    | 0          | 0               | 2    | 0    | 2          | 8          |
| 08:15 AM   | 0                         | 0    | 0    | 0          | 0               | 3    | 0    | 3          | 0                            | 0    | 0    | 0          | 0               | 6    | 0    | 6          | 9          |
| 08:30 AM   | 0                         | 0    | 0    | 0          | 0               | 1    | 0    | 1          | 0                            | 0    | 0    | 0          | 0               | 4    | 0    | 4          | 5          |
| 08:45 AM   | 0                         | 0    | 0    | 0          | 1               | 5    | 0    | 6          | 0                            | 0    | 0    | 0          | 0               | 8    | 0    | 8          | 14         |
| <b>Total Volume</b>  | 0                         | 0    | 0    | 0          | 3               | 13   | 0    | 16         | 0                            | 0    | 0    | 0          | 0               | 20   | 0    | 20         | 36         |
| % App. Total   | 0                         | 0    | 0    | 0          | 18.8            | 81.2 | 0    |            | 0                            | 0    | 0    | 0          | 0               | 100  | 0    |            |            |
| PHF  | .000                      | .000 | .000 | .000       | .375            | .650 | .000 | .667       | .000                         | .000 | .000 | .000       | .000            | .625 | .000 | .625       | .643       |

# Peggy Malone & Associates, Inc.

## (888) 247-8602

File Name : TM 3-Bellspring and US 11 AM  
 Site Code :  
 Start Date : 2/4/2015  
 Page No : 1

Groups Printed- Combined

| Start Time         | Bluespring Rd. Southbound |      |      |      |            | US 11 Westbound |      |      |      |            | Business Driveway Northbound |      |      |      |            | US 11 Eastbound |      |      |      |            | Int. Total |
|--------------------|---------------------------|------|------|------|------------|-----------------|------|------|------|------------|------------------------------|------|------|------|------------|-----------------|------|------|------|------------|------------|
|                    | Right                     | Thru | Left | Peds | App. Total | Right           | Thru | Left | Peds | App. Total | Right                        | Thru | Left | Peds | App. Total | Right           | Thru | Left | Peds | App. Total |            |
| 07:00 AM           | 1                         | 0    | 2    | 0    | 3          | 29              | 88   | 0    | 0    | 117        | 0                            | 0    | 0    | 0    | 0          | 0               | 130  | 0    | 0    | 130        | 250        |
| 07:15 AM           | 0                         | 0    | 1    | 0    | 1          | 28              | 118  | 0    | 0    | 146        | 0                            | 0    | 0    | 0    | 0          | 0               | 193  | 0    | 0    | 193        | 340        |
| 07:30 AM           | 1                         | 0    | 3    | 0    | 4          | 49              | 119  | 0    | 0    | 168        | 0                            | 0    | 0    | 0    | 0          | 0               | 242  | 0    | 0    | 242        | 414        |
| 07:45 AM           | 1                         | 0    | 4    | 0    | 5          | 42              | 128  | 0    | 0    | 170        | 0                            | 0    | 0    | 0    | 0          | 0               | 246  | 1    | 0    | 247        | 422        |
| <b>Total</b>       | 3                         | 0    | 10   | 0    | 13         | 148             | 453  | 0    | 0    | 601        | 0                            | 0    | 0    | 0    | 0          | 0               | 811  | 1    | 0    | 812        | 1426       |
| 08:00 AM           | 0                         | 0    | 2    | 0    | 2          | 45              | 105  | 0    | 0    | 150        | 0                            | 0    | 0    | 0    | 0          | 0               | 195  | 0    | 0    | 195        | 347        |
| 08:15 AM           | 1                         | 0    | 3    | 0    | 4          | 51              | 108  | 0    | 0    | 159        | 0                            | 0    | 0    | 0    | 0          | 0               | 200  | 1    | 0    | 201        | 364        |
| 08:30 AM           | 1                         | 0    | 2    | 0    | 3          | 52              | 117  | 0    | 0    | 169        | 0                            | 0    | 0    | 0    | 0          | 0               | 171  | 0    | 0    | 171        | 343        |
| 08:45 AM           | 0                         | 0    | 1    | 0    | 1          | 34              | 125  | 0    | 0    | 159        | 0                            | 0    | 0    | 0    | 0          | 0               | 193  | 0    | 0    | 193        | 353        |
| <b>Total</b>       | 2                         | 0    | 8    | 0    | 10         | 182             | 455  | 0    | 0    | 637        | 0                            | 0    | 0    | 0    | 0          | 0               | 759  | 1    | 0    | 760        | 1407       |
| <b>Grand Total</b> | 5                         | 0    | 18   | 0    | 23         | 330             | 908  | 0    | 0    | 1238       | 0                            | 0    | 0    | 0    | 0          | 0               | 1570 | 2    | 0    | 1572       | 2833       |
| Apprch %           | 21.7                      | 0    | 78.3 | 0    |            | 26.7            | 73.3 | 0    | 0    |            | 0                            | 0    | 0    | 0    |            | 0               | 99.9 | 0.1  | 0    |            |            |
| Total %            | 0.2                       | 0    | 0.6  | 0    | 0.8        | 11.6            | 32.1 | 0    | 0    | 43.7       | 0                            | 0    | 0    | 0    | 0          | 0               | 55.4 | 0.1  | 0    | 55.5       |            |

| Start Time   | Bluespring Rd. Southbound |      |      |            | US 11 Westbound |      |      |            | Business Driveway Northbound |      |      |            | US 11 Eastbound |      |      |            | Int. Total |
|--|---------------------------|------|------|------------|-----------------|------|------|------------|------------------------------|------|------|------------|-----------------|------|------|------------|------------|
|  | Right                     | Thru | Left | App. Total | Right           | Thru | Left | App. Total | Right                        | Thru | Left | App. Total | Right           | Thru | Left | App. Total |            |
| Peak Hour Analysis From 07:00 AM to 08:45 AM - Peak 1 of 1 |                           |      |      |            |                 |      |      |            |                              |      |      |            |                 |      |      |            |            |
| Peak Hour for Entire Intersection Begins at 07:30 AM       |                           |      |      |            |                 |      |      |            |                              |      |      |            |                 |      |      |            |            |
| 07:30 AM   | 1                         | 0    | 3    | 4          | 49              | 119  | 0    | 168        | 0                            | 0    | 0    | 0          | 0               | 242  | 0    | 242        | 414        |
| 07:45 AM   | 1                         | 0    | 4    | 5          | 42              | 128  | 0    | 170        | 0                            | 0    | 0    | 0          | 0               | 246  | 1    | 247        | 422        |
| 08:00 AM   | 0                         | 0    | 2    | 2          | 45              | 105  | 0    | 150        | 0                            | 0    | 0    | 0          | 0               | 195  | 0    | 195        | 347        |
| 08:15 AM   | 1                         | 0    | 3    | 4          | 51              | 108  | 0    | 159        | 0                            | 0    | 0    | 0          | 0               | 200  | 1    | 201        | 364        |
| <b>Total Volume</b>  | 3                         | 0    | 12   | 15         | 187             | 460  | 0    | 647        | 0                            | 0    | 0    | 0          | 0               | 883  | 2    | 885        | 1547       |
| % App. Total   | 20                        | 0    | 80   |            | 28.9            | 71.1 | 0    |            | 0                            | 0    | 0    |            | 0               | 99.8 | 0.2  |            |            |
| PHF  | .750                      | .000 | .750 | .750       | .917            | .898 | .000 | .951       | .000                         | .000 | .000 | .000       | .000            | .897 | .500 | .896       | .916       |

# Peggy Malone & Associates, Inc.

## (888) 247-8602

File Name : TM 3-Bellspring and US 11 MID  
 Site Code :  
 Start Date : 2/4/2015  
 Page No : 1

Groups Printed- Car

| Start Time         | Bluespring Rd. Southbound |          |           |          |            | US 11 Westbound |             |          |          |             | Business Driveway Northbound |          |          |          |            | US 11 Eastbound |             |          |          |             | Int. Total  |
|--------------------|---------------------------|----------|-----------|----------|------------|-----------------|-------------|----------|----------|-------------|------------------------------|----------|----------|----------|------------|-----------------|-------------|----------|----------|-------------|-------------|
|                    | Right                     | Thru     | Left      | Peds     | App. Total | Right           | Thru        | Left     | Peds     | App. Total  | Right                        | Thru     | Left     | Peds     | App. Total | Right           | Thru        | Left     | Peds     | App. Total  |             |
| 11:00 AM           | 1                         | 0        | 2         | 0        | 3          | 41              | 159         | 0        | 0        | 200         | 0                            | 0        | 0        | 0        | 0          | 0               | 169         | 1        | 0        | 170         | 373         |
| 11:15 AM           | 0                         | 0        | 4         | 0        | 4          | 53              | 171         | 1        | 0        | 225         | 1                            | 0        | 0        | 0        | 1          | 0               | 200         | 0        | 0        | 200         | 430         |
| 11:30 AM           | 1                         | 0        | 6         | 0        | 7          | 46              | 165         | 0        | 0        | 211         | 1                            | 0        | 0        | 0        | 1          | 0               | 192         | 1        | 0        | 193         | 412         |
| 11:45 AM           | 0                         | 0        | 2         | 0        | 2          | 36              | 204         | 0        | 0        | 240         | 0                            | 0        | 0        | 1        | 1          | 0               | 178         | 0        | 0        | 178         | 421         |
| <b>Total</b>       | <b>2</b>                  | <b>0</b> | <b>14</b> | <b>0</b> | <b>16</b>  | <b>176</b>      | <b>699</b>  | <b>1</b> | <b>0</b> | <b>876</b>  | <b>2</b>                     | <b>0</b> | <b>0</b> | <b>1</b> | <b>3</b>   | <b>0</b>        | <b>739</b>  | <b>2</b> | <b>0</b> | <b>741</b>  | <b>1636</b> |
| 12:00 PM           | 0                         | 0        | 2         | 0        | 2          | 48              | 202         | 1        | 0        | 251         | 1                            | 0        | 0        | 0        | 1          | 0               | 234         | 1        | 0        | 235         | 489         |
| 12:15 PM           | 0                         | 0        | 6         | 0        | 6          | 50              | 213         | 0        | 0        | 263         | 1                            | 0        | 0        | 0        | 1          | 0               | 242         | 3        | 0        | 245         | 515         |
| 12:30 PM           | 0                         | 0        | 2         | 0        | 2          | 38              | 166         | 0        | 0        | 204         | 2                            | 0        | 0        | 0        | 2          | 0               | 260         | 2        | 0        | 262         | 470         |
| 12:45 PM           | 1                         | 0        | 6         | 0        | 7          | 41              | 204         | 0        | 0        | 245         | 1                            | 0        | 0        | 0        | 1          | 1               | 222         | 1        | 0        | 224         | 477         |
| <b>Total</b>       | <b>1</b>                  | <b>0</b> | <b>16</b> | <b>0</b> | <b>17</b>  | <b>177</b>      | <b>785</b>  | <b>1</b> | <b>0</b> | <b>963</b>  | <b>5</b>                     | <b>0</b> | <b>0</b> | <b>0</b> | <b>5</b>   | <b>1</b>        | <b>958</b>  | <b>7</b> | <b>0</b> | <b>966</b>  | <b>1951</b> |
| <b>Grand Total</b> | <b>3</b>                  | <b>0</b> | <b>30</b> | <b>0</b> | <b>33</b>  | <b>353</b>      | <b>1484</b> | <b>2</b> | <b>0</b> | <b>1839</b> | <b>7</b>                     | <b>0</b> | <b>0</b> | <b>1</b> | <b>8</b>   | <b>1</b>        | <b>1697</b> | <b>9</b> | <b>0</b> | <b>1707</b> | <b>3587</b> |
| Apprch %           | 9.1                       | 0        | 90.9      | 0        |            | 19.2            | 80.7        | 0.1      | 0        |             | 87.5                         | 0        | 0        | 12.5     |            | 0.1             | 99.4        | 0.5      | 0        |             |             |
| Total %            | 0.1                       | 0        | 0.8       | 0        | 0.9        | 9.8             | 41.4        | 0.1      | 0        | 51.3        | 0.2                          | 0        | 0        | 0        | 0.2        | 0               | 47.3        | 0.3      | 0        | 47.6        |             |

| Start Time   | Bluespring Rd. Southbound |          |           |            | US 11 Westbound |            |          |            | Business Driveway Northbound |          |          |            | US 11 Eastbound |            |          |            | Int. Total  |
|--|---------------------------|----------|-----------|------------|-----------------|------------|----------|------------|------------------------------|----------|----------|------------|-----------------|------------|----------|------------|-------------|
|  | Right                     | Thru     | Left      | App. Total | Right           | Thru       | Left     | App. Total | Right                        | Thru     | Left     | App. Total | Right           | Thru       | Left     | App. Total |             |
| Peak Hour Analysis From 11:00 AM to 12:45 PM - Peak 1 of 1 |                           |          |           |            |                 |            |          |            |                              |          |          |            |                 |            |          |            |             |
| Peak Hour for Entire Intersection Begins at 12:00 PM       |                           |          |           |            |                 |            |          |            |                              |          |          |            |                 |            |          |            |             |
| 12:00 PM   | 0                         | 0        | 2         | 2          | 48              | 202        | 1        | 251        | 1                            | 0        | 0        | 1          | 0               | 234        | 1        | 235        | 489         |
| 12:15 PM   | 0                         | 0        | 6         | 6          | 50              | 213        | 0        | 263        | 1                            | 0        | 0        | 1          | 0               | 242        | 3        | 245        | 515         |
| 12:30 PM   | 0                         | 0        | 2         | 2          | 38              | 166        | 0        | 204        | 2                            | 0        | 0        | 2          | 0               | 260        | 2        | 262        | 470         |
| 12:45 PM   | 1                         | 0        | 6         | 7          | 41              | 204        | 0        | 245        | 1                            | 0        | 0        | 1          | 1               | 222        | 1        | 224        | 477         |
| <b>Total Volume</b>  | <b>1</b>                  | <b>0</b> | <b>16</b> | <b>17</b>  | <b>177</b>      | <b>785</b> | <b>1</b> | <b>963</b> | <b>5</b>                     | <b>0</b> | <b>0</b> | <b>5</b>   | <b>1</b>        | <b>958</b> | <b>7</b> | <b>966</b> | <b>1951</b> |
| % App. Total   | 5.9                       | 0        | 94.1      |            | 18.4            | 81.5       | 0.1      |            | 100                          | 0        | 0        |            | 0.1             | 99.2       | 0.7      |            |             |
| PHF  | .250                      | .000     | .667      | .607       | .885            | .921       | .250     | .915       | .625                         | .000     | .000     | .625       | .250            | .921       | .583     | .922       | .947        |

# Peggy Malone & Associates, Inc.

## (888) 247-8602

File Name : TM 3-Bellspring and US 11 MID  
 Site Code :  
 Start Date : 2/4/2015  
 Page No : 1

Groups Printed- Truck

| Start Time         | Bluespring Rd. Southbound |      |      |      |            | US 11 Westbound |      |      |      |            | Business Driveway Northbound |      |      |      |            | US 11 Eastbound |      |      |      |            | Int. Total |
|--------------------|---------------------------|------|------|------|------------|-----------------|------|------|------|------------|------------------------------|------|------|------|------------|-----------------|------|------|------|------------|------------|
|                    | Right                     | Thru | Left | Peds | App. Total | Right           | Thru | Left | Peds | App. Total | Right                        | Thru | Left | Peds | App. Total | Right           | Thru | Left | Peds | App. Total |            |
| 11:00 AM           | 0                         | 0    | 0    | 0    | 0          | 0               | 7    | 0    | 0    | 7          | 0                            | 0    | 0    | 0    | 0          | 0               | 6    | 0    | 0    | 6          | 13         |
| 11:15 AM           | 0                         | 0    | 0    | 0    | 0          | 1               | 4    | 0    | 0    | 5          | 0                            | 0    | 0    | 0    | 0          | 0               | 4    | 0    | 0    | 4          | 9          |
| 11:30 AM           | 0                         | 0    | 0    | 0    | 0          | 2               | 7    | 0    | 0    | 9          | 0                            | 0    | 0    | 0    | 0          | 0               | 5    | 0    | 0    | 5          | 14         |
| 11:45 AM           | 0                         | 0    | 0    | 0    | 0          | 0               | 3    | 0    | 0    | 3          | 0                            | 0    | 0    | 0    | 0          | 0               | 8    | 0    | 0    | 8          | 11         |
| <b>Total</b>       | 0                         | 0    | 0    | 0    | 0          | 3               | 21   | 0    | 0    | 24         | 0                            | 0    | 0    | 0    | 0          | 0               | 23   | 0    | 0    | 23         | 47         |
| 12:00 PM           | 0                         | 0    | 0    | 0    | 0          | 0               | 6    | 0    | 0    | 6          | 0                            | 0    | 0    | 0    | 0          | 0               | 5    | 0    | 0    | 5          | 11         |
| 12:15 PM           | 0                         | 0    | 0    | 0    | 0          | 0               | 2    | 0    | 0    | 2          | 0                            | 0    | 0    | 0    | 0          | 0               | 5    | 0    | 0    | 5          | 7          |
| 12:30 PM           | 0                         | 0    | 1    | 0    | 1          | 0               | 4    | 0    | 0    | 4          | 0                            | 0    | 0    | 0    | 0          | 0               | 4    | 0    | 0    | 4          | 9          |
| 12:45 PM           | 0                         | 0    | 0    | 0    | 0          | 1               | 6    | 0    | 0    | 7          | 0                            | 0    | 0    | 0    | 0          | 0               | 7    | 0    | 0    | 7          | 14         |
| <b>Total</b>       | 0                         | 0    | 1    | 0    | 1          | 1               | 18   | 0    | 0    | 19         | 0                            | 0    | 0    | 0    | 0          | 0               | 21   | 0    | 0    | 21         | 41         |
| <b>Grand Total</b> | 0                         | 0    | 1    | 0    | 1          | 4               | 39   | 0    | 0    | 43         | 0                            | 0    | 0    | 0    | 0          | 0               | 44   | 0    | 0    | 44         | 88         |
| Apprch %           | 0                         | 0    | 100  | 0    |            | 9.3             | 90.7 | 0    | 0    |            | 0                            | 0    | 0    | 0    |            | 0               | 100  | 0    | 0    |            |            |
| Total %            | 0                         | 0    | 1.1  | 0    | 1.1        | 4.5             | 44.3 | 0    | 0    | 48.9       | 0                            | 0    | 0    | 0    | 0          | 0               | 50   | 0    | 0    | 50         |            |

| Start Time   | Bluespring Rd. Southbound |      |      |            | US 11 Westbound |      |      |            | Business Driveway Northbound |      |      |            | US 11 Eastbound |      |      |            | Int. Total |
|--|---------------------------|------|------|------------|-----------------|------|------|------------|------------------------------|------|------|------------|-----------------|------|------|------------|------------|
|  | Right                     | Thru | Left | App. Total | Right           | Thru | Left | App. Total | Right                        | Thru | Left | App. Total | Right           | Thru | Left | App. Total |            |
| Peak Hour Analysis From 11:00 AM to 12:45 PM - Peak 1 of 1 |                           |      |      |            |                 |      |      |            |                              |      |      |            |                 |      |      |            |            |
| Peak Hour for Entire Intersection Begins at 11:00 AM       |                           |      |      |            |                 |      |      |            |                              |      |      |            |                 |      |      |            |            |
| 11:00 AM   | 0                         | 0    | 0    | 0          | 0               | 7    | 0    | 7          | 0                            | 0    | 0    | 0          | 0               | 6    | 0    | 6          | 13         |
| 11:15 AM   | 0                         | 0    | 0    | 0          | 1               | 4    | 0    | 5          | 0                            | 0    | 0    | 0          | 0               | 4    | 0    | 4          | 9          |
| 11:30 AM   | 0                         | 0    | 0    | 0          | 2               | 7    | 0    | 9          | 0                            | 0    | 0    | 0          | 0               | 5    | 0    | 5          | 14         |
| 11:45 AM   | 0                         | 0    | 0    | 0          | 0               | 3    | 0    | 3          | 0                            | 0    | 0    | 0          | 0               | 8    | 0    | 8          | 11         |
| <b>Total Volume</b>  | 0                         | 0    | 0    | 0          | 3               | 21   | 0    | 24         | 0                            | 0    | 0    | 0          | 0               | 23   | 0    | 23         | 47         |
| <b>% App. Total</b>  | 0                         | 0    | 0    |            | 12.5            | 87.5 | 0    |            | 0                            | 0    | 0    |            | 0               | 100  | 0    |            |            |
| PHF  | .000                      | .000 | .000 | .000       | .375            | .750 | .000 | .667       | .000                         | .000 | .000 | .000       | .000            | .719 | .000 | .719       | .839       |



# Peggy Malone & Associates, Inc.

## (888) 247-8602

File Name : TM 3-Bellspring and US 11 MID  
 Site Code :  
 Start Date : 2/4/2015  
 Page No : 1

Groups Printed- Combined

| Start Time         | Bluespring Rd. Southbound |          |           |          |            | US 11 Westbound |             |          |          |             | Business Driveway Northbound |          |          |          |            | US 11 Eastbound |             |          |          |             | Int. Total  |
|--------------------|---------------------------|----------|-----------|----------|------------|-----------------|-------------|----------|----------|-------------|------------------------------|----------|----------|----------|------------|-----------------|-------------|----------|----------|-------------|-------------|
|                    | Right                     | Thru     | Left      | Peds     | App. Total | Right           | Thru        | Left     | Peds     | App. Total  | Right                        | Thru     | Left     | Peds     | App. Total | Right           | Thru        | Left     | Peds     | App. Total  |             |
| 11:00 AM           | 1                         | 0        | 2         | 0        | 3          | 41              | 166         | 0        | 0        | 207         | 0                            | 0        | 0        | 0        | 0          | 0               | 175         | 1        | 0        | 176         | 386         |
| 11:15 AM           | 0                         | 0        | 4         | 0        | 4          | 54              | 175         | 1        | 0        | 230         | 1                            | 0        | 0        | 0        | 1          | 0               | 204         | 0        | 0        | 204         | 439         |
| 11:30 AM           | 1                         | 0        | 6         | 0        | 7          | 48              | 172         | 0        | 0        | 220         | 1                            | 0        | 0        | 0        | 1          | 0               | 197         | 1        | 0        | 198         | 426         |
| 11:45 AM           | 0                         | 0        | 2         | 0        | 2          | 36              | 207         | 0        | 0        | 243         | 0                            | 0        | 0        | 1        | 1          | 0               | 186         | 0        | 0        | 186         | 432         |
| <b>Total</b>       | <b>2</b>                  | <b>0</b> | <b>14</b> | <b>0</b> | <b>16</b>  | <b>179</b>      | <b>720</b>  | <b>1</b> | <b>0</b> | <b>900</b>  | <b>2</b>                     | <b>0</b> | <b>0</b> | <b>1</b> | <b>3</b>   | <b>0</b>        | <b>762</b>  | <b>2</b> | <b>0</b> | <b>764</b>  | <b>1683</b> |
| 12:00 PM           | 0                         | 0        | 2         | 0        | 2          | 48              | 208         | 1        | 0        | 257         | 1                            | 0        | 0        | 0        | 1          | 0               | 239         | 1        | 0        | 240         | 500         |
| 12:15 PM           | 0                         | 0        | 6         | 0        | 6          | 50              | 215         | 0        | 0        | 265         | 1                            | 0        | 0        | 0        | 1          | 0               | 247         | 3        | 0        | 250         | 522         |
| 12:30 PM           | 0                         | 0        | 3         | 0        | 3          | 38              | 170         | 0        | 0        | 208         | 2                            | 0        | 0        | 0        | 2          | 0               | 264         | 2        | 0        | 266         | 479         |
| 12:45 PM           | 1                         | 0        | 6         | 0        | 7          | 42              | 210         | 0        | 0        | 252         | 1                            | 0        | 0        | 0        | 1          | 1               | 229         | 1        | 0        | 231         | 491         |
| <b>Total</b>       | <b>1</b>                  | <b>0</b> | <b>17</b> | <b>0</b> | <b>18</b>  | <b>178</b>      | <b>803</b>  | <b>1</b> | <b>0</b> | <b>982</b>  | <b>5</b>                     | <b>0</b> | <b>0</b> | <b>0</b> | <b>5</b>   | <b>1</b>        | <b>979</b>  | <b>7</b> | <b>0</b> | <b>987</b>  | <b>1992</b> |
| <b>Grand Total</b> | <b>3</b>                  | <b>0</b> | <b>31</b> | <b>0</b> | <b>34</b>  | <b>357</b>      | <b>1523</b> | <b>2</b> | <b>0</b> | <b>1882</b> | <b>7</b>                     | <b>0</b> | <b>0</b> | <b>1</b> | <b>8</b>   | <b>1</b>        | <b>1741</b> | <b>9</b> | <b>0</b> | <b>1751</b> | <b>3675</b> |
| Apprch %           | 8.8                       | 0        | 91.2      | 0        |            | 19              | 80.9        | 0.1      | 0        |             | 87.5                         | 0        | 0        | 12.5     |            | 0.1             | 99.4        | 0.5      | 0        |             |             |
| Total %            | 0.1                       | 0        | 0.8       | 0        | 0.9        | 9.7             | 41.4        | 0.1      | 0        | 51.2        | 0.2                          | 0        | 0        | 0        | 0.2        | 0               | 47.4        | 0.2      | 0        | 47.6        |             |

| Start Time   | Bluespring Rd. Southbound |          |           |            | US 11 Westbound |            |          |            | Business Driveway Northbound |          |          |            | US 11 Eastbound |            |          |            | Int. Total  |
|--|---------------------------|----------|-----------|------------|-----------------|------------|----------|------------|------------------------------|----------|----------|------------|-----------------|------------|----------|------------|-------------|
|  | Right                     | Thru     | Left      | App. Total | Right           | Thru       | Left     | App. Total | Right                        | Thru     | Left     | App. Total | Right           | Thru       | Left     | App. Total |             |
| Peak Hour Analysis From 11:00 AM to 12:45 PM - Peak 1 of 1 |                           |          |           |            |                 |            |          |            |                              |          |          |            |                 |            |          |            |             |
| Peak Hour for Entire Intersection Begins at 12:00 PM       |                           |          |           |            |                 |            |          |            |                              |          |          |            |                 |            |          |            |             |
| 12:00 PM   | 0                         | 0        | 2         | 2          | 48              | 208        | 1        | 257        | 1                            | 0        | 0        | 1          | 0               | 239        | 1        | 240        | 500         |
| 12:15 PM   | 0                         | 0        | 6         | 6          | 50              | 215        | 0        | 265        | 1                            | 0        | 0        | 1          | 0               | 247        | 3        | 250        | 522         |
| 12:30 PM   | 0                         | 0        | 3         | 3          | 38              | 170        | 0        | 208        | 2                            | 0        | 0        | 2          | 0               | 264        | 2        | 266        | 479         |
| 12:45 PM   | 1                         | 0        | 6         | 7          | 42              | 210        | 0        | 252        | 1                            | 0        | 0        | 1          | 1               | 229        | 1        | 231        | 491         |
| <b>Total Volume</b>  | <b>1</b>                  | <b>0</b> | <b>17</b> | <b>18</b>  | <b>178</b>      | <b>803</b> | <b>1</b> | <b>982</b> | <b>5</b>                     | <b>0</b> | <b>0</b> | <b>5</b>   | <b>1</b>        | <b>979</b> | <b>7</b> | <b>987</b> | <b>1992</b> |
| % App. Total   | 5.6                       | 0        | 94.4      |            | 18.1            | 81.8       | 0.1      |            | 100                          | 0        | 0        |            | 0.1             | 99.2       | 0.7      |            |             |
| PHF  | .250                      | .000     | .708      | .643       | .890            | .934       | .250     | .926       | .625                         | .000     | .000     | .625       | .250            | .927       | .583     | .928       | .954        |

# Peggy Malone & Associates, Inc.

## (888) 247-8602

File Name : TM 3-Bellspring and US 11 PM  
 Site Code :  
 Start Date : 2/4/2015  
 Page No : 1

Groups Printed- Car

| Start Time         | Bluespring Rd. Southbound |          |           |          |            | US 11 Westbound |             |          |          |             | Business Driveway Northbound |          |          |          |            | US 11 Eastbound |             |          |          |             | Int. Total  |
|--------------------|---------------------------|----------|-----------|----------|------------|-----------------|-------------|----------|----------|-------------|------------------------------|----------|----------|----------|------------|-----------------|-------------|----------|----------|-------------|-------------|
|                    | Right                     | Thru     | Left      | Peds     | App. Total | Right           | Thru        | Left     | Peds     | App. Total  | Right                        | Thru     | Left     | Peds     | App. Total | Right           | Thru        | Left     | Peds     | App. Total  |             |
| 03:00 PM           | 0                         | 0        | 1         | 1        | 2          | 54              | 208         | 0        | 0        | 262         | 0                            | 0        | 0        | 0        | 0          | 1               | 245         | 0        | 0        | 246         | 510         |
| 03:15 PM           | 2                         | 0        | 4         | 0        | 6          | 56              | 198         | 0        | 0        | 254         | 0                            | 0        | 0        | 0        | 0          | 0               | 273         | 0        | 0        | 273         | 533         |
| 03:30 PM           | 0                         | 0        | 1         | 0        | 1          | 66              | 259         | 0        | 0        | 325         | 0                            | 0        | 0        | 0        | 0          | 0               | 271         | 1        | 0        | 272         | 598         |
| 03:45 PM           | 1                         | 0        | 1         | 0        | 2          | 61              | 236         | 0        | 0        | 297         | 0                            | 0        | 0        | 0        | 0          | 0               | 264         | 0        | 0        | 264         | 563         |
| <b>Total</b>       | <b>3</b>                  | <b>0</b> | <b>7</b>  | <b>1</b> | <b>11</b>  | <b>237</b>      | <b>901</b>  | <b>0</b> | <b>0</b> | <b>1138</b> | <b>0</b>                     | <b>0</b> | <b>0</b> | <b>0</b> | <b>0</b>   | <b>1</b>        | <b>1053</b> | <b>1</b> | <b>0</b> | <b>1055</b> | <b>2204</b> |
| 04:00 PM           | 0                         | 0        | 4         | 1        | 5          | 60              | 248         | 0        | 0        | 308         | 0                            | 0        | 0        | 0        | 0          | 0               | 262         | 1        | 0        | 263         | 576         |
| 04:15 PM           | 0                         | 0        | 10        | 0        | 10         | 64              | 192         | 0        | 0        | 256         | 0                            | 0        | 0        | 0        | 0          | 0               | 267         | 0        | 0        | 267         | 533         |
| 04:30 PM           | 0                         | 0        | 4         | 0        | 4          | 63              | 247         | 0        | 0        | 310         | 1                            | 0        | 0        | 0        | 1          | 1               | 270         | 0        | 0        | 271         | 586         |
| 04:45 PM           | 2                         | 0        | 5         | 0        | 7          | 74              | 248         | 0        | 0        | 322         | 0                            | 0        | 0        | 0        | 0          | 0               | 276         | 0        | 0        | 276         | 605         |
| <b>Total</b>       | <b>2</b>                  | <b>0</b> | <b>23</b> | <b>1</b> | <b>26</b>  | <b>261</b>      | <b>935</b>  | <b>0</b> | <b>0</b> | <b>1196</b> | <b>1</b>                     | <b>0</b> | <b>0</b> | <b>0</b> | <b>1</b>   | <b>1</b>        | <b>1075</b> | <b>1</b> | <b>0</b> | <b>1077</b> | <b>2300</b> |
| 05:00 PM           | 0                         | 0        | 3         | 0        | 3          | 74              | 290         | 0        | 0        | 364         | 0                            | 0        | 0        | 2        | 2          | 0               | 282         | 0        | 0        | 282         | 651         |
| 05:15 PM           | 3                         | 0        | 6         | 0        | 9          | 93              | 301         | 0        | 0        | 394         | 1                            | 0        | 0        | 0        | 1          | 0               | 258         | 1        | 0        | 259         | 663         |
| 05:30 PM           | 1                         | 0        | 5         | 0        | 6          | 61              | 222         | 0        | 0        | 283         | 0                            | 0        | 0        | 0        | 0          | 0               | 297         | 1        | 0        | 298         | 587         |
| 05:45 PM           | 0                         | 0        | 4         | 0        | 4          | 53              | 191         | 0        | 1        | 245         | 0                            | 0        | 0        | 0        | 0          | 0               | 257         | 2        | 0        | 259         | 508         |
| <b>Total</b>       | <b>4</b>                  | <b>0</b> | <b>18</b> | <b>0</b> | <b>22</b>  | <b>281</b>      | <b>1004</b> | <b>0</b> | <b>1</b> | <b>1286</b> | <b>1</b>                     | <b>0</b> | <b>0</b> | <b>2</b> | <b>3</b>   | <b>0</b>        | <b>1094</b> | <b>4</b> | <b>0</b> | <b>1098</b> | <b>2409</b> |
| <b>Grand Total</b> | <b>9</b>                  | <b>0</b> | <b>48</b> | <b>2</b> | <b>59</b>  | <b>779</b>      | <b>2840</b> | <b>0</b> | <b>1</b> | <b>3620</b> | <b>2</b>                     | <b>0</b> | <b>0</b> | <b>2</b> | <b>4</b>   | <b>2</b>        | <b>3222</b> | <b>6</b> | <b>0</b> | <b>3230</b> | <b>6913</b> |
| Apprch %           | 15.3                      | 0        | 81.4      | 3.4      |            | 21.5            | 78.5        | 0        | 0        |             | 50                           | 0        | 0        | 50       |            | 0.1             | 99.8        | 0.2      | 0        |             |             |
| Total %            | 0.1                       | 0        | 0.7       | 0        | 0.9        | 11.3            | 41.1        | 0        | 0        | 52.4        | 0                            | 0        | 0        | 0        | 0.1        | 0               | 46.6        | 0.1      | 0        | 46.7        |             |

| Start Time   | Bluespring Rd. Southbound |      |      |            | US 11 Westbound |      |      |            | Business Driveway Northbound |      |      |            | US 11 Eastbound |      |      |            | Int. Total |
|--|---------------------------|------|------|------------|-----------------|------|------|------------|------------------------------|------|------|------------|-----------------|------|------|------------|------------|
|  | Right                     | Thru | Left | App. Total | Right           | Thru | Left | App. Total | Right                        | Thru | Left | App. Total | Right           | Thru | Left | App. Total |            |
| Peak Hour Analysis From 03:00 PM to 05:45 PM - Peak 1 of 1 |                           |      |      |            |                 |      |      |            |                              |      |      |            |                 |      |      |            |            |
| Peak Hour for Entire Intersection Begins at 04:45 PM       |                           |      |      |            |                 |      |      |            |                              |      |      |            |                 |      |      |            |            |
| 04:45 PM   | 2                         | 0    | 5    | 7          | 74              | 248  | 0    | 322        | 0                            | 0    | 0    | 0          | 0               | 276  | 0    | 276        | 605        |
| 05:00 PM   | 0                         | 0    | 3    | 3          | 74              | 290  | 0    | 364        | 0                            | 0    | 0    | 0          | 0               | 282  | 0    | 282        | 649        |
| 05:15 PM   | 3                         | 0    | 6    | 9          | 93              | 301  | 0    | 394        | 1                            | 0    | 0    | 1          | 0               | 258  | 1    | 259        | 663        |
| 05:30 PM   | 1                         | 0    | 5    | 6          | 61              | 222  | 0    | 283        | 0                            | 0    | 0    | 0          | 0               | 297  | 1    | 298        | 587        |
| Total Volume   | 6                         | 0    | 19   | 25         | 302             | 1061 | 0    | 1363       | 1                            | 0    | 0    | 1          | 0               | 1113 | 2    | 1115       | 2504       |
| % App. Total   | 24                        | 0    | 76   |            | 22.2            | 77.8 | 0    |            | 100                          | 0    | 0    |            | 0               | 99.8 | 0.2  |            |            |
| PHF  | .500                      | .000 | .792 | .694       | .812            | .881 | .000 | .865       | .250                         | .000 | .000 | .250       | .000            | .937 | .500 | .935       | .944       |

# Peggy Malone & Associates, Inc.

## (888) 247-8602

File Name : TM 3-Bellspring and US 11 PM  
 Site Code :  
 Start Date : 2/4/2015  
 Page No : 1

Groups Printed- Truck

| Start Time         | Bluespring Rd. Southbound |          |          |          |            | US 11 Westbound |           |          |          |            | Business Driveway Northbound |          |          |          |            | US 11 Eastbound |           |          |          |            | Int. Total |
|--------------------|---------------------------|----------|----------|----------|------------|-----------------|-----------|----------|----------|------------|------------------------------|----------|----------|----------|------------|-----------------|-----------|----------|----------|------------|------------|
|                    | Right                     | Thru     | Left     | Peds     | App. Total | Right           | Thru      | Left     | Peds     | App. Total | Right                        | Thru     | Left     | Peds     | App. Total | Right           | Thru      | Left     | Peds     | App. Total |            |
| 03:00 PM           | 0                         | 0        | 1        | 0        | 1          | 2               | 6         | 0        | 0        | 8          | 0                            | 0        | 0        | 0        | 0          | 0               | 5         | 0        | 0        | 5          | 14         |
| 03:15 PM           | 0                         | 0        | 0        | 0        | 0          | 4               | 2         | 0        | 0        | 6          | 0                            | 0        | 0        | 0        | 0          | 0               | 5         | 0        | 0        | 5          | 11         |
| 03:30 PM           | 0                         | 0        | 0        | 0        | 0          | 0               | 6         | 0        | 0        | 6          | 0                            | 0        | 0        | 0        | 0          | 0               | 7         | 0        | 0        | 7          | 13         |
| 03:45 PM           | 0                         | 0        | 0        | 0        | 0          | 1               | 5         | 0        | 0        | 6          | 0                            | 0        | 0        | 0        | 0          | 0               | 4         | 0        | 0        | 4          | 10         |
| <b>Total</b>       | <b>0</b>                  | <b>0</b> | <b>1</b> | <b>0</b> | <b>1</b>   | <b>7</b>        | <b>19</b> | <b>0</b> | <b>0</b> | <b>26</b>  | <b>0</b>                     | <b>0</b> | <b>0</b> | <b>0</b> | <b>0</b>   | <b>0</b>        | <b>21</b> | <b>0</b> | <b>0</b> | <b>21</b>  | <b>48</b>  |
| 04:00 PM           | 0                         | 0        | 0        | 0        | 0          | 0               | 2         | 0        | 0        | 2          | 0                            | 0        | 0        | 0        | 0          | 0               | 7         | 0        | 0        | 7          | 9          |
| 04:15 PM           | 0                         | 0        | 0        | 0        | 0          | 0               | 2         | 0        | 0        | 2          | 0                            | 0        | 0        | 0        | 0          | 0               | 8         | 0        | 0        | 8          | 10         |
| 04:30 PM           | 0                         | 0        | 0        | 0        | 0          | 0               | 3         | 0        | 0        | 3          | 0                            | 0        | 0        | 0        | 0          | 0               | 5         | 0        | 0        | 5          | 8          |
| 04:45 PM           | 0                         | 0        | 0        | 0        | 0          | 1               | 3         | 0        | 0        | 4          | 0                            | 0        | 0        | 0        | 0          | 0               | 0         | 0        | 0        | 0          | 4          |
| <b>Total</b>       | <b>0</b>                  | <b>0</b> | <b>0</b> | <b>0</b> | <b>0</b>   | <b>1</b>        | <b>10</b> | <b>0</b> | <b>0</b> | <b>11</b>  | <b>0</b>                     | <b>0</b> | <b>0</b> | <b>0</b> | <b>0</b>   | <b>0</b>        | <b>20</b> | <b>0</b> | <b>0</b> | <b>20</b>  | <b>31</b>  |
| 05:00 PM           | 0                         | 0        | 0        | 0        | 0          | 0               | 3         | 0        | 0        | 3          | 0                            | 0        | 0        | 0        | 0          | 0               | 4         | 0        | 0        | 4          | 7          |
| 05:15 PM           | 0                         | 0        | 0        | 0        | 0          | 0               | 1         | 0        | 0        | 1          | 0                            | 0        | 0        | 0        | 0          | 0               | 2         | 0        | 0        | 2          | 3          |
| 05:30 PM           | 0                         | 0        | 0        | 0        | 0          | 0               | 0         | 0        | 0        | 0          | 0                            | 0        | 0        | 0        | 0          | 0               | 1         | 0        | 0        | 1          | 1          |
| 05:45 PM           | 0                         | 0        | 0        | 0        | 0          | 0               | 5         | 0        | 0        | 5          | 0                            | 0        | 0        | 0        | 0          | 0               | 0         | 0        | 0        | 0          | 5          |
| <b>Total</b>       | <b>0</b>                  | <b>0</b> | <b>0</b> | <b>0</b> | <b>0</b>   | <b>0</b>        | <b>9</b>  | <b>0</b> | <b>0</b> | <b>9</b>   | <b>0</b>                     | <b>0</b> | <b>0</b> | <b>0</b> | <b>0</b>   | <b>0</b>        | <b>7</b>  | <b>0</b> | <b>0</b> | <b>7</b>   | <b>16</b>  |
| <b>Grand Total</b> | <b>0</b>                  | <b>0</b> | <b>1</b> | <b>0</b> | <b>1</b>   | <b>8</b>        | <b>38</b> | <b>0</b> | <b>0</b> | <b>46</b>  | <b>0</b>                     | <b>0</b> | <b>0</b> | <b>0</b> | <b>0</b>   | <b>0</b>        | <b>48</b> | <b>0</b> | <b>0</b> | <b>48</b>  | <b>95</b>  |
| Apprch %           | 0                         | 0        | 100      | 0        |            | 17.4            | 82.6      | 0        | 0        |            | 0                            | 0        | 0        | 0        |            | 0               | 100       | 0        | 0        |            |            |
| Total %            | 0                         | 0        | 1.1      | 0        | 1.1        | 8.4             | 40        | 0        | 0        | 48.4       | 0                            | 0        | 0        | 0        | 0          | 0               | 50.5      | 0        | 0        | 50.5       |            |

| Start Time   | Bluespring Rd. Southbound |          |          |            | US 11 Westbound |           |          |            | Business Driveway Northbound |          |          |            | US 11 Eastbound |           |          |            | Int. Total |
|--|---------------------------|----------|----------|------------|-----------------|-----------|----------|------------|------------------------------|----------|----------|------------|-----------------|-----------|----------|------------|------------|
|  | Right                     | Thru     | Left     | App. Total | Right           | Thru      | Left     | App. Total | Right                        | Thru     | Left     | App. Total | Right           | Thru      | Left     | App. Total |            |
| Peak Hour Analysis From 03:00 PM to 05:45 PM - Peak 1 of 1 |                           |          |          |            |                 |           |          |            |                              |          |          |            |                 |           |          |            |            |
| Peak Hour for Entire Intersection Begins at 03:00 PM       |                           |          |          |            |                 |           |          |            |                              |          |          |            |                 |           |          |            |            |
| 03:00 PM   | 0                         | 0        | 1        | 1          | 2               | 6         | 0        | 8          | 0                            | 0        | 0        | 0          | 0               | 5         | 0        | 5          | 14         |
| 03:15 PM   | 0                         | 0        | 0        | 0          | 4               | 2         | 0        | 6          | 0                            | 0        | 0        | 0          | 0               | 5         | 0        | 5          | 11         |
| 03:30 PM   | 0                         | 0        | 0        | 0          | 0               | 6         | 0        | 6          | 0                            | 0        | 0        | 0          | 0               | 7         | 0        | 7          | 13         |
| 03:45 PM   | 0                         | 0        | 0        | 0          | 1               | 5         | 0        | 6          | 0                            | 0        | 0        | 0          | 0               | 4         | 0        | 4          | 10         |
| <b>Total Volume</b>  | <b>0</b>                  | <b>0</b> | <b>1</b> | <b>1</b>   | <b>7</b>        | <b>19</b> | <b>0</b> | <b>26</b>  | <b>0</b>                     | <b>0</b> | <b>0</b> | <b>0</b>   | <b>0</b>        | <b>21</b> | <b>0</b> | <b>21</b>  | <b>48</b>  |
| % App. Total   | 0                         | 0        | 100      |            | 26.9            | 73.1      | 0        |            | 0                            | 0        | 0        |            | 0               | 100       | 0        |            |            |
| PHF  | .000                      | .000     | .250     | .250       | .438            | .792      | .000     | .813       | .000                         | .000     | .000     | .000       | .000            | .750      | .000     | .750       | .857       |

# Peggy Malone & Associates, Inc.

## (888) 247-8602

File Name : TM 3-Bellspring and US 11 PM  
 Site Code :  
 Start Date : 2/4/2015  
 Page No : 1

Groups Printed- Combined

| Start Time         | Bluespring Rd. Southbound |          |           |          |            | US 11 Westbound |             |          |          |             | Business Driveway Northbound |          |          |          |            | US 11 Eastbound |             |          |          |             | Int. Total  |
|--------------------|---------------------------|----------|-----------|----------|------------|-----------------|-------------|----------|----------|-------------|------------------------------|----------|----------|----------|------------|-----------------|-------------|----------|----------|-------------|-------------|
|                    | Right                     | Thru     | Left      | Peds     | App. Total | Right           | Thru        | Left     | Peds     | App. Total  | Right                        | Thru     | Left     | Peds     | App. Total | Right           | Thru        | Left     | Peds     | App. Total  |             |
| 03:00 PM           | 0                         | 0        | 2         | 1        | 3          | 56              | 214         | 0        | 0        | 270         | 0                            | 0        | 0        | 0        | 0          | 1               | 250         | 0        | 0        | 251         | 524         |
| 03:15 PM           | 2                         | 0        | 4         | 0        | 6          | 60              | 200         | 0        | 0        | 260         | 0                            | 0        | 0        | 0        | 0          | 0               | 278         | 0        | 0        | 278         | 544         |
| 03:30 PM           | 0                         | 0        | 1         | 0        | 1          | 66              | 265         | 0        | 0        | 331         | 0                            | 0        | 0        | 0        | 0          | 0               | 278         | 1        | 0        | 279         | 611         |
| 03:45 PM           | 1                         | 0        | 1         | 0        | 2          | 62              | 241         | 0        | 0        | 303         | 0                            | 0        | 0        | 0        | 0          | 0               | 268         | 0        | 0        | 268         | 573         |
| <b>Total</b>       | <b>3</b>                  | <b>0</b> | <b>8</b>  | <b>1</b> | <b>12</b>  | <b>244</b>      | <b>920</b>  | <b>0</b> | <b>0</b> | <b>1164</b> | <b>0</b>                     | <b>0</b> | <b>0</b> | <b>0</b> | <b>0</b>   | <b>1</b>        | <b>1074</b> | <b>1</b> | <b>0</b> | <b>1076</b> | <b>2252</b> |
| 04:00 PM           | 0                         | 0        | 4         | 1        | 5          | 60              | 250         | 0        | 0        | 310         | 0                            | 0        | 0        | 0        | 0          | 0               | 269         | 1        | 0        | 270         | 585         |
| 04:15 PM           | 0                         | 0        | 10        | 0        | 10         | 64              | 194         | 0        | 0        | 258         | 0                            | 0        | 0        | 0        | 0          | 0               | 275         | 0        | 0        | 275         | 543         |
| 04:30 PM           | 0                         | 0        | 4         | 0        | 4          | 63              | 250         | 0        | 0        | 313         | 1                            | 0        | 0        | 0        | 1          | 1               | 275         | 0        | 0        | 276         | 594         |
| 04:45 PM           | 2                         | 0        | 5         | 0        | 7          | 75              | 251         | 0        | 0        | 326         | 0                            | 0        | 0        | 0        | 0          | 0               | 276         | 0        | 0        | 276         | 609         |
| <b>Total</b>       | <b>2</b>                  | <b>0</b> | <b>23</b> | <b>1</b> | <b>26</b>  | <b>262</b>      | <b>945</b>  | <b>0</b> | <b>0</b> | <b>1207</b> | <b>1</b>                     | <b>0</b> | <b>0</b> | <b>0</b> | <b>1</b>   | <b>1</b>        | <b>1095</b> | <b>1</b> | <b>0</b> | <b>1097</b> | <b>2331</b> |
| 05:00 PM           | 0                         | 0        | 3         | 0        | 3          | 74              | 293         | 0        | 0        | 367         | 0                            | 0        | 0        | 2        | 2          | 0               | 286         | 0        | 0        | 286         | 658         |
| 05:15 PM           | 3                         | 0        | 6         | 0        | 9          | 93              | 302         | 0        | 0        | 395         | 1                            | 0        | 0        | 0        | 1          | 0               | 260         | 1        | 0        | 261         | 666         |
| 05:30 PM           | 1                         | 0        | 5         | 0        | 6          | 61              | 222         | 0        | 0        | 283         | 0                            | 0        | 0        | 0        | 0          | 0               | 298         | 1        | 0        | 299         | 588         |
| 05:45 PM           | 0                         | 0        | 4         | 0        | 4          | 53              | 196         | 0        | 1        | 250         | 0                            | 0        | 0        | 0        | 0          | 0               | 257         | 2        | 0        | 259         | 513         |
| <b>Total</b>       | <b>4</b>                  | <b>0</b> | <b>18</b> | <b>0</b> | <b>22</b>  | <b>281</b>      | <b>1013</b> | <b>0</b> | <b>1</b> | <b>1295</b> | <b>1</b>                     | <b>0</b> | <b>0</b> | <b>2</b> | <b>3</b>   | <b>0</b>        | <b>1101</b> | <b>4</b> | <b>0</b> | <b>1105</b> | <b>2425</b> |
| <b>Grand Total</b> | <b>9</b>                  | <b>0</b> | <b>49</b> | <b>2</b> | <b>60</b>  | <b>787</b>      | <b>2878</b> | <b>0</b> | <b>1</b> | <b>3666</b> | <b>2</b>                     | <b>0</b> | <b>0</b> | <b>2</b> | <b>4</b>   | <b>2</b>        | <b>3270</b> | <b>6</b> | <b>0</b> | <b>3278</b> | <b>7008</b> |
| Apprch %           | 15                        | 0        | 81.7      | 3.3      |            | 21.5            | 78.5        | 0        | 0        |             | 50                           | 0        | 0        | 50       |            | 0.1             | 99.8        | 0.2      | 0        |             |             |
| Total %            | 0.1                       | 0        | 0.7       | 0        | 0.9        | 11.2            | 41.1        | 0        | 0        | 52.3        | 0                            | 0        | 0        | 0        | 0.1        | 0               | 46.7        | 0.1      | 0        | 46.8        |             |

| Start Time   | Bluespring Rd. Southbound |      |          |            | US 11 Westbound |            |      |            | Business Driveway Northbound |      |      |            | US 11 Eastbound |            |          |            | Int. Total |
|--|---------------------------|------|----------|------------|-----------------|------------|------|------------|------------------------------|------|------|------------|-----------------|------------|----------|------------|------------|
|  | Right                     | Thru | Left     | App. Total | Right           | Thru       | Left | App. Total | Right                        | Thru | Left | App. Total | Right           | Thru       | Left     | App. Total |            |
| Peak Hour Analysis From 03:00 PM to 05:45 PM - Peak 1 of 1 |                           |      |          |            |                 |            |      |            |                              |      |      |            |                 |            |          |            |            |
| Peak Hour for Entire Intersection Begins at 04:30 PM       |                           |      |          |            |                 |            |      |            |                              |      |      |            |                 |            |          |            |            |
| 04:30 PM   | 0                         | 0    | 4        | 4          | 63              | 250        | 0    | 313        | <b>1</b>                     | 0    | 0    | <b>1</b>   | <b>1</b>        | 275        | 0        | 276        | 594        |
| 04:45 PM   | 2                         | 0    | 5        | 7          | 75              | 251        | 0    | 326        | 0                            | 0    | 0    | 0          | 0               | 276        | 0        | 276        | 609        |
| 05:00 PM   | 0                         | 0    | 3        | 3          | 74              | 293        | 0    | 367        | 0                            | 0    | 0    | 0          | 0               | <b>286</b> | 0        | <b>286</b> | 656        |
| 05:15 PM   | <b>3</b>                  | 0    | <b>6</b> | <b>9</b>   | <b>93</b>       | <b>302</b> | 0    | <b>395</b> | <b>1</b>                     | 0    | 0    | <b>1</b>   | 0               | 260        | <b>1</b> | 261        | <b>666</b> |
| Total Volume   | 5                         | 0    | 18       | 23         | 305             | 1096       | 0    | 1401       | 2                            | 0    | 0    | 2          | 1               | 1097       | 1        | 1099       | 2525       |
| % App. Total   | 21.7                      | 0    | 78.3     |            | 21.8            | 78.2       | 0    |            | 100                          | 0    | 0    |            | 0.1             | 99.8       | 0.1      |            |            |
| PHF  | .417                      | .000 | .750     | .639       | .820            | .907       | .000 | .887       | .500                         | .000 | .000 | .500       | .250            | .959       | .250     | .961       | .948       |

# Peggy Malone & Associates, Inc.

## (888) 247-8602

File Name : TM 4-Nicewander and US 11 AM  
 Site Code :  
 Start Date : 2/4/2015  
 Page No : 1

Groups Printed- Car

| Start Time         | Nicewander Way Southbound |          |           |          |            | US 11 Westbound |          |          |          |            | Business Driveway Northbound |          |          |          |            | US 11 Eastbound |          |          |          |            | Int. Total |
|--------------------|---------------------------|----------|-----------|----------|------------|-----------------|----------|----------|----------|------------|------------------------------|----------|----------|----------|------------|-----------------|----------|----------|----------|------------|------------|
|                    | Right                     | Thru     | Left      | Peds     | App. Total | Right           | Thru     | Left     | Peds     | App. Total | Right                        | Thru     | Left     | Peds     | App. Total | Right           | Thru     | Left     | Peds     | App. Total |            |
| 07:00 AM           | 1                         | 0        | 1         | 0        | 2          | 0               | 0        | 0        | 0        | 0          | 0                            | 0        | 0        | 0        | 0          | 0               | 0        | 0        | 0        | 0          | 0          |
| 07:15 AM           | 1                         | 0        | 2         | 0        | 3          | 1               | 0        | 0        | 0        | 1          | 0                            | 0        | 0        | 0        | 0          | 0               | 0        | 1        | 0        | 1          | 1          |
| 07:30 AM           | 1                         | 0        | 1         | 0        | 2          | 0               | 0        | 0        | 0        | 0          | 0                            | 0        | 0        | 0        | 0          | 1               | 0        | 1        | 0        | 2          | 4          |
| 07:45 AM           | 3                         | 0        | 2         | 0        | 5          | 0               | 0        | 0        | 0        | 0          | 0                            | 0        | 0        | 0        | 0          | 0               | 0        | 1        | 0        | 1          | 6          |
| <b>Total</b>       | <b>6</b>                  | <b>0</b> | <b>6</b>  | <b>0</b> | <b>12</b>  | <b>1</b>        | <b>0</b> | <b>0</b> | <b>0</b> | <b>1</b>   | <b>0</b>                     | <b>0</b> | <b>0</b> | <b>0</b> | <b>0</b>   | <b>1</b>        | <b>0</b> | <b>3</b> | <b>0</b> | <b>4</b>   | <b>17</b>  |
| 08:00 AM           | 4                         | 0        | 2         | 0        | 6          | 0               | 0        | 0        | 0        | 0          | 0                            | 0        | 0        | 0        | 0          | 0               | 0        | 0        | 0        | 0          | 6          |
| 08:15 AM           | 0                         | 0        | 2         | 0        | 2          | 1               | 0        | 1        | 0        | 2          | 0                            | 0        | 0        | 0        | 0          | 3               | 0        | 2        | 0        | 5          | 9          |
| 08:30 AM           | 1                         | 0        | 0         | 0        | 1          | 0               | 0        | 0        | 0        | 0          | 0                            | 0        | 0        | 0        | 0          | 0               | 0        | 0        | 0        | 0          | 1          |
| 08:45 AM           | 0                         | 0        | 3         | 0        | 3          | 1               | 0        | 0        | 0        | 1          | 0                            | 0        | 0        | 1        | 1          | 0               | 0        | 2        | 0        | 2          | 7          |
| <b>Total</b>       | <b>5</b>                  | <b>0</b> | <b>7</b>  | <b>0</b> | <b>12</b>  | <b>2</b>        | <b>0</b> | <b>1</b> | <b>0</b> | <b>3</b>   | <b>0</b>                     | <b>0</b> | <b>0</b> | <b>1</b> | <b>1</b>   | <b>3</b>        | <b>0</b> | <b>4</b> | <b>0</b> | <b>7</b>   | <b>23</b>  |
| <b>Grand Total</b> | <b>11</b>                 | <b>0</b> | <b>13</b> | <b>0</b> | <b>24</b>  | <b>3</b>        | <b>0</b> | <b>1</b> | <b>0</b> | <b>4</b>   | <b>0</b>                     | <b>0</b> | <b>0</b> | <b>1</b> | <b>1</b>   | <b>4</b>        | <b>0</b> | <b>7</b> | <b>0</b> | <b>11</b>  | <b>40</b>  |
| Apprch %           | 45.8                      | 0        | 54.2      | 0        |            | 75              | 0        | 25       | 0        |            | 0                            | 0        | 0        | 100      |            | 36.4            | 0        | 63.6     | 0        |            |            |
| Total %            | 27.5                      | 0        | 32.5      | 0        | 60         | 7.5             | 0        | 2.5      | 0        | 10         | 0                            | 0        | 0        | 2.5      | 2.5        | 10              | 0        | 17.5     | 0        | 27.5       |            |

| Start Time   | Nicewander Way Southbound |          |          |            | US 11 Westbound |          |          |            | Business Driveway Northbound |          |          |            | US 11 Eastbound |          |          |            | Int. Total |
|--|---------------------------|----------|----------|------------|-----------------|----------|----------|------------|------------------------------|----------|----------|------------|-----------------|----------|----------|------------|------------|
|  | Right                     | Thru     | Left     | App. Total | Right           | Thru     | Left     | App. Total | Right                        | Thru     | Left     | App. Total | Right           | Thru     | Left     | App. Total |            |
| Peak Hour Analysis From 07:00 AM to 08:45 AM - Peak 1 of 1 |                           |          |          |            |                 |          |          |            |                              |          |          |            |                 |          |          |            |            |
| Peak Hour for Entire Intersection Begins at 07:30 AM       |                           |          |          |            |                 |          |          |            |                              |          |          |            |                 |          |          |            |            |
| 07:30 AM   | 1                         | 0        | 1        | 2          | 0               | 0        | 0        | 0          | 0                            | 0        | 0        | 0          | 1               | 0        | 1        | 2          | 4          |
| 07:45 AM   | 3                         | 0        | 2        | 5          | 0               | 0        | 0        | 0          | 0                            | 0        | 0        | 0          | 0               | 0        | 1        | 1          | 6          |
| 08:00 AM   | 4                         | 0        | 2        | 6          | 0               | 0        | 0        | 0          | 0                            | 0        | 0        | 0          | 0               | 0        | 0        | 0          | 6          |
| 08:15 AM   | 0                         | 0        | 2        | 2          | 1               | 0        | 1        | 2          | 0                            | 0        | 0        | 0          | 3               | 0        | 2        | 5          | 9          |
| <b>Total Volume</b>  | <b>8</b>                  | <b>0</b> | <b>7</b> | <b>15</b>  | <b>1</b>        | <b>0</b> | <b>1</b> | <b>2</b>   | <b>0</b>                     | <b>0</b> | <b>0</b> | <b>0</b>   | <b>4</b>        | <b>0</b> | <b>4</b> | <b>8</b>   | <b>25</b>  |
| % App. Total   | 53.3                      | 0        | 46.7     |            | 50              | 0        | 50       |            | 0                            | 0        | 0        |            | 50              | 0        | 50       |            |            |
| PHF  | .500                      | .000     | .875     | .625       | .250            | .000     | .250     | .250       | .000                         | .000     | .000     | .000       | .333            | .000     | .500     | .400       | .694       |

# Peggy Malone & Associates, Inc.

## (888) 247-8602

File Name : TM 4-Nicewander and US 11 AM  
 Site Code :  
 Start Date : 2/4/2015  
 Page No : 1

Groups Printed- Truck

| Start Time         | Nicewander Way Southbound |      |      |      |            | US 11 Westbound |      |      |      |            | Business Driveway Northbound |      |      |      |            | US 11 Eastbound |      |      |      |            | Int. Total |
|--------------------|---------------------------|------|------|------|------------|-----------------|------|------|------|------------|------------------------------|------|------|------|------------|-----------------|------|------|------|------------|------------|
|                    | Right                     | Thru | Left | Peds | App. Total | Right           | Thru | Left | Peds | App. Total | Right                        | Thru | Left | Peds | App. Total | Right           | Thru | Left | Peds | App. Total |            |
| 07:00 AM           | 0                         | 0    | 0    | 0    | 0          | 0               | 0    | 0    | 0    | 0          | 0                            | 0    | 0    | 0    | 0          | 0               | 0    | 0    | 0    | 0          | 0          |
| 07:15 AM           | 0                         | 0    | 0    | 0    | 0          | 0               | 0    | 0    | 0    | 0          | 0                            | 0    | 0    | 0    | 0          | 0               | 0    | 0    | 0    | 0          | 0          |
| 07:30 AM           | 0                         | 0    | 0    | 0    | 0          | 0               | 0    | 0    | 0    | 0          | 0                            | 0    | 0    | 0    | 0          | 0               | 0    | 0    | 0    | 0          | 0          |
| 07:45 AM           | 0                         | 0    | 0    | 0    | 0          | 0               | 0    | 0    | 0    | 0          | 1                            | 0    | 0    | 0    | 1          | 1               | 0    | 0    | 0    | 1          | 2          |
| <b>Total</b>       | 0                         | 0    | 0    | 0    | 0          | 0               | 0    | 0    | 0    | 0          | 1                            | 0    | 0    | 0    | 1          | 1               | 0    | 0    | 0    | 1          | 2          |
| 08:00 AM           | 0                         | 0    | 0    | 0    | 0          | 0               | 0    | 0    | 0    | 0          | 0                            | 0    | 0    | 0    | 0          | 0               | 0    | 0    | 0    | 0          | 0          |
| 08:15 AM           | 0                         | 0    | 0    | 0    | 0          | 0               | 0    | 0    | 0    | 0          | 0                            | 0    | 0    | 0    | 0          | 1               | 0    | 0    | 0    | 1          | 1          |
| 08:30 AM           | 0                         | 0    | 0    | 0    | 0          | 0               | 0    | 0    | 0    | 0          | 0                            | 0    | 0    | 0    | 0          | 0               | 0    | 0    | 0    | 0          | 0          |
| 08:45 AM           | 0                         | 0    | 0    | 0    | 0          | 0               | 0    | 0    | 0    | 0          | 0                            | 0    | 0    | 0    | 0          | 0               | 0    | 0    | 0    | 0          | 0          |
| <b>Total</b>       | 0                         | 0    | 0    | 0    | 0          | 0               | 0    | 0    | 0    | 0          | 0                            | 0    | 0    | 0    | 0          | 1               | 0    | 0    | 0    | 1          | 1          |
| <b>Grand Total</b> | 0                         | 0    | 0    | 0    | 0          | 0               | 0    | 0    | 0    | 0          | 1                            | 0    | 0    | 0    | 1          | 2               | 0    | 0    | 0    | 2          | 3          |
| Apprch %           | 0                         | 0    | 0    | 0    |            | 0               | 0    | 0    | 0    |            | 100                          | 0    | 0    | 0    |            | 100             | 0    | 0    | 0    |            |            |
| Total %            | 0                         | 0    | 0    | 0    | 0          | 0               | 0    | 0    | 0    | 0          | 33.3                         | 0    | 0    | 0    | 33.3       | 66.7            | 0    | 0    | 0    | 66.7       |            |

| Start Time   | Nicewander Way Southbound |      |      |            | US 11 Westbound |      |      |            | Business Driveway Northbound |      |      |            | US 11 Eastbound |      |      |            | Int. Total |
|--|---------------------------|------|------|------------|-----------------|------|------|------------|------------------------------|------|------|------------|-----------------|------|------|------------|------------|
|  | Right                     | Thru | Left | App. Total | Right           | Thru | Left | App. Total | Right                        | Thru | Left | App. Total | Right           | Thru | Left | App. Total |            |
| Peak Hour Analysis From 07:00 AM to 08:45 AM - Peak 1 of 1 |                           |      |      |            |                 |      |      |            |                              |      |      |            |                 |      |      |            |            |
| Peak Hour for Entire Intersection Begins at 07:30 AM       |                           |      |      |            |                 |      |      |            |                              |      |      |            |                 |      |      |            |            |
| 07:30 AM   | 0                         | 0    | 0    | 0          | 0               | 0    | 0    | 0          | 0                            | 0    | 0    | 0          | 0               | 0    | 0    | 0          |            |
| 07:45 AM   | 0                         | 0    | 0    | 0          | 0               | 0    | 0    | 0          | 1                            | 0    | 0    | 1          | 1               | 0    | 0    | 1          |            |
| 08:00 AM   | 0                         | 0    | 0    | 0          | 0               | 0    | 0    | 0          | 0                            | 0    | 0    | 0          | 0               | 0    | 0    | 0          |            |
| 08:15 AM   | 0                         | 0    | 0    | 0          | 0               | 0    | 0    | 0          | 0                            | 0    | 0    | 0          | 1               | 0    | 0    | 1          |            |
| <b>Total Volume</b>  | 0                         | 0    | 0    | 0          | 0               | 0    | 0    | 0          | 1                            | 0    | 0    | 1          | 2               | 0    | 0    | 2          |            |
| % App. Total   | 0                         | 0    | 0    |            | 0               | 0    | 0    |            | 100                          | 0    | 0    |            | 100             | 0    | 0    |            |            |
| PHF  | .000                      | .000 | .000 | .000       | .000            | .000 | .000 | .000       | .250                         | .000 | .000 | .250       | .500            | .000 | .000 | .500       |            |

# Peggy Malone & Associates, Inc.

## (888) 247-8602

File Name : TM 4-Nicewander and US 11 AM  
 Site Code :  
 Start Date : 2/4/2015  
 Page No : 1

Groups Printed- Combined

| Start Time         | Nicewander Way Southbound |          |           |          |            | US 11 Westbound |          |          |          |            | Business Driveway Northbound |          |          |          |            | US 11 Eastbound |          |          |          |            | Int. Total |
|--------------------|---------------------------|----------|-----------|----------|------------|-----------------|----------|----------|----------|------------|------------------------------|----------|----------|----------|------------|-----------------|----------|----------|----------|------------|------------|
|                    | Right                     | Thru     | Left      | Peds     | App. Total | Right           | Thru     | Left     | Peds     | App. Total | Right                        | Thru     | Left     | Peds     | App. Total | Right           | Thru     | Left     | Peds     | App. Total |            |
| 07:00 AM           | 1                         | 0        | 1         | 0        | 2          | 0               | 0        | 0        | 0        | 0          | 0                            | 0        | 0        | 0        | 0          | 0               | 0        | 0        | 0        | 0          | 0          |
| 07:15 AM           | 1                         | 0        | 2         | 0        | 3          | 1               | 0        | 0        | 0        | 1          | 0                            | 0        | 0        | 0        | 0          | 0               | 0        | 1        | 0        | 1          | 1          |
| 07:30 AM           | 1                         | 0        | 1         | 0        | 2          | 0               | 0        | 0        | 0        | 0          | 0                            | 0        | 0        | 0        | 0          | 1               | 0        | 1        | 0        | 2          | 4          |
| 07:45 AM           | 3                         | 0        | 2         | 0        | 5          | 0               | 0        | 0        | 0        | 0          | 1                            | 0        | 0        | 0        | 1          | 1               | 0        | 1        | 0        | 2          | 8          |
| <b>Total</b>       | <b>6</b>                  | <b>0</b> | <b>6</b>  | <b>0</b> | <b>12</b>  | <b>1</b>        | <b>0</b> | <b>0</b> | <b>0</b> | <b>1</b>   | <b>1</b>                     | <b>0</b> | <b>0</b> | <b>0</b> | <b>1</b>   | <b>2</b>        | <b>0</b> | <b>3</b> | <b>0</b> | <b>5</b>   | <b>19</b>  |
| 08:00 AM           | 4                         | 0        | 2         | 0        | 6          | 0               | 0        | 0        | 0        | 0          | 0                            | 0        | 0        | 0        | 0          | 0               | 0        | 0        | 0        | 0          | 6          |
| 08:15 AM           | 0                         | 0        | 2         | 0        | 2          | 1               | 0        | 1        | 0        | 2          | 0                            | 0        | 0        | 0        | 0          | 4               | 0        | 2        | 0        | 6          | 10         |
| 08:30 AM           | 1                         | 0        | 0         | 0        | 1          | 0               | 0        | 0        | 0        | 0          | 0                            | 0        | 0        | 0        | 0          | 0               | 0        | 0        | 0        | 0          | 1          |
| 08:45 AM           | 0                         | 0        | 3         | 0        | 3          | 1               | 0        | 0        | 0        | 1          | 0                            | 0        | 0        | 1        | 1          | 0               | 0        | 2        | 0        | 2          | 7          |
| <b>Total</b>       | <b>5</b>                  | <b>0</b> | <b>7</b>  | <b>0</b> | <b>12</b>  | <b>2</b>        | <b>0</b> | <b>1</b> | <b>0</b> | <b>3</b>   | <b>0</b>                     | <b>0</b> | <b>0</b> | <b>1</b> | <b>1</b>   | <b>4</b>        | <b>0</b> | <b>4</b> | <b>0</b> | <b>8</b>   | <b>24</b>  |
| <b>Grand Total</b> | <b>11</b>                 | <b>0</b> | <b>13</b> | <b>0</b> | <b>24</b>  | <b>3</b>        | <b>0</b> | <b>1</b> | <b>0</b> | <b>4</b>   | <b>1</b>                     | <b>0</b> | <b>0</b> | <b>1</b> | <b>2</b>   | <b>6</b>        | <b>0</b> | <b>7</b> | <b>0</b> | <b>13</b>  | <b>43</b>  |
| Apprch %           | 45.8                      | 0        | 54.2      | 0        |            | 75              | 0        | 25       | 0        |            | 50                           | 0        | 0        | 50       |            | 46.2            | 0        | 53.8     | 0        |            |            |
| Total %            | 25.6                      | 0        | 30.2      | 0        | 55.8       | 7               | 0        | 2.3      | 0        | 9.3        | 2.3                          | 0        | 0        | 2.3      | 4.7        | 14              | 0        | 16.3     | 0        | 30.2       |            |

| Start Time   | Nicewander Way Southbound |          |          |            | US 11 Westbound |          |          |            | Business Driveway Northbound |          |          |            | US 11 Eastbound |          |          |            | Int. Total |
|--|---------------------------|----------|----------|------------|-----------------|----------|----------|------------|------------------------------|----------|----------|------------|-----------------|----------|----------|------------|------------|
|  | Right                     | Thru     | Left     | App. Total | Right           | Thru     | Left     | App. Total | Right                        | Thru     | Left     | App. Total | Right           | Thru     | Left     | App. Total |            |
| Peak Hour Analysis From 07:00 AM to 08:45 AM - Peak 1 of 1 |                           |          |          |            |                 |          |          |            |                              |          |          |            |                 |          |          |            |            |
| Peak Hour for Entire Intersection Begins at 07:30 AM       |                           |          |          |            |                 |          |          |            |                              |          |          |            |                 |          |          |            |            |
| 07:30 AM   | 1                         | 0        | 1        | 2          | 0               | 0        | 0        | 0          | 0                            | 0        | 0        | 0          | 1               | 0        | 1        | 2          | 4          |
| 07:45 AM   | 3                         | 0        | 2        | 5          | 0               | 0        | 0        | 0          | 1                            | 0        | 0        | 1          | 1               | 0        | 1        | 2          | 8          |
| 08:00 AM   | 4                         | 0        | 2        | 6          | 0               | 0        | 0        | 0          | 0                            | 0        | 0        | 0          | 0               | 0        | 0        | 0          | 6          |
| 08:15 AM   | 0                         | 0        | 2        | 2          | 1               | 0        | 1        | 2          | 0                            | 0        | 0        | 0          | 4               | 0        | 2        | 6          | 10         |
| <b>Total Volume</b>  | <b>8</b>                  | <b>0</b> | <b>7</b> | <b>15</b>  | <b>1</b>        | <b>0</b> | <b>1</b> | <b>2</b>   | <b>1</b>                     | <b>0</b> | <b>0</b> | <b>1</b>   | <b>6</b>        | <b>0</b> | <b>4</b> | <b>10</b>  | <b>28</b>  |
| % App. Total   | 53.3                      | 0        | 46.7     |            | 50              | 0        | 50       |            | 100                          | 0        | 0        |            | 60              | 0        | 40       |            |            |
| PHF  | .500                      | .000     | .875     | .625       | .250            | .000     | .250     | .250       | .250                         | .000     | .000     | .250       | .375            | .000     | .500     | .417       | .700       |

# Peggy Malone & Associates, Inc.

## (888) 247-8602

File Name : TM 4-Nicewander and US 11 MID  
 Site Code :  
 Start Date : 2/4/2015  
 Page No : 1

Groups Printed- Car

| Start Time         | Nicewander Way Southbound |          |          |          |            | US 11 Westbound |          |           |          |            | Business Driveway Northbound |          |           |          |            | US 11 Eastbound |          |          |          |            | Int. Total |
|--------------------|---------------------------|----------|----------|----------|------------|-----------------|----------|-----------|----------|------------|------------------------------|----------|-----------|----------|------------|-----------------|----------|----------|----------|------------|------------|
|                    | Right                     | Thru     | Left     | Peds     | App. Total | Right           | Thru     | Left      | Peds     | App. Total | Right                        | Thru     | Left      | Peds     | App. Total | Right           | Thru     | Left     | Peds     | App. Total |            |
| 11:00 AM           | 0                         | 0        | 3        | 0        | 3          | 1               | 0        | 3         | 1        | 5          | 1                            | 0        | 1         | 0        | 2          | 4               | 0        | 1        | 0        | 5          | 15         |
| 11:15 AM           | 2                         | 0        | 0        | 0        | 2          | 0               | 0        | 1         | 0        | 1          | 1                            | 0        | 3         | 0        | 4          | 5               | 0        | 0        | 0        | 5          | 12         |
| 11:30 AM           | 1                         | 0        | 0        | 0        | 1          | 0               | 0        | 4         | 0        | 4          | 1                            | 0        | 3         | 0        | 4          | 7               | 0        | 0        | 0        | 7          | 16         |
| 11:45 AM           | 1                         | 0        | 2        | 0        | 3          | 2               | 0        | 8         | 0        | 10         | 2                            | 0        | 5         | 1        | 8          | 4               | 0        | 0        | 0        | 4          | 25         |
| <b>Total</b>       | <b>4</b>                  | <b>0</b> | <b>5</b> | <b>0</b> | <b>9</b>   | <b>3</b>        | <b>0</b> | <b>16</b> | <b>1</b> | <b>20</b>  | <b>5</b>                     | <b>0</b> | <b>12</b> | <b>1</b> | <b>18</b>  | <b>20</b>       | <b>0</b> | <b>1</b> | <b>0</b> | <b>21</b>  | <b>68</b>  |
| 12:00 PM           | 1                         | 0        | 1        | 0        | 2          | 0               | 0        | 10        | 0        | 10         | 3                            | 0        | 6         | 0        | 9          | 8               | 0        | 2        | 0        | 10         | 31         |
| 12:15 PM           | 0                         | 0        | 1        | 0        | 1          | 0               | 0        | 6         | 0        | 6          | 11                           | 0        | 3         | 0        | 14         | 8               | 0        | 2        | 0        | 10         | 31         |
| 12:30 PM           | 0                         | 0        | 0        | 0        | 0          | 1               | 0        | 8         | 0        | 9          | 7                            | 0        | 8         | 0        | 15         | 7               | 0        | 0        | 0        | 7          | 31         |
| 12:45 PM           | 0                         | 0        | 0        | 0        | 0          | 0               | 0        | 2         | 0        | 2          | 9                            | 0        | 10        | 0        | 19         | 9               | 0        | 0        | 0        | 9          | 30         |
| <b>Total</b>       | <b>1</b>                  | <b>0</b> | <b>2</b> | <b>0</b> | <b>3</b>   | <b>1</b>        | <b>0</b> | <b>26</b> | <b>0</b> | <b>27</b>  | <b>30</b>                    | <b>0</b> | <b>27</b> | <b>0</b> | <b>57</b>  | <b>32</b>       | <b>0</b> | <b>4</b> | <b>0</b> | <b>36</b>  | <b>123</b> |
| <b>Grand Total</b> | <b>5</b>                  | <b>0</b> | <b>7</b> | <b>0</b> | <b>12</b>  | <b>4</b>        | <b>0</b> | <b>42</b> | <b>1</b> | <b>47</b>  | <b>35</b>                    | <b>0</b> | <b>39</b> | <b>1</b> | <b>75</b>  | <b>52</b>       | <b>0</b> | <b>5</b> | <b>0</b> | <b>57</b>  | <b>191</b> |
| Apprch %           | 41.7                      | 0        | 58.3     | 0        |            | 8.5             | 0        | 89.4      | 2.1      |            | 46.7                         | 0        | 52        | 1.3      |            | 91.2            | 0        | 8.8      | 0        |            |            |
| Total %            | 2.6                       | 0        | 3.7      | 0        | 6.3        | 2.1             | 0        | 22        | 0.5      | 24.6       | 18.3                         | 0        | 20.4      | 0.5      | 39.3       | 27.2            | 0        | 2.6      | 0        | 29.8       |            |

| Start Time   | Nicewander Way Southbound |          |          |            | US 11 Westbound |          |           |            | Business Driveway Northbound |          |           |            | US 11 Eastbound |          |          |            | Int. Total |
|--|---------------------------|----------|----------|------------|-----------------|----------|-----------|------------|------------------------------|----------|-----------|------------|-----------------|----------|----------|------------|------------|
|  | Right                     | Thru     | Left     | App. Total | Right           | Thru     | Left      | App. Total | Right                        | Thru     | Left      | App. Total | Right           | Thru     | Left     | App. Total |            |
| Peak Hour Analysis From 11:00 AM to 12:45 PM - Peak 1 of 1 |                           |          |          |            |                 |          |           |            |                              |          |           |            |                 |          |          |            |            |
| Peak Hour for Entire Intersection Begins at 12:00 PM       |                           |          |          |            |                 |          |           |            |                              |          |           |            |                 |          |          |            |            |
| 12:00 PM   | 1                         | 0        | 1        | 2          | 0               | 0        | 10        | 10         | 3                            | 0        | 6         | 9          | 8               | 0        | 2        | 10         | 31         |
| 12:15 PM   | 0                         | 0        | 1        | 1          | 0               | 0        | 6         | 6          | 11                           | 0        | 3         | 14         | 8               | 0        | 2        | 10         | 31         |
| 12:30 PM   | 0                         | 0        | 0        | 0          | 1               | 0        | 8         | 9          | 7                            | 0        | 8         | 15         | 7               | 0        | 0        | 7          | 31         |
| 12:45 PM   | 0                         | 0        | 0        | 0          | 0               | 0        | 2         | 2          | 9                            | 0        | 10        | 19         | 9               | 0        | 0        | 9          | 30         |
| <b>Total Volume</b>  | <b>1</b>                  | <b>0</b> | <b>2</b> | <b>3</b>   | <b>1</b>        | <b>0</b> | <b>26</b> | <b>27</b>  | <b>30</b>                    | <b>0</b> | <b>27</b> | <b>57</b>  | <b>32</b>       | <b>0</b> | <b>4</b> | <b>36</b>  | <b>123</b> |
| % App. Total   | 33.3                      | 0        | 66.7     |            | 3.7             | 0        | 96.3      |            | 52.6                         | 0        | 47.4      |            | 88.9            | 0        | 11.1     |            |            |
| PHF  | .250                      | .000     | .500     | .375       | .250            | .000     | .650      | .675       | .682                         | .000     | .675      | .750       | .889            | .000     | .500     | .900       | .992       |



# Peggy Malone & Associates, Inc.

## (888) 247-8602

File Name : TM 4-Nicewander and US 11 MID  
 Site Code :  
 Start Date : 2/4/2015  
 Page No : 1

Groups Printed- Truck

| Start Time         | Nicewander Way Southbound |      |      |      |            | US 11 Westbound |      |      |      |            | Business Driveway Northbound |      |      |      |            | US 11 Eastbound |      |      |      |            | Int. Total |
|--------------------|---------------------------|------|------|------|------------|-----------------|------|------|------|------------|------------------------------|------|------|------|------------|-----------------|------|------|------|------------|------------|
|                    | Right                     | Thru | Left | Peds | App. Total | Right           | Thru | Left | Peds | App. Total | Right                        | Thru | Left | Peds | App. Total | Right           | Thru | Left | Peds | App. Total |            |
| 11:00 AM           | 0                         | 0    | 0    | 0    | 0          | 0               | 0    | 0    | 0    | 0          | 0                            | 0    | 0    | 0    | 0          | 0               | 0    | 0    | 0    | 0          | 0          |
| 11:15 AM           | 0                         | 0    | 0    | 0    | 0          | 0               | 0    | 0    | 0    | 0          | 0                            | 0    | 1    | 0    | 1          | 1               | 0    | 0    | 0    | 0          | 1          |
| 11:30 AM           | 0                         | 0    | 0    | 0    | 0          | 0               | 0    | 0    | 0    | 0          | 0                            | 0    | 0    | 0    | 0          | 0               | 0    | 0    | 0    | 0          | 0          |
| 11:45 AM           | 0                         | 0    | 0    | 0    | 0          | 0               | 0    | 0    | 0    | 0          | 0                            | 0    | 1    | 0    | 1          | 0               | 0    | 0    | 0    | 0          | 0          |
| <b>Total</b>       | 0                         | 0    | 0    | 0    | 0          | 0               | 0    | 0    | 0    | 0          | 0                            | 0    | 2    | 0    | 2          | 1               | 0    | 0    | 0    | 1          | 3          |
| 12:00 PM           | 0                         | 0    | 0    | 0    | 0          | 0               | 0    | 0    | 0    | 0          | 0                            | 0    | 0    | 0    | 0          | 0               | 0    | 0    | 0    | 0          | 0          |
| 12:15 PM           | 0                         | 0    | 0    | 0    | 0          | 0               | 0    | 0    | 0    | 0          | 1                            | 0    | 0    | 0    | 1          | 0               | 0    | 0    | 0    | 0          | 1          |
| 12:30 PM           | 0                         | 0    | 0    | 0    | 0          | 0               | 0    | 1    | 0    | 1          | 0                            | 0    | 0    | 0    | 0          | 0               | 0    | 0    | 0    | 0          | 1          |
| 12:45 PM           | 1                         | 0    | 0    | 0    | 1          | 1               | 0    | 0    | 0    | 1          | 0                            | 0    | 1    | 0    | 1          | 0               | 0    | 0    | 0    | 0          | 3          |
| <b>Total</b>       | 1                         | 0    | 0    | 0    | 1          | 1               | 0    | 1    | 0    | 2          | 1                            | 0    | 1    | 0    | 2          | 0               | 0    | 0    | 0    | 0          | 5          |
| <b>Grand Total</b> | 1                         | 0    | 0    | 0    | 1          | 1               | 0    | 1    | 0    | 2          | 1                            | 0    | 3    | 0    | 4          | 1               | 0    | 0    | 0    | 1          | 8          |
| Apprch %           | 100                       | 0    | 0    | 0    |            | 50              | 0    | 50   | 0    |            | 25                           | 0    | 75   | 0    |            | 100             | 0    | 0    | 0    |            |            |
| Total %            | 12.5                      | 0    | 0    | 0    | 12.5       | 12.5            | 0    | 12.5 | 0    | 25         | 12.5                         | 0    | 37.5 | 0    | 50         | 12.5            | 0    | 0    | 0    | 12.5       |            |

| Start Time   | Nicewander Way Southbound |      |      |            | US 11 Westbound |      |      |            | Business Driveway Northbound |      |      |            | US 11 Eastbound |      |      |            | Int. Total |
|--|---------------------------|------|------|------------|-----------------|------|------|------------|------------------------------|------|------|------------|-----------------|------|------|------------|------------|
|  | Right                     | Thru | Left | App. Total | Right           | Thru | Left | App. Total | Right                        | Thru | Left | App. Total | Right           | Thru | Left | App. Total |            |
| Peak Hour Analysis From 11:00 AM to 12:45 PM - Peak 1 of 1 |                           |      |      |            |                 |      |      |            |                              |      |      |            |                 |      |      |            |            |
| Peak Hour for Entire Intersection Begins at 12:00 PM       |                           |      |      |            |                 |      |      |            |                              |      |      |            |                 |      |      |            |            |
| 12:00 PM   | 0                         | 0    | 0    | 0          | 0               | 0    | 0    | 0          | 0                            | 0    | 0    | 0          | 0               | 0    | 0    | 0          | 0          |
| 12:15 PM   | 0                         | 0    | 0    | 0          | 0               | 0    | 0    | 0          | 1                            | 0    | 0    | 1          | 0               | 0    | 0    | 0          | 1          |
| 12:30 PM   | 0                         | 0    | 0    | 0          | 0               | 0    | 1    | 1          | 0                            | 0    | 0    | 0          | 0               | 0    | 0    | 0          | 1          |
| 12:45 PM   | 1                         | 0    | 0    | 1          | 1               | 0    | 0    | 1          | 0                            | 0    | 1    | 1          | 0               | 0    | 0    | 0          | 3          |
| <b>Total Volume</b>  | 1                         | 0    | 0    | 1          | 1               | 0    | 1    | 2          | 1                            | 0    | 1    | 2          | 0               | 0    | 0    | 0          | 5          |
| % App. Total   | 100                       | 0    | 0    |            | 50              | 0    | 50   |            | 50                           | 0    | 50   |            | 0               | 0    | 0    |            |            |
| PHF  | .250                      | .000 | .000 | .250       | .250            | .000 | .250 | .500       | .250                         | .000 | .250 | .500       | .000            | .000 | .000 | .000       | .417       |

# Peggy Malone & Associates, Inc.

## (888) 247-8602

File Name : TM 4-Nicewander and US 11 MID  
 Site Code :  
 Start Date : 2/4/2015  
 Page No : 1

Groups Printed- Combined

| Start Time         | Nicewander Way Southbound |          |          |          |            | US 11 Westbound |          |           |          |            | Business Driveway Northbound |          |           |          |            | US 11 Eastbound |          |          |          |            | Int. Total |
|--------------------|---------------------------|----------|----------|----------|------------|-----------------|----------|-----------|----------|------------|------------------------------|----------|-----------|----------|------------|-----------------|----------|----------|----------|------------|------------|
|                    | Right                     | Thru     | Left     | Peds     | App. Total | Right           | Thru     | Left      | Peds     | App. Total | Right                        | Thru     | Left      | Peds     | App. Total | Right           | Thru     | Left     | Peds     | App. Total |            |
| 11:00 AM           | 0                         | 0        | 3        | 0        | 3          | 1               | 0        | 3         | 1        | 5          | 1                            | 0        | 1         | 0        | 2          | 4               | 0        | 1        | 0        | 5          | 15         |
| 11:15 AM           | 2                         | 0        | 0        | 0        | 2          | 0               | 0        | 1         | 0        | 1          | 1                            | 0        | 4         | 0        | 5          | 6               | 0        | 0        | 0        | 6          | 14         |
| 11:30 AM           | 1                         | 0        | 0        | 0        | 1          | 0               | 0        | 4         | 0        | 4          | 1                            | 0        | 3         | 0        | 4          | 7               | 0        | 0        | 0        | 7          | 16         |
| 11:45 AM           | 1                         | 0        | 2        | 0        | 3          | 2               | 0        | 8         | 0        | 10         | 2                            | 0        | 6         | 1        | 9          | 4               | 0        | 0        | 0        | 4          | 26         |
| <b>Total</b>       | <b>4</b>                  | <b>0</b> | <b>5</b> | <b>0</b> | <b>9</b>   | <b>3</b>        | <b>0</b> | <b>16</b> | <b>1</b> | <b>20</b>  | <b>5</b>                     | <b>0</b> | <b>14</b> | <b>1</b> | <b>20</b>  | <b>21</b>       | <b>0</b> | <b>1</b> | <b>0</b> | <b>22</b>  | <b>71</b>  |
| 12:00 PM           | 1                         | 0        | 1        | 0        | 2          | 0               | 0        | 10        | 0        | 10         | 3                            | 0        | 6         | 0        | 9          | 8               | 0        | 2        | 0        | 10         | 31         |
| 12:15 PM           | 0                         | 0        | 1        | 0        | 1          | 0               | 0        | 6         | 0        | 6          | 12                           | 0        | 3         | 0        | 15         | 8               | 0        | 2        | 0        | 10         | 32         |
| 12:30 PM           | 0                         | 0        | 0        | 0        | 0          | 1               | 0        | 9         | 0        | 10         | 7                            | 0        | 8         | 0        | 15         | 7               | 0        | 0        | 0        | 7          | 32         |
| 12:45 PM           | 1                         | 0        | 0        | 0        | 1          | 1               | 0        | 2         | 0        | 3          | 9                            | 0        | 11        | 0        | 20         | 9               | 0        | 0        | 0        | 9          | 33         |
| <b>Total</b>       | <b>2</b>                  | <b>0</b> | <b>2</b> | <b>0</b> | <b>4</b>   | <b>2</b>        | <b>0</b> | <b>27</b> | <b>0</b> | <b>29</b>  | <b>31</b>                    | <b>0</b> | <b>28</b> | <b>0</b> | <b>59</b>  | <b>32</b>       | <b>0</b> | <b>4</b> | <b>0</b> | <b>36</b>  | <b>128</b> |
| <b>Grand Total</b> | <b>6</b>                  | <b>0</b> | <b>7</b> | <b>0</b> | <b>13</b>  | <b>5</b>        | <b>0</b> | <b>43</b> | <b>1</b> | <b>49</b>  | <b>36</b>                    | <b>0</b> | <b>42</b> | <b>1</b> | <b>79</b>  | <b>53</b>       | <b>0</b> | <b>5</b> | <b>0</b> | <b>58</b>  | <b>199</b> |
| Apprch %           | 46.2                      | 0        | 53.8     | 0        |            | 10.2            | 0        | 87.8      | 2        |            | 45.6                         | 0        | 53.2      | 1.3      |            | 91.4            | 0        | 8.6      | 0        |            |            |
| Total %            | 3                         | 0        | 3.5      | 0        | 6.5        | 2.5             | 0        | 21.6      | 0.5      | 24.6       | 18.1                         | 0        | 21.1      | 0.5      | 39.7       | 26.6            | 0        | 2.5      | 0        | 29.1       |            |

| Start Time   | Nicewander Way Southbound |          |          |            | US 11 Westbound |          |           |            | Business Driveway Northbound |          |           |            | US 11 Eastbound |          |          |            | Int. Total |
|--|---------------------------|----------|----------|------------|-----------------|----------|-----------|------------|------------------------------|----------|-----------|------------|-----------------|----------|----------|------------|------------|
|  | Right                     | Thru     | Left     | App. Total | Right           | Thru     | Left      | App. Total | Right                        | Thru     | Left      | App. Total | Right           | Thru     | Left     | App. Total |            |
| Peak Hour Analysis From 11:00 AM to 12:45 PM - Peak 1 of 1 |                           |          |          |            |                 |          |           |            |                              |          |           |            |                 |          |          |            |            |
| Peak Hour for Entire Intersection Begins at 12:00 PM       |                           |          |          |            |                 |          |           |            |                              |          |           |            |                 |          |          |            |            |
| 12:00 PM   | 1                         | 0        | 1        | 2          | 0               | 0        | 10        | 10         | 3                            | 0        | 6         | 9          | 8               | 0        | 2        | 10         | 31         |
| 12:15 PM   | 0                         | 0        | 1        | 1          | 0               | 0        | 6         | 6          | 12                           | 0        | 3         | 15         | 8               | 0        | 2        | 10         | 32         |
| 12:30 PM   | 0                         | 0        | 0        | 0          | 1               | 0        | 9         | 10         | 7                            | 0        | 8         | 15         | 7               | 0        | 0        | 7          | 32         |
| 12:45 PM   | 1                         | 0        | 0        | 1          | 1               | 0        | 2         | 3          | 9                            | 0        | 11        | 20         | 9               | 0        | 0        | 9          | 33         |
| <b>Total Volume</b>  | <b>2</b>                  | <b>0</b> | <b>2</b> | <b>4</b>   | <b>2</b>        | <b>0</b> | <b>27</b> | <b>29</b>  | <b>31</b>                    | <b>0</b> | <b>28</b> | <b>59</b>  | <b>32</b>       | <b>0</b> | <b>4</b> | <b>36</b>  | <b>128</b> |
| % App. Total   | 50                        | 0        | 50       |            | 6.9             | 0        | 93.1      |            | 52.5                         | 0        | 47.5      |            | 88.9            | 0        | 11.1     |            |            |
| PHF  | .500                      | .000     | .500     | .500       | .500            | .000     | .675      | .725       | .646                         | .000     | .636      | .738       | .889            | .000     | .500     | .900       | .970       |

# Peggy Malone & Associates, Inc.

## (888) 247-8602

File Name : TM 4-Nicewander and US 11 PM  
 Site Code :  
 Start Date : 2/4/2015  
 Page No : 1

Groups Printed- Car

| Start Time         | Nicewander Way Southbound |          |           |          |            | US 11 Westbound |          |           |          |            | Business Driveway Northbound |          |           |          |            | US 11 Eastbound |          |           |          |            | Int. Total |
|--------------------|---------------------------|----------|-----------|----------|------------|-----------------|----------|-----------|----------|------------|------------------------------|----------|-----------|----------|------------|-----------------|----------|-----------|----------|------------|------------|
|                    | Right                     | Thru     | Left      | Peds     | App. Total | Right           | Thru     | Left      | Peds     | App. Total | Right                        | Thru     | Left      | Peds     | App. Total | Right           | Thru     | Left      | Peds     | App. Total |            |
| 03:00 PM           | 1                         | 0        | 1         | 0        | 2          | 1               | 0        | 3         | 0        | 4          | 0                            | 0        | 3         | 1        | 4          | 1               | 0        | 0         | 0        | 1          | 11         |
| 03:15 PM           | 2                         | 0        | 1         | 0        | 3          | 2               | 0        | 2         | 0        | 4          | 0                            | 0        | 5         | 0        | 5          | 5               | 0        | 2         | 0        | 7          | 19         |
| 03:30 PM           | 1                         | 0        | 0         | 0        | 1          | 0               | 0        | 1         | 0        | 1          | 6                            | 0        | 3         | 0        | 9          | 10              | 0        | 1         | 0        | 11         | 22         |
| 03:45 PM           | 0                         | 0        | 1         | 0        | 1          | 1               | 0        | 2         | 1        | 4          | 1                            | 0        | 7         | 0        | 8          | 2               | 0        | 0         | 0        | 2          | 15         |
| <b>Total</b>       | <b>4</b>                  | <b>0</b> | <b>3</b>  | <b>0</b> | <b>7</b>   | <b>4</b>        | <b>0</b> | <b>8</b>  | <b>1</b> | <b>13</b>  | <b>7</b>                     | <b>0</b> | <b>18</b> | <b>1</b> | <b>26</b>  | <b>18</b>       | <b>0</b> | <b>3</b>  | <b>0</b> | <b>21</b>  | <b>67</b>  |
| 04:00 PM           | 1                         | 0        | 4         | 1        | 6          | 2               | 0        | 3         | 0        | 5          | 2                            | 0        | 3         | 0        | 5          | 3               | 0        | 3         | 0        | 6          | 22         |
| 04:15 PM           | 2                         | 0        | 1         | 0        | 3          | 1               | 0        | 3         | 0        | 4          | 0                            | 0        | 7         | 0        | 7          | 9               | 0        | 0         | 0        | 9          | 23         |
| 04:30 PM           | 1                         | 0        | 0         | 0        | 1          | 1               | 0        | 5         | 0        | 6          | 0                            | 0        | 6         | 0        | 6          | 8               | 0        | 0         | 0        | 8          | 21         |
| 04:45 PM           | 0                         | 0        | 0         | 0        | 0          | 1               | 0        | 6         | 0        | 7          | 3                            | 0        | 3         | 1        | 7          | 6               | 0        | 0         | 0        | 6          | 20         |
| <b>Total</b>       | <b>4</b>                  | <b>0</b> | <b>5</b>  | <b>1</b> | <b>10</b>  | <b>5</b>        | <b>0</b> | <b>17</b> | <b>0</b> | <b>22</b>  | <b>5</b>                     | <b>0</b> | <b>19</b> | <b>1</b> | <b>25</b>  | <b>26</b>       | <b>0</b> | <b>3</b>  | <b>0</b> | <b>29</b>  | <b>86</b>  |
| 05:00 PM           | 2                         | 0        | 1         | 0        | 3          | 2               | 0        | 6         | 0        | 8          | 4                            | 0        | 5         | 2        | 11         | 10              | 0        | 0         | 0        | 10         | 32         |
| 05:15 PM           | 0                         | 0        | 1         | 0        | 1          | 1               | 0        | 1         | 0        | 2          | 4                            | 0        | 7         | 0        | 11         | 6               | 0        | 1         | 0        | 7          | 21         |
| 05:30 PM           | 1                         | 0        | 1         | 0        | 2          | 1               | 0        | 8         | 0        | 9          | 6                            | 0        | 2         | 0        | 8          | 5               | 0        | 2         | 0        | 7          | 26         |
| 05:45 PM           | 1                         | 0        | 1         | 0        | 2          | 1               | 0        | 3         | 1        | 5          | 4                            | 0        | 4         | 0        | 8          | 5               | 0        | 1         | 0        | 6          | 21         |
| <b>Total</b>       | <b>4</b>                  | <b>0</b> | <b>4</b>  | <b>0</b> | <b>8</b>   | <b>5</b>        | <b>0</b> | <b>18</b> | <b>1</b> | <b>24</b>  | <b>18</b>                    | <b>0</b> | <b>18</b> | <b>2</b> | <b>38</b>  | <b>26</b>       | <b>0</b> | <b>4</b>  | <b>0</b> | <b>30</b>  | <b>100</b> |
| <b>Grand Total</b> | <b>12</b>                 | <b>0</b> | <b>12</b> | <b>1</b> | <b>25</b>  | <b>14</b>       | <b>0</b> | <b>43</b> | <b>2</b> | <b>59</b>  | <b>30</b>                    | <b>0</b> | <b>55</b> | <b>4</b> | <b>89</b>  | <b>70</b>       | <b>0</b> | <b>10</b> | <b>0</b> | <b>80</b>  | <b>253</b> |
| Apprch %           | 48                        | 0        | 48        | 4        |            | 23.7            | 0        | 72.9      | 3.4      |            | 33.7                         | 0        | 61.8      | 4.5      |            | 87.5            | 0        | 12.5      | 0        |            |            |
| Total %            | 4.7                       | 0        | 4.7       | 0.4      | 9.9        | 5.5             | 0        | 17        | 0.8      | 23.3       | 11.9                         | 0        | 21.7      | 1.6      | 35.2       | 27.7            | 0        | 4         | 0        | 31.6       |            |

| Start Time   | Nicewander Way Southbound |          |          |            | US 11 Westbound |          |           |            | Business Driveway Northbound |          |           |            | US 11 Eastbound |          |          |            | Int. Total |
|--|---------------------------|----------|----------|------------|-----------------|----------|-----------|------------|------------------------------|----------|-----------|------------|-----------------|----------|----------|------------|------------|
|  | Right                     | Thru     | Left     | App. Total | Right           | Thru     | Left      | App. Total | Right                        | Thru     | Left      | App. Total | Right           | Thru     | Left     | App. Total |            |
| Peak Hour Analysis From 03:00 PM to 05:45 PM - Peak 1 of 1 |                           |          |          |            |                 |          |           |            |                              |          |           |            |                 |          |          |            |            |
| Peak Hour for Entire Intersection Begins at 05:00 PM       |                           |          |          |            |                 |          |           |            |                              |          |           |            |                 |          |          |            |            |
| 05:00 PM   | 2                         | 0        | 1        | 3          | 2               | 0        | 6         | 8          | 4                            | 0        | 5         | 9          | 10              | 0        | 0        | 10         | 30         |
| 05:15 PM   | 0                         | 0        | 1        | 1          | 1               | 0        | 1         | 2          | 4                            | 0        | 7         | 11         | 6               | 0        | 1        | 7          | 21         |
| 05:30 PM   | 1                         | 0        | 1        | 2          | 1               | 0        | 8         | 9          | 6                            | 0        | 2         | 8          | 5               | 0        | 2        | 7          | 26         |
| 05:45 PM   | 1                         | 0        | 1        | 2          | 1               | 0        | 3         | 4          | 4                            | 0        | 4         | 8          | 5               | 0        | 1        | 6          | 20         |
| <b>Total Volume</b>  | <b>4</b>                  | <b>0</b> | <b>4</b> | <b>8</b>   | <b>5</b>        | <b>0</b> | <b>18</b> | <b>23</b>  | <b>18</b>                    | <b>0</b> | <b>18</b> | <b>36</b>  | <b>26</b>       | <b>0</b> | <b>4</b> | <b>30</b>  | <b>97</b>  |
| % App. Total   | 50                        | 0        | 50       |            | 21.7            | 0        | 78.3      |            | 50                           | 0        | 50        |            | 86.7            | 0        | 13.3     |            |            |
| PHF  | .500                      | .000     | 1.00     | .667       | .625            | .000     | .563      | .639       | .750                         | .000     | .643      | .818       | .650            | .000     | .500     | .750       | .808       |

# Peggy Malone & Associates, Inc.

## (888) 247-8602

File Name : TM 4-Nicewander and US 11 PM  
 Site Code :  
 Start Date : 2/4/2015  
 Page No : 1

Groups Printed- Truck

| Start Time  | Nicewander Way Southbound |      |      |      |            | US 11 Westbound |      |      |      |            | Business Driveway Northbound |      |      |      |            | US 11 Eastbound |      |      |      |            | Int. Total |
|-------------|---------------------------|------|------|------|------------|-----------------|------|------|------|------------|------------------------------|------|------|------|------------|-----------------|------|------|------|------------|------------|
|             | Right                     | Thru | Left | Peds | App. Total | Right           | Thru | Left | Peds | App. Total | Right                        | Thru | Left | Peds | App. Total | Right           | Thru | Left | Peds | App. Total |            |
| 03:00 PM    | 0                         | 0    | 0    | 0    | 0          | 0               | 0    | 0    | 0    | 0          | 0                            | 0    | 0    | 0    | 0          | 0               | 0    | 0    | 0    | 0          | 0          |
| 03:15 PM    | 0                         | 0    | 0    | 0    | 0          | 0               | 0    | 0    | 0    | 0          | 0                            | 0    | 0    | 0    | 0          | 0               | 0    | 0    | 0    | 0          | 0          |
| 03:30 PM    | 0                         | 0    | 0    | 0    | 0          | 0               | 0    | 0    | 0    | 0          | 0                            | 0    | 0    | 0    | 0          | 0               | 0    | 0    | 0    | 0          | 0          |
| 03:45 PM    | 0                         | 0    | 0    | 0    | 0          | 0               | 0    | 0    | 0    | 0          | 0                            | 0    | 0    | 0    | 0          | 0               | 0    | 0    | 0    | 0          | 0          |
| Total       | 0                         | 0    | 0    | 0    | 0          | 0               | 0    | 0    | 0    | 0          | 0                            | 0    | 0    | 0    | 0          | 0               | 0    | 0    | 0    | 0          | 0          |
| 04:00 PM    | 0                         | 0    | 0    | 0    | 0          | 0               | 0    | 0    | 0    | 0          | 0                            | 0    | 0    | 0    | 0          | 0               | 0    | 0    | 0    | 0          | 0          |
| 04:15 PM    | 0                         | 0    | 0    | 0    | 0          | 0               | 0    | 0    | 0    | 0          | 0                            | 0    | 0    | 0    | 0          | 0               | 0    | 0    | 0    | 0          | 0          |
| 04:30 PM    | 0                         | 0    | 0    | 0    | 0          | 0               | 0    | 0    | 0    | 0          | 0                            | 0    | 0    | 0    | 0          | 0               | 0    | 0    | 0    | 0          | 0          |
| 04:45 PM    | 0                         | 0    | 0    | 0    | 0          | 0               | 0    | 0    | 0    | 0          | 0                            | 0    | 0    | 0    | 0          | 0               | 0    | 0    | 0    | 0          | 0          |
| Total       | 0                         | 0    | 0    | 0    | 0          | 0               | 0    | 0    | 0    | 0          | 0                            | 0    | 0    | 0    | 0          | 0               | 0    | 0    | 0    | 0          | 0          |
| 05:00 PM    | 0                         | 0    | 0    | 0    | 0          | 0               | 0    | 0    | 0    | 0          | 0                            | 0    | 0    | 0    | 0          | 0               | 0    | 0    | 0    | 0          | 0          |
| 05:15 PM    | 0                         | 0    | 0    | 0    | 0          | 0               | 0    | 0    | 0    | 0          | 0                            | 0    | 0    | 0    | 0          | 0               | 0    | 0    | 0    | 0          | 0          |
| 05:30 PM    | 0                         | 0    | 0    | 0    | 0          | 0               | 0    | 0    | 0    | 0          | 0                            | 0    | 0    | 0    | 0          | 0               | 0    | 0    | 0    | 0          | 0          |
| 05:45 PM    | 0                         | 0    | 0    | 0    | 0          | 0               | 0    | 0    | 0    | 0          | 0                            | 0    | 0    | 0    | 0          | 0               | 0    | 0    | 0    | 0          | 0          |
| Total       | 0                         | 0    | 0    | 0    | 0          | 0               | 0    | 0    | 0    | 0          | 0                            | 0    | 0    | 0    | 0          | 0               | 0    | 0    | 0    | 0          | 0          |
| Grand Total | 0                         | 0    | 0    | 0    | 0          | 0               | 0    | 0    | 0    | 0          | 0                            | 0    | 0    | 0    | 0          | 0               | 0    | 0    | 0    | 0          | 0          |
| Apprch %    | 0                         | 0    | 0    | 0    |            | 0               | 0    | 0    | 0    |            | 0                            | 0    | 0    | 0    |            | 0               | 0    | 0    | 0    |            |            |
| Total %     |                           |      |      |      |            |                 |      |      |      |            |                              |      |      |      |            |                 |      |      |      |            |            |

| Start Time   | Nicewander Way Southbound |      |      |            | US 11 Westbound |      |      |            | Business Driveway Northbound |      |      |            | US 11 Eastbound |      |      |            | Int. Total |
|--|---------------------------|------|------|------------|-----------------|------|------|------------|------------------------------|------|------|------------|-----------------|------|------|------------|------------|
|  | Right                     | Thru | Left | App. Total | Right           | Thru | Left | App. Total | Right                        | Thru | Left | App. Total | Right           | Thru | Left | App. Total |            |
| Peak Hour Analysis From 03:00 PM to 05:45 PM - Peak 1 of 1 |                           |      |      |            |                 |      |      |            |                              |      |      |            |                 |      |      |            |            |
| Peak Hour for Entire Intersection Begins at 03:00 PM       |                           |      |      |            |                 |      |      |            |                              |      |      |            |                 |      |      |            |            |
| 03:00 PM   | 0                         | 0    | 0    | 0          | 0               | 0    | 0    | 0          | 0                            | 0    | 0    | 0          | 0               | 0    | 0    | 0          | 0          |
| 03:15 PM   | 0                         | 0    | 0    | 0          | 0               | 0    | 0    | 0          | 0                            | 0    | 0    | 0          | 0               | 0    | 0    | 0          | 0          |
| 03:30 PM   | 0                         | 0    | 0    | 0          | 0               | 0    | 0    | 0          | 0                            | 0    | 0    | 0          | 0               | 0    | 0    | 0          | 0          |
| 03:45 PM   | 0                         | 0    | 0    | 0          | 0               | 0    | 0    | 0          | 0                            | 0    | 0    | 0          | 0               | 0    | 0    | 0          | 0          |
| Total Volume   | 0                         | 0    | 0    | 0          | 0               | 0    | 0    | 0          | 0                            | 0    | 0    | 0          | 0               | 0    | 0    | 0          | 0          |
| % App. Total   | 0                         | 0    | 0    |            | 0               | 0    | 0    |            | 0                            | 0    | 0    |            | 0               | 0    | 0    |            |            |
| PHF  | .000                      | .000 | .000 | .000       | .000            | .000 | .000 | .000       | .000                         | .000 | .000 | .000       | .000            | .000 | .000 | .000       | .000       |

# Peggy Malone & Associates, Inc.

## (888) 247-8602

File Name : TM 4-Nicewander and US 11 PM  
 Site Code :  
 Start Date : 2/4/2015  
 Page No : 1

Groups Printed- Combined

| Start Time         | Nicewander Way Southbound |          |           |          |            | US 11 Westbound |          |           |          |            | Business Driveway Northbound |          |           |          |            | US 11 Eastbound |          |           |          |            | Int. Total |
|--------------------|---------------------------|----------|-----------|----------|------------|-----------------|----------|-----------|----------|------------|------------------------------|----------|-----------|----------|------------|-----------------|----------|-----------|----------|------------|------------|
|                    | Right                     | Thru     | Left      | Peds     | App. Total | Right           | Thru     | Left      | Peds     | App. Total | Right                        | Thru     | Left      | Peds     | App. Total | Right           | Thru     | Left      | Peds     | App. Total |            |
| 03:00 PM           | 1                         | 0        | 1         | 0        | 2          | 1               | 0        | 3         | 0        | 4          | 0                            | 0        | 3         | 1        | 4          | 1               | 0        | 0         | 0        | 1          | 11         |
| 03:15 PM           | 2                         | 0        | 1         | 0        | 3          | 2               | 0        | 2         | 0        | 4          | 0                            | 0        | 5         | 0        | 5          | 5               | 0        | 2         | 0        | 7          | 19         |
| 03:30 PM           | 1                         | 0        | 0         | 0        | 1          | 0               | 0        | 1         | 0        | 1          | 6                            | 0        | 3         | 0        | 9          | 10              | 0        | 1         | 0        | 11         | 22         |
| 03:45 PM           | 0                         | 0        | 1         | 0        | 1          | 1               | 0        | 2         | 1        | 4          | 1                            | 0        | 7         | 0        | 8          | 2               | 0        | 0         | 0        | 2          | 15         |
| <b>Total</b>       | <b>4</b>                  | <b>0</b> | <b>3</b>  | <b>0</b> | <b>7</b>   | <b>4</b>        | <b>0</b> | <b>8</b>  | <b>1</b> | <b>13</b>  | <b>7</b>                     | <b>0</b> | <b>18</b> | <b>1</b> | <b>26</b>  | <b>18</b>       | <b>0</b> | <b>3</b>  | <b>0</b> | <b>21</b>  | <b>67</b>  |
| 04:00 PM           | 1                         | 0        | 4         | 1        | 6          | 2               | 0        | 3         | 0        | 5          | 2                            | 0        | 3         | 0        | 5          | 3               | 0        | 3         | 0        | 6          | 22         |
| 04:15 PM           | 2                         | 0        | 1         | 0        | 3          | 1               | 0        | 3         | 0        | 4          | 0                            | 0        | 7         | 0        | 7          | 9               | 0        | 0         | 0        | 9          | 23         |
| 04:30 PM           | 1                         | 0        | 0         | 0        | 1          | 1               | 0        | 5         | 0        | 6          | 0                            | 0        | 6         | 0        | 6          | 8               | 0        | 0         | 0        | 8          | 21         |
| 04:45 PM           | 0                         | 0        | 0         | 0        | 0          | 1               | 0        | 6         | 0        | 7          | 3                            | 0        | 3         | 1        | 7          | 6               | 0        | 0         | 0        | 6          | 20         |
| <b>Total</b>       | <b>4</b>                  | <b>0</b> | <b>5</b>  | <b>1</b> | <b>10</b>  | <b>5</b>        | <b>0</b> | <b>17</b> | <b>0</b> | <b>22</b>  | <b>5</b>                     | <b>0</b> | <b>19</b> | <b>1</b> | <b>25</b>  | <b>26</b>       | <b>0</b> | <b>3</b>  | <b>0</b> | <b>29</b>  | <b>86</b>  |
| 05:00 PM           | 2                         | 0        | 1         | 0        | 3          | 2               | 0        | 6         | 0        | 8          | 4                            | 0        | 5         | 2        | 11         | 10              | 0        | 0         | 0        | 10         | 32         |
| 05:15 PM           | 0                         | 0        | 1         | 0        | 1          | 1               | 0        | 1         | 0        | 2          | 4                            | 0        | 7         | 0        | 11         | 6               | 0        | 1         | 0        | 7          | 21         |
| 05:30 PM           | 1                         | 0        | 1         | 0        | 2          | 1               | 0        | 8         | 0        | 9          | 6                            | 0        | 2         | 0        | 8          | 5               | 0        | 2         | 0        | 7          | 26         |
| 05:45 PM           | 1                         | 0        | 1         | 0        | 2          | 1               | 0        | 3         | 1        | 5          | 4                            | 0        | 4         | 0        | 8          | 5               | 0        | 1         | 0        | 6          | 21         |
| <b>Total</b>       | <b>4</b>                  | <b>0</b> | <b>4</b>  | <b>0</b> | <b>8</b>   | <b>5</b>        | <b>0</b> | <b>18</b> | <b>1</b> | <b>24</b>  | <b>18</b>                    | <b>0</b> | <b>18</b> | <b>2</b> | <b>38</b>  | <b>26</b>       | <b>0</b> | <b>4</b>  | <b>0</b> | <b>30</b>  | <b>100</b> |
| <b>Grand Total</b> | <b>12</b>                 | <b>0</b> | <b>12</b> | <b>1</b> | <b>25</b>  | <b>14</b>       | <b>0</b> | <b>43</b> | <b>2</b> | <b>59</b>  | <b>30</b>                    | <b>0</b> | <b>55</b> | <b>4</b> | <b>89</b>  | <b>70</b>       | <b>0</b> | <b>10</b> | <b>0</b> | <b>80</b>  | <b>253</b> |
| Apprch %           | 48                        | 0        | 48        | 4        |            | 23.7            | 0        | 72.9      | 3.4      |            | 33.7                         | 0        | 61.8      | 4.5      |            | 87.5            | 0        | 12.5      | 0        |            |            |
| Total %            | 4.7                       | 0        | 4.7       | 0.4      | 9.9        | 5.5             | 0        | 17        | 0.8      | 23.3       | 11.9                         | 0        | 21.7      | 1.6      | 35.2       | 27.7            | 0        | 4         | 0        | 31.6       |            |

| Start Time   | Nicewander Way Southbound |          |          |            | US 11 Westbound |          |           |            | Business Driveway Northbound |          |           |            | US 11 Eastbound |          |          |            | Int. Total |
|--|---------------------------|----------|----------|------------|-----------------|----------|-----------|------------|------------------------------|----------|-----------|------------|-----------------|----------|----------|------------|------------|
|  | Right                     | Thru     | Left     | App. Total | Right           | Thru     | Left      | App. Total | Right                        | Thru     | Left      | App. Total | Right           | Thru     | Left     | App. Total |            |
| Peak Hour Analysis From 03:00 PM to 05:45 PM - Peak 1 of 1 |                           |          |          |            |                 |          |           |            |                              |          |           |            |                 |          |          |            |            |
| Peak Hour for Entire Intersection Begins at 05:00 PM       |                           |          |          |            |                 |          |           |            |                              |          |           |            |                 |          |          |            |            |
| 05:00 PM   | 2                         | 0        | 1        | 3          | 2               | 0        | 6         | 8          | 4                            | 0        | 5         | 9          | 10              | 0        | 0        | 10         | 30         |
| 05:15 PM   | 0                         | 0        | 1        | 1          | 1               | 0        | 1         | 2          | 4                            | 0        | 7         | 11         | 6               | 0        | 1        | 7          | 21         |
| 05:30 PM   | 1                         | 0        | 1        | 2          | 1               | 0        | 8         | 9          | 6                            | 0        | 2         | 8          | 5               | 0        | 2        | 7          | 26         |
| 05:45 PM   | 1                         | 0        | 1        | 2          | 1               | 0        | 3         | 4          | 4                            | 0        | 4         | 8          | 5               | 0        | 1        | 6          | 20         |
| <b>Total Volume</b>  | <b>4</b>                  | <b>0</b> | <b>4</b> | <b>8</b>   | <b>5</b>        | <b>0</b> | <b>18</b> | <b>23</b>  | <b>18</b>                    | <b>0</b> | <b>18</b> | <b>36</b>  | <b>26</b>       | <b>0</b> | <b>4</b> | <b>30</b>  | <b>97</b>  |
| % App. Total   | 50                        | 0        | 50       |            | 21.7            | 0        | 78.3      |            | 50                           | 0        | 50        |            | 86.7            | 0        | 13.3     |            |            |
| PHF  | .500                      | .000     | 1.00     | .667       | .625            | .000     | .563      | .639       | .750                         | .000     | .643      | .818       | .650            | .000     | .500     | .750       | .808       |

# Peggy Malone & Associates, Inc.

## (888) 247-8602

File Name : TM 5-Warren Newcomb and US 11 AM  
 Site Code :  
 Start Date : 2/4/2015  
 Page No : 1

Groups Printed- Car

| Start Time         | Warren Newcomb Dr. Southbound |          |           |          |            | US 11 Westbound |             |            |          |             | Kroger Driveway Northbound |          |            |          |            | US 11 Eastbound |             |           |          |             | Int. Total  |
|--------------------|-------------------------------|----------|-----------|----------|------------|-----------------|-------------|------------|----------|-------------|----------------------------|----------|------------|----------|------------|-----------------|-------------|-----------|----------|-------------|-------------|
|                    | Right                         | Thru     | Left      | Peds     | App. Total | Right           | Thru        | Left       | Peds     | App. Total  | Right                      | Thru     | Left       | Peds     | App. Total | Right           | Thru        | Left      | Peds     | App. Total  |             |
| 07:00 AM           | 7                             | 0        | 5         | 0        | 12         | 0               | 98          | 10         | 0        | 108         | 8                          | 0        | 6          | 0        | 14         | 23              | 102         | 5         | 0        | 130         | 264         |
| 07:15 AM           | 8                             | 0        | 10        | 0        | 18         | 0               | 126         | 9          | 0        | 135         | 16                         | 0        | 14         | 0        | 30         | 22              | 156         | 7         | 0        | 185         | 368         |
| 07:30 AM           | 10                            | 0        | 13        | 0        | 23         | 0               | 144         | 13         | 0        | 157         | 8                          | 0        | 13         | 0        | 21         | 21              | 204         | 8         | 0        | 233         | 434         |
| 07:45 AM           | 11                            | 1        | 10        | 0        | 22         | 2               | 138         | 10         | 0        | 150         | 17                         | 0        | 12         | 0        | 29         | 33              | 205         | 9         | 0        | 247         | 448         |
| <b>Total</b>       | <b>36</b>                     | <b>1</b> | <b>38</b> | <b>0</b> | <b>75</b>  | <b>2</b>        | <b>506</b>  | <b>42</b>  | <b>0</b> | <b>550</b>  | <b>49</b>                  | <b>0</b> | <b>45</b>  | <b>0</b> | <b>94</b>  | <b>99</b>       | <b>667</b>  | <b>29</b> | <b>0</b> | <b>795</b>  | <b>1514</b> |
| 08:00 AM           | 7                             | 0        | 7         | 0        | 14         | 1               | 125         | 7          | 0        | 133         | 16                         | 1        | 10         | 0        | 27         | 30              | 161         | 2         | 0        | 193         | 367         |
| 08:15 AM           | 14                            | 2        | 4         | 0        | 20         | 0               | 125         | 13         | 0        | 138         | 12                         | 0        | 20         | 0        | 32         | 19              | 173         | 5         | 0        | 197         | 387         |
| 08:30 AM           | 10                            | 0        | 9         | 0        | 19         | 1               | 146         | 17         | 0        | 164         | 15                         | 0        | 17         | 0        | 32         | 22              | 129         | 7         | 0        | 158         | 373         |
| 08:45 AM           | 8                             | 0        | 2         | 0        | 10         | 0               | 120         | 23         | 1        | 144         | 14                         | 0        | 22         | 0        | 36         | 25              | 154         | 9         | 0        | 188         | 378         |
| <b>Total</b>       | <b>39</b>                     | <b>2</b> | <b>22</b> | <b>0</b> | <b>63</b>  | <b>2</b>        | <b>516</b>  | <b>60</b>  | <b>1</b> | <b>579</b>  | <b>57</b>                  | <b>1</b> | <b>69</b>  | <b>0</b> | <b>127</b> | <b>96</b>       | <b>617</b>  | <b>23</b> | <b>0</b> | <b>736</b>  | <b>1505</b> |
| <b>Grand Total</b> | <b>75</b>                     | <b>3</b> | <b>60</b> | <b>0</b> | <b>138</b> | <b>4</b>        | <b>1022</b> | <b>102</b> | <b>1</b> | <b>1129</b> | <b>106</b>                 | <b>1</b> | <b>114</b> | <b>0</b> | <b>221</b> | <b>195</b>      | <b>1284</b> | <b>52</b> | <b>0</b> | <b>1531</b> | <b>3019</b> |
| Apprch %           | 54.3                          | 2.2      | 43.5      | 0        |            | 0.4             | 90.5        | 9          | 0.1      |             | 48                         | 0.5      | 51.6       | 0        |            | 12.7            | 83.9        | 3.4       | 0        |             |             |
| Total %            | 2.5                           | 0.1      | 2         | 0        | 4.6        | 0.1             | 33.9        | 3.4        | 0        | 37.4        | 3.5                        | 0        | 3.8        | 0        | 7.3        | 6.5             | 42.5        | 1.7       | 0        | 50.7        |             |

| Start Time   | Warren Newcomb Dr. Southbound |          |           |            | US 11 Westbound |            |           |            | Kroger Driveway Northbound |      |           |            | US 11 Eastbound |      |      |            | Int. Total |
|--|-------------------------------|----------|-----------|------------|-----------------|------------|-----------|------------|----------------------------|------|-----------|------------|-----------------|------|------|------------|------------|
|  | Right                         | Thru     | Left      | App. Total | Right           | Thru       | Left      | App. Total | Right                      | Thru | Left      | App. Total | Right           | Thru | Left | App. Total |            |
| Peak Hour Analysis From 07:00 AM to 08:45 AM - Peak 1 of 1 |                               |          |           |            |                 |            |           |            |                            |      |           |            |                 |      |      |            |            |
| Peak Hour for Entire Intersection Begins at 07:30 AM       |                               |          |           |            |                 |            |           |            |                            |      |           |            |                 |      |      |            |            |
| 07:30 AM   | 10                            | 0        | <b>13</b> | <b>23</b>  | 0               | <b>144</b> | <b>13</b> | <b>157</b> | 8                          | 0    | 13        | 21         | 21              | 204  | 8    | 233        | 434        |
| 07:45 AM   | 11                            | 1        | 10        | 22         | 2               | 138        | 10        | 150        | 17                         | 0    | 12        | 29         | 33              | 205  | 9    | 247        | 448        |
| 08:00 AM   | 7                             | 0        | 7         | 14         | 1               | 125        | 7         | 133        | 16                         | 1    | 10        | 27         | 30              | 161  | 2    | 193        | 367        |
| 08:15 AM   | <b>14</b>                     | <b>2</b> | 4         | 20         | 0               | 125        | 13        | 138        | 12                         | 0    | <b>20</b> | <b>32</b>  | 19              | 173  | 5    | 197        | 387        |
| Total Volume   | 42                            | 3        | 34        | 79         | 3               | 532        | 43        | 578        | 53                         | 1    | 55        | 109        | 103             | 743  | 24   | 870        | 1636       |
| % App. Total   | 53.2                          | 3.8      | 43        |            | 0.5             | 92         | 7.4       |            | 48.6                       | 0.9  | 50.5      |            | 11.8            | 85.4 | 2.8  |            |            |
| PHF  | .750                          | .375     | .654      | .859       | .375            | .924       | .827      | .920       | .779                       | .250 | .688      | .852       | .780            | .906 | .667 | .881       | .913       |

# Peggy Malone & Associates, Inc.

## (888) 247-8602

File Name : TM 5-Warren Newcomb and US 11 AM  
 Site Code :  
 Start Date : 2/4/2015  
 Page No : 1

Groups Printed- Truck

| Start Time         | Warren Newcomb Dr. Southbound |          |          |          |            | US 11 Westbound |           |          |          |            | Kroger Driveway Northbound |          |          |          |            | US 11 Eastbound |           |          |          |            | Int. Total |
|--------------------|-------------------------------|----------|----------|----------|------------|-----------------|-----------|----------|----------|------------|----------------------------|----------|----------|----------|------------|-----------------|-----------|----------|----------|------------|------------|
|                    | Right                         | Thru     | Left     | Peds     | App. Total | Right           | Thru      | Left     | Peds     | App. Total | Right                      | Thru     | Left     | Peds     | App. Total | Right           | Thru      | Left     | Peds     | App. Total |            |
| 07:00 AM           | 0                             | 0        | 0        | 0        | 0          | 0               | 0         | 0        | 0        | 0          | 1                          | 0        | 0        | 0        | 1          | 1               | 0         | 0        | 0        | 1          | 2          |
| 07:15 AM           | 0                             | 0        | 0        | 0        | 0          | 0               | 1         | 0        | 0        | 1          | 0                          | 0        | 1        | 0        | 1          | 1               | 2         | 0        | 0        | 3          | 5          |
| 07:30 AM           | 0                             | 0        | 0        | 0        | 0          | 0               | 2         | 1        | 0        | 3          | 0                          | 1        | 0        | 0        | 1          | 0               | 2         | 0        | 0        | 2          | 6          |
| 07:45 AM           | 2                             | 0        | 0        | 0        | 2          | 0               | 3         | 1        | 0        | 4          | 0                          | 0        | 1        | 0        | 1          | 1               | 3         | 1        | 0        | 5          | 12         |
| <b>Total</b>       | <b>2</b>                      | <b>0</b> | <b>0</b> | <b>0</b> | <b>2</b>   | <b>0</b>        | <b>6</b>  | <b>2</b> | <b>0</b> | <b>8</b>   | <b>1</b>                   | <b>1</b> | <b>2</b> | <b>0</b> | <b>4</b>   | <b>3</b>        | <b>7</b>  | <b>1</b> | <b>0</b> | <b>11</b>  | <b>25</b>  |
| 08:00 AM           | 1                             | 0        | 0        | 0        | 1          | 0               | 4         | 1        | 0        | 5          | 1                          | 0        | 1        | 0        | 2          | 1               | 3         | 0        | 0        | 4          | 12         |
| 08:15 AM           | 0                             | 0        | 0        | 0        | 0          | 0               | 4         | 1        | 0        | 5          | 0                          | 0        | 1        | 0        | 1          | 1               | 6         | 0        | 0        | 7          | 13         |
| 08:30 AM           | 1                             | 0        | 0        | 0        | 1          | 0               | 2         | 1        | 0        | 3          | 1                          | 0        | 0        | 0        | 1          | 2               | 2         | 0        | 0        | 4          | 9          |
| 08:45 AM           | 0                             | 0        | 0        | 0        | 0          | 0               | 4         | 0        | 0        | 4          | 1                          | 0        | 3        | 0        | 4          | 1               | 8         | 0        | 0        | 9          | 17         |
| <b>Total</b>       | <b>2</b>                      | <b>0</b> | <b>0</b> | <b>0</b> | <b>2</b>   | <b>0</b>        | <b>14</b> | <b>3</b> | <b>0</b> | <b>17</b>  | <b>3</b>                   | <b>0</b> | <b>5</b> | <b>0</b> | <b>8</b>   | <b>5</b>        | <b>19</b> | <b>0</b> | <b>0</b> | <b>24</b>  | <b>51</b>  |
| <b>Grand Total</b> | <b>4</b>                      | <b>0</b> | <b>0</b> | <b>0</b> | <b>4</b>   | <b>0</b>        | <b>20</b> | <b>5</b> | <b>0</b> | <b>25</b>  | <b>4</b>                   | <b>1</b> | <b>7</b> | <b>0</b> | <b>12</b>  | <b>8</b>        | <b>26</b> | <b>1</b> | <b>0</b> | <b>35</b>  | <b>76</b>  |
| Apprch %           | 100                           | 0        | 0        | 0        |            | 0               | 80        | 20       | 0        |            | 33.3                       | 8.3      | 58.3     | 0        |            | 22.9            | 74.3      | 2.9      | 0        |            |            |
| Total %            | 5.3                           | 0        | 0        | 0        | 5.3        | 0               | 26.3      | 6.6      | 0        | 32.9       | 5.3                        | 1.3      | 9.2      | 0        | 15.8       | 10.5            | 34.2      | 1.3      | 0        | 46.1       |            |

| Start Time   | Warren Newcomb Dr. Southbound |          |          |            | US 11 Westbound |           |          |            | Kroger Driveway Northbound |          |          |            | US 11 Eastbound |           |          |            | Int. Total |
|--|-------------------------------|----------|----------|------------|-----------------|-----------|----------|------------|----------------------------|----------|----------|------------|-----------------|-----------|----------|------------|------------|
|  | Right                         | Thru     | Left     | App. Total | Right           | Thru      | Left     | App. Total | Right                      | Thru     | Left     | App. Total | Right           | Thru      | Left     | App. Total |            |
| Peak Hour Analysis From 07:00 AM to 08:45 AM - Peak 1 of 1 |                               |          |          |            |                 |           |          |            |                            |          |          |            |                 |           |          |            |            |
| Peak Hour for Entire Intersection Begins at 08:00 AM       |                               |          |          |            |                 |           |          |            |                            |          |          |            |                 |           |          |            |            |
| 08:00 AM   | 1                             | 0        | 0        | 1          | 0               | 4         | 1        | 5          | 1                          | 0        | 1        | 2          | 1               | 3         | 0        | 4          | 12         |
| 08:15 AM   | 0                             | 0        | 0        | 0          | 0               | 4         | 1        | 5          | 0                          | 0        | 1        | 1          | 1               | 6         | 0        | 7          | 13         |
| 08:30 AM   | 1                             | 0        | 0        | 1          | 0               | 2         | 1        | 3          | 1                          | 0        | 0        | 1          | 2               | 2         | 0        | 4          | 9          |
| 08:45 AM   | 0                             | 0        | 0        | 0          | 0               | 4         | 0        | 4          | 1                          | 0        | 3        | 4          | 1               | 8         | 0        | 9          | 17         |
| <b>Total Volume</b>  | <b>2</b>                      | <b>0</b> | <b>0</b> | <b>2</b>   | <b>0</b>        | <b>14</b> | <b>3</b> | <b>17</b>  | <b>3</b>                   | <b>0</b> | <b>5</b> | <b>8</b>   | <b>5</b>        | <b>19</b> | <b>0</b> | <b>24</b>  | <b>51</b>  |
| % App. Total   | 100                           | 0        | 0        |            | 0               | 82.4      | 17.6     |            | 37.5                       | 0        | 62.5     |            | 20.8            | 79.2      | 0        |            |            |
| PHF  | .500                          | .000     | .000     | .500       | .000            | .875      | .750     | .850       | .750                       | .000     | .417     | .500       | .625            | .594      | .000     | .667       | .750       |

# Peggy Malone & Associates, Inc.

## (888) 247-8602

File Name : TM 5-Warren Newcomb and US 11 AM  
 Site Code :  
 Start Date : 2/4/2015  
 Page No : 1

Groups Printed- Combined

| Start Time         | Warren Newcomb Dr. Southbound |          |           |          |            | US 11 Westbound |             |            |          |             | Kroger Driveway Northbound |          |            |          |            | US 11 Eastbound |             |           |          |             | Int. Total  |
|--------------------|-------------------------------|----------|-----------|----------|------------|-----------------|-------------|------------|----------|-------------|----------------------------|----------|------------|----------|------------|-----------------|-------------|-----------|----------|-------------|-------------|
|                    | Right                         | Thru     | Left      | Peds     | App. Total | Right           | Thru        | Left       | Peds     | App. Total  | Right                      | Thru     | Left       | Peds     | App. Total | Right           | Thru        | Left      | Peds     | App. Total  |             |
| 07:00 AM           | 7                             | 0        | 5         | 0        | 12         | 0               | 98          | 10         | 0        | 108         | 9                          | 0        | 6          | 0        | 15         | 24              | 102         | 5         | 0        | 131         | 266         |
| 07:15 AM           | 8                             | 0        | 10        | 0        | 18         | 0               | 127         | 9          | 0        | 136         | 16                         | 0        | 15         | 0        | 31         | 23              | 158         | 7         | 0        | 188         | 373         |
| 07:30 AM           | 10                            | 0        | 13        | 0        | 23         | 0               | 146         | 14         | 0        | 160         | 8                          | 1        | 13         | 0        | 22         | 21              | 206         | 8         | 0        | 235         | 440         |
| 07:45 AM           | 13                            | 1        | 10        | 0        | 24         | 2               | 141         | 11         | 0        | 154         | 17                         | 0        | 13         | 0        | 30         | 34              | 208         | 10        | 0        | 252         | 460         |
| <b>Total</b>       | <b>38</b>                     | <b>1</b> | <b>38</b> | <b>0</b> | <b>77</b>  | <b>2</b>        | <b>512</b>  | <b>44</b>  | <b>0</b> | <b>558</b>  | <b>50</b>                  | <b>1</b> | <b>47</b>  | <b>0</b> | <b>98</b>  | <b>102</b>      | <b>674</b>  | <b>30</b> | <b>0</b> | <b>806</b>  | <b>1539</b> |
| 08:00 AM           | 8                             | 0        | 7         | 0        | 15         | 1               | 129         | 8          | 0        | 138         | 17                         | 1        | 11         | 0        | 29         | 31              | 164         | 2         | 0        | 197         | 379         |
| 08:15 AM           | 14                            | 2        | 4         | 0        | 20         | 0               | 129         | 14         | 0        | 143         | 12                         | 0        | 21         | 0        | 33         | 20              | 179         | 5         | 0        | 204         | 400         |
| 08:30 AM           | 11                            | 0        | 9         | 0        | 20         | 1               | 148         | 18         | 0        | 167         | 16                         | 0        | 17         | 0        | 33         | 24              | 131         | 7         | 0        | 162         | 382         |
| 08:45 AM           | 8                             | 0        | 2         | 0        | 10         | 0               | 124         | 23         | 1        | 148         | 15                         | 0        | 25         | 0        | 40         | 26              | 162         | 9         | 0        | 197         | 395         |
| <b>Total</b>       | <b>41</b>                     | <b>2</b> | <b>22</b> | <b>0</b> | <b>65</b>  | <b>2</b>        | <b>530</b>  | <b>63</b>  | <b>1</b> | <b>596</b>  | <b>60</b>                  | <b>1</b> | <b>74</b>  | <b>0</b> | <b>135</b> | <b>101</b>      | <b>636</b>  | <b>23</b> | <b>0</b> | <b>760</b>  | <b>1556</b> |
| <b>Grand Total</b> | <b>79</b>                     | <b>3</b> | <b>60</b> | <b>0</b> | <b>142</b> | <b>4</b>        | <b>1042</b> | <b>107</b> | <b>1</b> | <b>1154</b> | <b>110</b>                 | <b>2</b> | <b>121</b> | <b>0</b> | <b>233</b> | <b>203</b>      | <b>1310</b> | <b>53</b> | <b>0</b> | <b>1566</b> | <b>3095</b> |
| Apprch %           | 55.6                          | 2.1      | 42.3      | 0        |            | 0.3             | 90.3        | 9.3        | 0.1      |             | 47.2                       | 0.9      | 51.9       | 0        |            | 13              | 83.7        | 3.4       | 0        |             |             |
| Total %            | 2.6                           | 0.1      | 1.9       | 0        | 4.6        | 0.1             | 33.7        | 3.5        | 0        | 37.3        | 3.6                        | 0.1      | 3.9        | 0        | 7.5        | 6.6             | 42.3        | 1.7       | 0        | 50.6        |             |

| Start Time   | Warren Newcomb Dr. Southbound |          |           |            | US 11 Westbound |            |           |            | Kroger Driveway Northbound |          |           |            | US 11 Eastbound |            |           |            | Int. Total |
|--|-------------------------------|----------|-----------|------------|-----------------|------------|-----------|------------|----------------------------|----------|-----------|------------|-----------------|------------|-----------|------------|------------|
|  | Right                         | Thru     | Left      | App. Total | Right           | Thru       | Left      | App. Total | Right                      | Thru     | Left      | App. Total | Right           | Thru       | Left      | App. Total |            |
| Peak Hour Analysis From 07:00 AM to 08:45 AM - Peak 1 of 1 |                               |          |           |            |                 |            |           |            |                            |          |           |            |                 |            |           |            |            |
| Peak Hour for Entire Intersection Begins at 07:30 AM       |                               |          |           |            |                 |            |           |            |                            |          |           |            |                 |            |           |            |            |
| 07:30 AM   | 10                            | 0        | <b>13</b> | 23         | 0               | <b>146</b> | <b>14</b> | <b>160</b> | 8                          | <b>1</b> | 13        | 22         | 21              | 206        | 8         | 235        | 440        |
| 07:45 AM   | 13                            | 1        | 10        | <b>24</b>  | <b>2</b>        | 141        | 11        | 154        | <b>17</b>                  | 0        | 13        | 30         | <b>34</b>       | <b>208</b> | <b>10</b> | <b>252</b> | <b>460</b> |
| 08:00 AM   | 8                             | 0        | 7         | 15         | 1               | 129        | 8         | 138        | 17                         | 1        | 11        | 29         | 31              | 164        | 2         | 197        | 379        |
| 08:15 AM   | <b>14</b>                     | <b>2</b> | 4         | 20         | 0               | 129        | 14        | 143        | 12                         | 0        | <b>21</b> | <b>33</b>  | 20              | 179        | 5         | 204        | 400        |
| <b>Total Volume</b>  | 45                            | 3        | 34        | 82         | 3               | 545        | 47        | 595        | 54                         | 2        | 58        | 114        | 106             | 757        | 25        | 888        | 1679       |
| % App. Total   | 54.9                          | 3.7      | 41.5      |            | 0.5             | 91.6       | 7.9       |            | 47.4                       | 1.8      | 50.9      |            | 11.9            | 85.2       | 2.8       |            |            |
| PHF  | .804                          | .375     | .654      | .854       | .375            | .933       | .839      | .930       | .794                       | .500     | .690      | .864       | .779            | .910       | .625      | .881       | .913       |



# Peggy Malone & Associates, Inc.

## (888) 247-8602

File Name : TM 5-Warren Newcomb and US 11 MID  
 Site Code :  
 Start Date : 2/4/2015  
 Page No : 1

Groups Printed- Car

| Start Time         | Warren Newcomb Dr. Southbound |           |           |          |            | US 11 Westbound |             |            |          |             | Kroger Driveway Northbound |           |            |          |            | US 11 Eastbound |             |           |          |             | Int. Total  |
|--------------------|-------------------------------|-----------|-----------|----------|------------|-----------------|-------------|------------|----------|-------------|----------------------------|-----------|------------|----------|------------|-----------------|-------------|-----------|----------|-------------|-------------|
|                    | Right                         | Thru      | Left      | Peds     | App. Total | Right           | Thru        | Left       | Peds     | App. Total  | Right                      | Thru      | Left       | Peds     | App. Total | Right           | Thru        | Left      | Peds     | App. Total  |             |
| 11:00 AM           | 12                            | 2         | 10        | 0        | 24         | 0               | 157         | 16         | 0        | 173         | 17                         | 5         | 31         | 0        | 53         | 29              | 136         | 6         | 0        | 171         | 421         |
| 11:15 AM           | 18                            | 2         | 7         | 0        | 27         | 0               | 184         | 21         | 0        | 205         | 31                         | 2         | 21         | 0        | 54         | 38              | 154         | 8         | 0        | 200         | 486         |
| 11:30 AM           | 14                            | 0         | 6         | 0        | 20         | 0               | 156         | 16         | 0        | 172         | 35                         | 4         | 27         | 0        | 66         | 26              | 152         | 6         | 0        | 184         | 442         |
| 11:45 AM           | 13                            | 3         | 15        | 0        | 31         | 2               | 206         | 17         | 0        | 225         | 22                         | 4         | 29         | 0        | 55         | 24              | 145         | 13        | 0        | 182         | 493         |
| <b>Total</b>       | <b>57</b>                     | <b>7</b>  | <b>38</b> | <b>0</b> | <b>102</b> | <b>2</b>        | <b>703</b>  | <b>70</b>  | <b>0</b> | <b>775</b>  | <b>105</b>                 | <b>15</b> | <b>108</b> | <b>0</b> | <b>228</b> | <b>117</b>      | <b>587</b>  | <b>33</b> | <b>0</b> | <b>737</b>  | <b>1842</b> |
| 12:00 PM           | 13                            | 2         | 5         | 0        | 20         | 0               | 210         | 24         | 0        | 234         | 23                         | 1         | 28         | 0        | 52         | 32              | 186         | 8         | 0        | 226         | 532         |
| 12:15 PM           | 26                            | 5         | 11        | 0        | 42         | 0               | 200         | 23         | 0        | 223         | 33                         | 1         | 22         | 0        | 56         | 30              | 209         | 8         | 0        | 247         | 568         |
| 12:30 PM           | 16                            | 2         | 12        | 0        | 30         | 0               | 161         | 15         | 0        | 176         | 25                         | 1         | 24         | 0        | 50         | 43              | 203         | 9         | 0        | 255         | 511         |
| 12:45 PM           | 18                            | 1         | 8         | 0        | 27         | 0               | 183         | 19         | 0        | 202         | 24                         | 2         | 26         | 1        | 53         | 22              | 205         | 7         | 0        | 234         | 516         |
| <b>Total</b>       | <b>73</b>                     | <b>10</b> | <b>36</b> | <b>0</b> | <b>119</b> | <b>0</b>        | <b>754</b>  | <b>81</b>  | <b>0</b> | <b>835</b>  | <b>105</b>                 | <b>5</b>  | <b>100</b> | <b>1</b> | <b>211</b> | <b>127</b>      | <b>803</b>  | <b>32</b> | <b>0</b> | <b>962</b>  | <b>2127</b> |
| <b>Grand Total</b> | <b>130</b>                    | <b>17</b> | <b>74</b> | <b>0</b> | <b>221</b> | <b>2</b>        | <b>1457</b> | <b>151</b> | <b>0</b> | <b>1610</b> | <b>210</b>                 | <b>20</b> | <b>208</b> | <b>1</b> | <b>439</b> | <b>244</b>      | <b>1390</b> | <b>65</b> | <b>0</b> | <b>1699</b> | <b>3969</b> |
| Apprch %           | 58.8                          | 7.7       | 33.5      | 0        |            | 0.1             | 90.5        | 9.4        | 0        |             | 47.8                       | 4.6       | 47.4       | 0.2      |            | 14.4            | 81.8        | 3.8       | 0        |             |             |
| Total %            | 3.3                           | 0.4       | 1.9       | 0        | 5.6        | 0.1             | 36.7        | 3.8        | 0        | 40.6        | 5.3                        | 0.5       | 5.2        | 0        | 11.1       | 6.1             | 35          | 1.6       | 0        | 42.8        |             |

| Start Time   | Warren Newcomb Dr. Southbound |           |           |            | US 11 Westbound |            |           |            | Kroger Driveway Northbound |          |            |            | US 11 Eastbound |            |           |            | Int. Total  |
|--|-------------------------------|-----------|-----------|------------|-----------------|------------|-----------|------------|----------------------------|----------|------------|------------|-----------------|------------|-----------|------------|-------------|
|  | Right                         | Thru      | Left      | App. Total | Right           | Thru       | Left      | App. Total | Right                      | Thru     | Left       | App. Total | Right           | Thru       | Left      | App. Total |             |
| Peak Hour Analysis From 11:00 AM to 12:45 PM - Peak 1 of 1 |                               |           |           |            |                 |            |           |            |                            |          |            |            |                 |            |           |            |             |
| Peak Hour for Entire Intersection Begins at 12:00 PM       |                               |           |           |            |                 |            |           |            |                            |          |            |            |                 |            |           |            |             |
| 12:00 PM   | 13                            | 2         | 5         | 20         | 0               | <b>210</b> | <b>24</b> | <b>234</b> | 23                         | 1        | <b>28</b>  | 52         | 32              | 186        | 8         | 226        | 532         |
| 12:15 PM   | <b>26</b>                     | <b>5</b>  | 11        | <b>42</b>  | 0               | 200        | 23        | 223        | <b>33</b>                  | 1        | 22         | <b>56</b>  | 30              | <b>209</b> | 8         | 247        | <b>568</b>  |
| 12:30 PM   | 16                            | 2         | <b>12</b> | 30         | 0               | 161        | 15        | 176        | 25                         | 1        | 24         | 50         | <b>43</b>       | 203        | <b>9</b>  | <b>255</b> | 511         |
| 12:45 PM   | 18                            | 1         | 8         | 27         | 0               | 183        | 19        | 202        | 24                         | <b>2</b> | 26         | 52         | 22              | 205        | 7         | 234        | 515         |
| <b>Total Volume</b>  | <b>73</b>                     | <b>10</b> | <b>36</b> | <b>119</b> | <b>0</b>        | <b>754</b> | <b>81</b> | <b>835</b> | <b>105</b>                 | <b>5</b> | <b>100</b> | <b>210</b> | <b>127</b>      | <b>803</b> | <b>32</b> | <b>962</b> | <b>2126</b> |
| % App. Total   | 61.3                          | 8.4       | 30.3      |            | 0               | 90.3       | 9.7       |            | 50                         | 2.4      | 47.6       |            | 13.2            | 83.5       | 3.3       |            |             |
| PHF  | .702                          | .500      | .750      | .708       | .000            | .898       | .844      | .892       | .795                       | .625     | .893       | .938       | .738            | .961       | .889      | .943       | .936        |

# Peggy Malone & Associates, Inc.

## (888) 247-8602

File Name : TM 5-Warren Newcomb and US 11 MID  
 Site Code :  
 Start Date : 2/4/2015  
 Page No : 1

Groups Printed- Truck

| Start Time         | Warren Newcomb Dr. Southbound |          |          |          |            | US 11 Westbound |           |          |          |            | Kroger Driveway Northbound |          |          |          |            | US 11 Eastbound |           |          |          |            | Int. Total |
|--------------------|-------------------------------|----------|----------|----------|------------|-----------------|-----------|----------|----------|------------|----------------------------|----------|----------|----------|------------|-----------------|-----------|----------|----------|------------|------------|
|                    | Right                         | Thru     | Left     | Peds     | App. Total | Right           | Thru      | Left     | Peds     | App. Total | Right                      | Thru     | Left     | Peds     | App. Total | Right           | Thru      | Left     | Peds     | App. Total |            |
| 11:00 AM           | 0                             | 0        | 0        | 0        | 0          | 0               | 5         | 0        | 0        | 5          | 3                          | 0        | 0        | 0        | 3          | 2               | 3         | 0        | 0        | 5          | 13         |
| 11:15 AM           | 1                             | 0        | 0        | 0        | 1          | 0               | 2         | 1        | 0        | 3          | 0                          | 0        | 1        | 0        | 1          | 0               | 3         | 0        | 0        | 3          | 8          |
| 11:30 AM           | 0                             | 0        | 0        | 0        | 0          | 0               | 8         | 0        | 0        | 8          | 0                          | 0        | 1        | 0        | 1          | 0               | 3         | 0        | 0        | 3          | 12         |
| 11:45 AM           | 0                             | 0        | 0        | 0        | 0          | 0               | 3         | 2        | 0        | 5          | 0                          | 0        | 1        | 0        | 1          | 0               | 8         | 0        | 0        | 8          | 14         |
| <b>Total</b>       | <b>1</b>                      | <b>0</b> | <b>0</b> | <b>0</b> | <b>1</b>   | <b>0</b>        | <b>18</b> | <b>3</b> | <b>0</b> | <b>21</b>  | <b>3</b>                   | <b>0</b> | <b>3</b> | <b>0</b> | <b>6</b>   | <b>2</b>        | <b>17</b> | <b>0</b> | <b>0</b> | <b>19</b>  | <b>47</b>  |
| 12:00 PM           | 0                             | 0        | 0        | 0        | 0          | 0               | 2         | 1        | 0        | 3          | 1                          | 0        | 0        | 0        | 1          | 1               | 4         | 0        | 0        | 5          | 9          |
| 12:15 PM           | 0                             | 0        | 0        | 0        | 0          | 0               | 2         | 0        | 0        | 2          | 1                          | 0        | 0        | 0        | 1          | 0               | 5         | 0        | 0        | 5          | 8          |
| 12:30 PM           | 0                             | 0        | 0        | 0        | 0          | 1               | 3         | 1        | 0        | 5          | 1                          | 0        | 0        | 0        | 1          | 1               | 3         | 0        | 0        | 4          | 10         |
| 12:45 PM           | 1                             | 0        | 0        | 0        | 1          | 0               | 4         | 1        | 0        | 5          | 1                          | 0        | 1        | 0        | 2          | 0               | 6         | 0        | 0        | 6          | 14         |
| <b>Total</b>       | <b>1</b>                      | <b>0</b> | <b>0</b> | <b>0</b> | <b>1</b>   | <b>1</b>        | <b>11</b> | <b>3</b> | <b>0</b> | <b>15</b>  | <b>4</b>                   | <b>0</b> | <b>1</b> | <b>0</b> | <b>5</b>   | <b>2</b>        | <b>18</b> | <b>0</b> | <b>0</b> | <b>20</b>  | <b>41</b>  |
| <b>Grand Total</b> | <b>2</b>                      | <b>0</b> | <b>0</b> | <b>0</b> | <b>2</b>   | <b>1</b>        | <b>29</b> | <b>6</b> | <b>0</b> | <b>36</b>  | <b>7</b>                   | <b>0</b> | <b>4</b> | <b>0</b> | <b>11</b>  | <b>4</b>        | <b>35</b> | <b>0</b> | <b>0</b> | <b>39</b>  | <b>88</b>  |
| Apprch %           | 100                           | 0        | 0        | 0        |            | 2.8             | 80.6      | 16.7     | 0        |            | 63.6                       | 0        | 36.4     | 0        |            | 10.3            | 89.7      | 0        | 0        |            |            |
| Total %            | 2.3                           | 0        | 0        | 0        | 2.3        | 1.1             | 33        | 6.8      | 0        | 40.9       | 8                          | 0        | 4.5      | 0        | 12.5       | 4.5             | 39.8      | 0        | 0        | 44.3       |            |

| Start Time   | Warren Newcomb Dr. Southbound |          |          |            | US 11 Westbound |           |          |            | Kroger Driveway Northbound |          |          |            | US 11 Eastbound |           |          |            | Int. Total |
|--|-------------------------------|----------|----------|------------|-----------------|-----------|----------|------------|----------------------------|----------|----------|------------|-----------------|-----------|----------|------------|------------|
|  | Right                         | Thru     | Left     | App. Total | Right           | Thru      | Left     | App. Total | Right                      | Thru     | Left     | App. Total | Right           | Thru      | Left     | App. Total |            |
| Peak Hour Analysis From 11:00 AM to 12:45 PM - Peak 1 of 1 |                               |          |          |            |                 |           |          |            |                            |          |          |            |                 |           |          |            |            |
| Peak Hour for Entire Intersection Begins at 11:00 AM       |                               |          |          |            |                 |           |          |            |                            |          |          |            |                 |           |          |            |            |
| 11:00 AM   | 0                             | 0        | 0        | 0          | 0               | 5         | 0        | 5          | 3                          | 0        | 0        | 3          | 2               | 3         | 0        | 5          | 13         |
| 11:15 AM   | 1                             | 0        | 0        | 1          | 0               | 2         | 1        | 3          | 0                          | 0        | 1        | 1          | 0               | 3         | 0        | 3          | 8          |
| 11:30 AM   | 0                             | 0        | 0        | 0          | 0               | 8         | 0        | 8          | 0                          | 0        | 1        | 1          | 0               | 3         | 0        | 3          | 12         |
| 11:45 AM   | 0                             | 0        | 0        | 0          | 0               | 3         | 2        | 5          | 0                          | 0        | 1        | 1          | 0               | 8         | 0        | 8          | 14         |
| <b>Total Volume</b>  | <b>1</b>                      | <b>0</b> | <b>0</b> | <b>1</b>   | <b>0</b>        | <b>18</b> | <b>3</b> | <b>21</b>  | <b>3</b>                   | <b>0</b> | <b>3</b> | <b>6</b>   | <b>2</b>        | <b>17</b> | <b>0</b> | <b>19</b>  | <b>47</b>  |
| % App. Total   | 100                           | 0        | 0        |            | 0               | 85.7      | 14.3     |            | 50                         | 0        | 50       |            | 10.5            | 89.5      | 0        |            |            |
| PHF  | .250                          | .000     | .000     | .250       | .000            | .563      | .375     | .656       | .250                       | .000     | .750     | .500       | .250            | .531      | .000     | .594       | .839       |

# Peggy Malone & Associates, Inc.

## (888) 247-8602

File Name : TM 5-Warren Newcomb and US 11 MID  
 Site Code :  
 Start Date : 2/4/2015  
 Page No : 1

Groups Printed- Combined

| Start Time         | Warren Newcomb Dr. Southbound |           |           |          |            | US 11 Westbound |             |            |          |             | Kroger Driveway Northbound |           |            |          |            | US 11 Eastbound |             |           |          |             | Int. Total  |
|--------------------|-------------------------------|-----------|-----------|----------|------------|-----------------|-------------|------------|----------|-------------|----------------------------|-----------|------------|----------|------------|-----------------|-------------|-----------|----------|-------------|-------------|
|                    | Right                         | Thru      | Left      | Peds     | App. Total | Right           | Thru        | Left       | Peds     | App. Total  | Right                      | Thru      | Left       | Peds     | App. Total | Right           | Thru        | Left      | Peds     | App. Total  |             |
| 11:00 AM           | 12                            | 2         | 10        | 0        | 24         | 0               | 162         | 16         | 0        | 178         | 20                         | 5         | 31         | 0        | 56         | 31              | 139         | 6         | 0        | 176         | 434         |
| 11:15 AM           | 19                            | 2         | 7         | 0        | 28         | 0               | 186         | 22         | 0        | 208         | 31                         | 2         | 22         | 0        | 55         | 38              | 157         | 8         | 0        | 203         | 494         |
| 11:30 AM           | 14                            | 0         | 6         | 0        | 20         | 0               | 164         | 16         | 0        | 180         | 35                         | 4         | 28         | 0        | 67         | 26              | 155         | 6         | 0        | 187         | 454         |
| 11:45 AM           | 13                            | 3         | 15        | 0        | 31         | 2               | 209         | 19         | 0        | 230         | 22                         | 4         | 30         | 0        | 56         | 24              | 153         | 13        | 0        | 190         | 507         |
| <b>Total</b>       | <b>58</b>                     | <b>7</b>  | <b>38</b> | <b>0</b> | <b>103</b> | <b>2</b>        | <b>721</b>  | <b>73</b>  | <b>0</b> | <b>796</b>  | <b>108</b>                 | <b>15</b> | <b>111</b> | <b>0</b> | <b>234</b> | <b>119</b>      | <b>604</b>  | <b>33</b> | <b>0</b> | <b>756</b>  | <b>1889</b> |
| 12:00 PM           | 13                            | 2         | 5         | 0        | 20         | 0               | 212         | 25         | 0        | 237         | 24                         | 1         | 28         | 0        | 53         | 33              | 190         | 8         | 0        | 231         | 541         |
| 12:15 PM           | 26                            | 5         | 11        | 0        | 42         | 0               | 202         | 23         | 0        | 225         | 34                         | 1         | 22         | 0        | 57         | 30              | 214         | 8         | 0        | 252         | 576         |
| 12:30 PM           | 16                            | 2         | 12        | 0        | 30         | 1               | 164         | 16         | 0        | 181         | 26                         | 1         | 24         | 0        | 51         | 44              | 206         | 9         | 0        | 259         | 521         |
| 12:45 PM           | 19                            | 1         | 8         | 0        | 28         | 0               | 187         | 20         | 0        | 207         | 25                         | 2         | 27         | 1        | 55         | 22              | 211         | 7         | 0        | 240         | 530         |
| <b>Total</b>       | <b>74</b>                     | <b>10</b> | <b>36</b> | <b>0</b> | <b>120</b> | <b>1</b>        | <b>765</b>  | <b>84</b>  | <b>0</b> | <b>850</b>  | <b>109</b>                 | <b>5</b>  | <b>101</b> | <b>1</b> | <b>216</b> | <b>129</b>      | <b>821</b>  | <b>32</b> | <b>0</b> | <b>982</b>  | <b>2168</b> |
| <b>Grand Total</b> | <b>132</b>                    | <b>17</b> | <b>74</b> | <b>0</b> | <b>223</b> | <b>3</b>        | <b>1486</b> | <b>157</b> | <b>0</b> | <b>1646</b> | <b>217</b>                 | <b>20</b> | <b>212</b> | <b>1</b> | <b>450</b> | <b>248</b>      | <b>1425</b> | <b>65</b> | <b>0</b> | <b>1738</b> | <b>4057</b> |
| Apprch %           | 59.2                          | 7.6       | 33.2      | 0        |            | 0.2             | 90.3        | 9.5        | 0        |             | 48.2                       | 4.4       | 47.1       | 0.2      |            | 14.3            | 82          | 3.7       | 0        |             |             |
| Total %            | 3.3                           | 0.4       | 1.8       | 0        | 5.5        | 0.1             | 36.6        | 3.9        | 0        | 40.6        | 5.3                        | 0.5       | 5.2        | 0        | 11.1       | 6.1             | 35.1        | 1.6       | 0        | 42.8        |             |

| Start Time   | Warren Newcomb Dr. Southbound |          |           |            | US 11 Westbound |            |           |            | Kroger Driveway Northbound |          |           |            | US 11 Eastbound |            |          |            | Int. Total |
|--|-------------------------------|----------|-----------|------------|-----------------|------------|-----------|------------|----------------------------|----------|-----------|------------|-----------------|------------|----------|------------|------------|
|  | Right                         | Thru     | Left      | App. Total | Right           | Thru       | Left      | App. Total | Right                      | Thru     | Left      | App. Total | Right           | Thru       | Left     | App. Total |            |
| Peak Hour Analysis From 11:00 AM to 12:45 PM - Peak 1 of 1 |                               |          |           |            |                 |            |           |            |                            |          |           |            |                 |            |          |            |            |
| Peak Hour for Entire Intersection Begins at 12:00 PM       |                               |          |           |            |                 |            |           |            |                            |          |           |            |                 |            |          |            |            |
| 12:00 PM   | 13                            | 2        | 5         | 20         | 0               | <b>212</b> | <b>25</b> | <b>237</b> | 24                         | 1        | <b>28</b> | 53         | 33              | 190        | 8        | 231        | 541        |
| 12:15 PM   | <b>26</b>                     | <b>5</b> | 11        | <b>42</b>  | 0               | 202        | 23        | 225        | <b>34</b>                  | 1        | 22        | <b>57</b>  | 30              | <b>214</b> | 8        | 252        | <b>576</b> |
| 12:30 PM   | 16                            | 2        | <b>12</b> | 30         | <b>1</b>        | 164        | 16        | 181        | 26                         | 1        | 24        | 51         | <b>44</b>       | 206        | <b>9</b> | <b>259</b> | 521        |
| 12:45 PM   | 19                            | 1        | 8         | 28         | 0               | 187        | 20        | 207        | 25                         | <b>2</b> | 27        | 54         | 22              | 211        | 7        | 240        | 529        |
| <b>Total Volume</b>  | 74                            | 10       | 36        | 120        | 1               | 765        | 84        | 850        | 109                        | 5        | 101       | 215        | 129             | 821        | 32       | 982        | 2167       |
| % App. Total   | 61.7                          | 8.3      | 30        |            | 0.1             | 90         | 9.9       |            | 50.7                       | 2.3      | 47        |            | 13.1            | 83.6       | 3.3      |            |            |
| PHF  | .712                          | .500     | .750      | .714       | .250            | .902       | .840      | .897       | .801                       | .625     | .902      | .943       | .733            | .959       | .889     | .948       | .941       |

# Peggy Malone & Associates, Inc.

## (888) 247-8602

File Name : TM 5-Warren Newcomb and US 11 PM  
 Site Code :  
 Start Date : 2/4/2015  
 Page No : 1

Groups Printed- Car

| Start Time         | Warren Newcomb Dr. Southbound |           |            |          |            | US 11 Westbound |             |            |          |             | Kroger Driveway Northbound |           |            |          |            | US 11 Eastbound |             |           |          |             | Int. Total  |
|--------------------|-------------------------------|-----------|------------|----------|------------|-----------------|-------------|------------|----------|-------------|----------------------------|-----------|------------|----------|------------|-----------------|-------------|-----------|----------|-------------|-------------|
|                    | Right                         | Thru      | Left       | Peds     | App. Total | Right           | Thru        | Left       | Peds     | App. Total  | Right                      | Thru      | Left       | Peds     | App. Total | Right           | Thru        | Left      | Peds     | App. Total  |             |
| 03:00 PM           | 23                            | 3         | 7          | 0        | 33         | 1               | 222         | 25         | 0        | 248         | 35                         | 0         | 18         | 0        | 53         | 30              | 214         | 7         | 0        | 251         | 585         |
| 03:15 PM           | 14                            | 1         | 4          | 0        | 19         | 0               | 189         | 27         | 0        | 216         | 29                         | 3         | 39         | 0        | 71         | 35              | 218         | 2         | 0        | 255         | 561         |
| 03:30 PM           | 22                            | 2         | 9          | 0        | 33         | 4               | 293         | 21         | 0        | 318         | 20                         | 1         | 30         | 0        | 51         | 22              | 231         | 5         | 0        | 258         | 660         |
| 03:45 PM           | 18                            | 3         | 7          | 0        | 28         | 1               | 230         | 38         | 0        | 269         | 42                         | 3         | 25         | 1        | 71         | 39              | 203         | 10        | 0        | 252         | 620         |
| <b>Total</b>       | <b>77</b>                     | <b>9</b>  | <b>27</b>  | <b>0</b> | <b>113</b> | <b>6</b>        | <b>934</b>  | <b>111</b> | <b>0</b> | <b>1051</b> | <b>126</b>                 | <b>7</b>  | <b>112</b> | <b>1</b> | <b>246</b> | <b>126</b>      | <b>866</b>  | <b>24</b> | <b>0</b> | <b>1016</b> | <b>2426</b> |
| 04:00 PM           | 18                            | 1         | 7          | 0        | 26         | 0               | 263         | 25         | 0        | 288         | 31                         | 2         | 43         | 0        | 76         | 33              | 229         | 6         | 0        | 268         | 658         |
| 04:15 PM           | 15                            | 2         | 13         | 0        | 30         | 0               | 206         | 27         | 0        | 233         | 47                         | 2         | 26         | 0        | 75         | 39              | 196         | 11        | 0        | 246         | 584         |
| 04:30 PM           | 19                            | 1         | 8          | 0        | 28         | 0               | 257         | 39         | 0        | 296         | 45                         | 2         | 45         | 0        | 92         | 44              | 221         | 8         | 0        | 273         | 689         |
| 04:45 PM           | 14                            | 1         | 9          | 0        | 24         | 2               | 286         | 30         | 0        | 318         | 38                         | 0         | 32         | 0        | 70         | 32              | 226         | 6         | 0        | 264         | 676         |
| <b>Total</b>       | <b>66</b>                     | <b>5</b>  | <b>37</b>  | <b>0</b> | <b>108</b> | <b>2</b>        | <b>1012</b> | <b>121</b> | <b>0</b> | <b>1135</b> | <b>161</b>                 | <b>6</b>  | <b>146</b> | <b>0</b> | <b>313</b> | <b>148</b>      | <b>872</b>  | <b>31</b> | <b>0</b> | <b>1051</b> | <b>2607</b> |
| 05:00 PM           | 13                            | 3         | 12         | 0        | 28         | 4               | 299         | 24         | 0        | 327         | 45                         | 1         | 47         | 0        | 93         | 25              | 254         | 7         | 0        | 286         | 734         |
| 05:15 PM           | 19                            | 2         | 6          | 0        | 27         | 0               | 337         | 32         | 0        | 369         | 30                         | 3         | 26         | 1        | 60         | 29              | 210         | 8         | 0        | 247         | 703         |
| 05:30 PM           | 18                            | 0         | 12         | 0        | 30         | 0               | 234         | 24         | 1        | 259         | 33                         | 0         | 33         | 0        | 66         | 50              | 255         | 5         | 0        | 310         | 665         |
| 05:45 PM           | 6                             | 3         | 8          | 0        | 17         | 2               | 201         | 31         | 0        | 234         | 46                         | 3         | 33         | 0        | 82         | 35              | 198         | 10        | 0        | 243         | 576         |
| <b>Total</b>       | <b>56</b>                     | <b>8</b>  | <b>38</b>  | <b>0</b> | <b>102</b> | <b>6</b>        | <b>1071</b> | <b>111</b> | <b>1</b> | <b>1189</b> | <b>154</b>                 | <b>7</b>  | <b>139</b> | <b>1</b> | <b>301</b> | <b>139</b>      | <b>917</b>  | <b>30</b> | <b>0</b> | <b>1086</b> | <b>2678</b> |
| <b>Grand Total</b> | <b>199</b>                    | <b>22</b> | <b>102</b> | <b>0</b> | <b>323</b> | <b>14</b>       | <b>3017</b> | <b>343</b> | <b>1</b> | <b>3375</b> | <b>441</b>                 | <b>20</b> | <b>397</b> | <b>2</b> | <b>860</b> | <b>413</b>      | <b>2655</b> | <b>85</b> | <b>0</b> | <b>3153</b> | <b>7711</b> |
| Apprch %           | 61.6                          | 6.8       | 31.6       | 0        |            | 0.4             | 89.4        | 10.2       | 0        |             | 51.3                       | 2.3       | 46.2       | 0.2      |            | 13.1            | 84.2        | 2.7       | 0        |             |             |
| Total %            | 2.6                           | 0.3       | 1.3        | 0        | 4.2        | 0.2             | 39.1        | 4.4        | 0        | 43.8        | 5.7                        | 0.3       | 5.1        | 0        | 11.2       | 5.4             | 34.4        | 1.1       | 0        | 40.9        |             |

| Start Time   | Warren Newcomb Dr. Southbound |          |           |            | US 11 Westbound |            |           |            | Kroger Driveway Northbound |          |           |            | US 11 Eastbound |            |          |            | Int. Total |
|--|-------------------------------|----------|-----------|------------|-----------------|------------|-----------|------------|----------------------------|----------|-----------|------------|-----------------|------------|----------|------------|------------|
|  | Right                         | Thru     | Left      | App. Total | Right           | Thru       | Left      | App. Total | Right                      | Thru     | Left      | App. Total | Right           | Thru       | Left     | App. Total |            |
| Peak Hour Analysis From 03:00 PM to 05:45 PM - Peak 1 of 1 |                               |          |           |            |                 |            |           |            |                            |          |           |            |                 |            |          |            |            |
| Peak Hour for Entire Intersection Begins at 04:30 PM       |                               |          |           |            |                 |            |           |            |                            |          |           |            |                 |            |          |            |            |
| 04:30 PM   | <b>19</b>                     | 1        | 8         | <b>28</b>  | 0               | 257        | <b>39</b> | 296        | <b>45</b>                  | 2        | 45        | 92         | <b>44</b>       | 221        | <b>8</b> | 273        | 689        |
| 04:45 PM   | 14                            | 1        | 9         | 24         | 2               | 286        | 30        | 318        | 38                         | 0        | 32        | 70         | 32              | 226        | 6        | 264        | 676        |
| 05:00 PM   | 13                            | <b>3</b> | <b>12</b> | <b>28</b>  | <b>4</b>        | 299        | 24        | 327        | 45                         | 1        | <b>47</b> | <b>93</b>  | 25              | <b>254</b> | 7        | <b>286</b> | <b>734</b> |
| 05:15 PM   | 19                            | 2        | 6         | 27         | 0               | <b>337</b> | 32        | <b>369</b> | 30                         | <b>3</b> | 26        | 59         | 29              | 210        | 8        | 247        | 702        |
| Total Volume   | 65                            | 7        | 35        | 107        | 6               | 1179       | 125       | 1310       | 158                        | 6        | 150       | 314        | 130             | 911        | 29       | 1070       | 2801       |
| % App. Total   | 60.7                          | 6.5      | 32.7      |            | 0.5             | 90         | 9.5       |            | 50.3                       | 1.9      | 47.8      |            | 12.1            | 85.1       | 2.7      |            |            |
| PHF  | .855                          | .583     | .729      | .955       | .375            | .875       | .801      | .888       | .878                       | .500     | .798      | .844       | .739            | .897       | .906     | .935       | .954       |

# Peggy Malone & Associates, Inc.

## (888) 247-8602

File Name : TM 5-Warren Newcomb and US 11 PM  
 Site Code :  
 Start Date : 2/4/2015  
 Page No : 1

Groups Printed- Truck

| Start Time         | Warren Newcomb Dr. Southbound |          |          |          |            | US 11 Westbound |           |          |          |            | Kroger Driveway Northbound |          |          |          |            | US 11 Eastbound |           |          |          |            | Int. Total |
|--------------------|-------------------------------|----------|----------|----------|------------|-----------------|-----------|----------|----------|------------|----------------------------|----------|----------|----------|------------|-----------------|-----------|----------|----------|------------|------------|
|                    | Right                         | Thru     | Left     | Peds     | App. Total | Right           | Thru      | Left     | Peds     | App. Total | Right                      | Thru     | Left     | Peds     | App. Total | Right           | Thru      | Left     | Peds     | App. Total |            |
| 03:00 PM           | 0                             | 0        | 0        | 0        | 0          | 0               | 8         | 0        | 0        | 8          | 1                          | 0        | 0        | 0        | 1          | 2               | 7         | 0        | 0        | 9          | 18         |
| 03:15 PM           | 0                             | 0        | 0        | 0        | 0          | 1               | 4         | 0        | 0        | 5          | 0                          | 0        | 2        | 0        | 2          | 1               | 4         | 0        | 0        | 5          | 12         |
| 03:30 PM           | 1                             | 0        | 1        | 0        | 2          | 0               | 5         | 0        | 0        | 5          | 0                          | 0        | 0        | 0        | 0          | 1               | 7         | 0        | 0        | 8          | 15         |
| 03:45 PM           | 0                             | 0        | 0        | 0        | 0          | 0               | 2         | 1        | 0        | 3          | 0                          | 0        | 3        | 0        | 3          | 2               | 2         | 0        | 0        | 4          | 10         |
| <b>Total</b>       | <b>1</b>                      | <b>0</b> | <b>1</b> | <b>0</b> | <b>2</b>   | <b>1</b>        | <b>19</b> | <b>1</b> | <b>0</b> | <b>21</b>  | <b>1</b>                   | <b>0</b> | <b>5</b> | <b>0</b> | <b>6</b>   | <b>6</b>        | <b>20</b> | <b>0</b> | <b>0</b> | <b>26</b>  | <b>55</b>  |
| 04:00 PM           | 0                             | 0        | 0        | 0        | 0          | 0               | 2         | 0        | 0        | 2          | 1                          | 0        | 0        | 0        | 1          | 1               | 5         | 0        | 0        | 6          | 9          |
| 04:15 PM           | 0                             | 0        | 0        | 0        | 0          | 0               | 2         | 0        | 0        | 2          | 0                          | 0        | 0        | 0        | 0          | 0               | 5         | 0        | 0        | 5          | 7          |
| 04:30 PM           | 0                             | 0        | 1        | 0        | 1          | 0               | 2         | 1        | 0        | 3          | 0                          | 0        | 1        | 0        | 1          | 1               | 4         | 0        | 0        | 5          | 10         |
| 04:45 PM           | 0                             | 0        | 0        | 0        | 0          | 0               | 3         | 1        | 0        | 4          | 0                          | 0        | 1        | 0        | 1          | 0               | 1         | 0        | 0        | 1          | 6          |
| <b>Total</b>       | <b>0</b>                      | <b>0</b> | <b>1</b> | <b>0</b> | <b>1</b>   | <b>0</b>        | <b>9</b>  | <b>2</b> | <b>0</b> | <b>11</b>  | <b>1</b>                   | <b>0</b> | <b>2</b> | <b>0</b> | <b>3</b>   | <b>2</b>        | <b>15</b> | <b>0</b> | <b>0</b> | <b>17</b>  | <b>32</b>  |
| 05:00 PM           | 0                             | 0        | 0        | 0        | 0          | 0               | 2         | 0        | 0        | 2          | 1                          | 0        | 0        | 0        | 1          | 1               | 4         | 0        | 0        | 5          | 8          |
| 05:15 PM           | 0                             | 0        | 0        | 0        | 0          | 0               | 1         | 0        | 0        | 1          | 0                          | 0        | 0        | 0        | 0          | 0               | 2         | 0        | 0        | 2          | 3          |
| 05:30 PM           | 0                             | 0        | 0        | 0        | 0          | 0               | 0         | 0        | 0        | 0          | 0                          | 0        | 0        | 0        | 0          | 0               | 1         | 0        | 0        | 1          | 1          |
| 05:45 PM           | 0                             | 0        | 0        | 0        | 0          | 0               | 4         | 1        | 0        | 5          | 0                          | 0        | 1        | 0        | 1          | 0               | 0         | 0        | 0        | 0          | 6          |
| <b>Total</b>       | <b>0</b>                      | <b>0</b> | <b>0</b> | <b>0</b> | <b>0</b>   | <b>0</b>        | <b>7</b>  | <b>1</b> | <b>0</b> | <b>8</b>   | <b>1</b>                   | <b>0</b> | <b>1</b> | <b>0</b> | <b>2</b>   | <b>1</b>        | <b>7</b>  | <b>0</b> | <b>0</b> | <b>8</b>   | <b>18</b>  |
| <b>Grand Total</b> | <b>1</b>                      | <b>0</b> | <b>2</b> | <b>0</b> | <b>3</b>   | <b>1</b>        | <b>35</b> | <b>4</b> | <b>0</b> | <b>40</b>  | <b>3</b>                   | <b>0</b> | <b>8</b> | <b>0</b> | <b>11</b>  | <b>9</b>        | <b>42</b> | <b>0</b> | <b>0</b> | <b>51</b>  | <b>105</b> |
| Apprch %           | 33.3                          | 0        | 66.7     | 0        |            | 2.5             | 87.5      | 10       | 0        |            | 27.3                       | 0        | 72.7     | 0        |            | 17.6            | 82.4      | 0        | 0        |            |            |
| Total %            | 1                             | 0        | 1.9      | 0        | 2.9        | 1               | 33.3      | 3.8      | 0        | 38.1       | 2.9                        | 0        | 7.6      | 0        | 10.5       | 8.6             | 40        | 0        | 0        | 48.6       |            |

| Start Time   | Warren Newcomb Dr. Southbound |          |          |            | US 11 Westbound |           |          |            | Kroger Driveway Northbound |          |          |            | US 11 Eastbound |           |          |            | Int. Total |
|--|-------------------------------|----------|----------|------------|-----------------|-----------|----------|------------|----------------------------|----------|----------|------------|-----------------|-----------|----------|------------|------------|
|  | Right                         | Thru     | Left     | App. Total | Right           | Thru      | Left     | App. Total | Right                      | Thru     | Left     | App. Total | Right           | Thru      | Left     | App. Total |            |
| Peak Hour Analysis From 03:00 PM to 05:45 PM - Peak 1 of 1 |                               |          |          |            |                 |           |          |            |                            |          |          |            |                 |           |          |            |            |
| Peak Hour for Entire Intersection Begins at 03:00 PM       |                               |          |          |            |                 |           |          |            |                            |          |          |            |                 |           |          |            |            |
| 03:00 PM   | 0                             | 0        | 0        | 0          | 0               | 8         | 0        | 8          | 1                          | 0        | 0        | 1          | 2               | 7         | 0        | 9          | 18         |
| 03:15 PM   | 0                             | 0        | 0        | 0          | 1               | 4         | 0        | 5          | 0                          | 0        | 2        | 2          | 1               | 4         | 0        | 5          | 12         |
| 03:30 PM   | 1                             | 0        | 1        | 2          | 0               | 5         | 0        | 5          | 0                          | 0        | 0        | 0          | 1               | 7         | 0        | 8          | 15         |
| 03:45 PM   | 0                             | 0        | 0        | 0          | 0               | 2         | 1        | 3          | 0                          | 0        | 3        | 3          | 2               | 2         | 0        | 4          | 10         |
| <b>Total Volume</b>  | <b>1</b>                      | <b>0</b> | <b>1</b> | <b>2</b>   | <b>1</b>        | <b>19</b> | <b>1</b> | <b>21</b>  | <b>1</b>                   | <b>0</b> | <b>5</b> | <b>6</b>   | <b>6</b>        | <b>20</b> | <b>0</b> | <b>26</b>  | <b>55</b>  |
| % App. Total   | 50                            | 0        | 50       |            | 4.8             | 90.5      | 4.8      |            | 16.7                       | 0        | 83.3     |            | 23.1            | 76.9      | 0        |            |            |
| PHF  | .250                          | .000     | .250     | .250       | .250            | .594      | .250     | .656       | .250                       | .000     | .417     | .500       | .750            | .714      | .000     | .722       | .764       |

# Peggy Malone & Associates, Inc.

## (888) 247-8602

File Name : TM 5-Warren Newcomb and US 11 PM

Site Code :

Start Date : 2/4/2015

Page No : 1

Groups Printed- Combined

| Start Time         | Warren Newcomb Dr. Southbound |           |            |          |            | US 11 Westbound |             |            |          |             | Kroger Driveway Northbound |           |            |          |            | US 11 Eastbound |             |           |          |             | Int. Total  |
|--------------------|-------------------------------|-----------|------------|----------|------------|-----------------|-------------|------------|----------|-------------|----------------------------|-----------|------------|----------|------------|-----------------|-------------|-----------|----------|-------------|-------------|
|                    | Right                         | Thru      | Left       | Peds     | App. Total | Right           | Thru        | Left       | Peds     | App. Total  | Right                      | Thru      | Left       | Peds     | App. Total | Right           | Thru        | Left      | Peds     | App. Total  |             |
| 03:00 PM           | 23                            | 3         | 7          | 0        | 33         | 1               | 230         | 25         | 0        | 256         | 36                         | 0         | 18         | 0        | 54         | 32              | 221         | 7         | 0        | 260         | 603         |
| 03:15 PM           | 14                            | 1         | 4          | 0        | 19         | 1               | 193         | 27         | 0        | 221         | 29                         | 3         | 41         | 0        | 73         | 36              | 222         | 2         | 0        | 260         | 573         |
| 03:30 PM           | 23                            | 2         | 10         | 0        | 35         | 4               | 298         | 21         | 0        | 323         | 20                         | 1         | 30         | 0        | 51         | 23              | 238         | 5         | 0        | 266         | 675         |
| 03:45 PM           | 18                            | 3         | 7          | 0        | 28         | 1               | 232         | 39         | 0        | 272         | 42                         | 3         | 28         | 1        | 74         | 41              | 205         | 10        | 0        | 256         | 630         |
| <b>Total</b>       | <b>78</b>                     | <b>9</b>  | <b>28</b>  | <b>0</b> | <b>115</b> | <b>7</b>        | <b>953</b>  | <b>112</b> | <b>0</b> | <b>1072</b> | <b>127</b>                 | <b>7</b>  | <b>117</b> | <b>1</b> | <b>252</b> | <b>132</b>      | <b>886</b>  | <b>24</b> | <b>0</b> | <b>1042</b> | <b>2481</b> |
| 04:00 PM           | 18                            | 1         | 7          | 0        | 26         | 0               | 265         | 25         | 0        | 290         | 32                         | 2         | 43         | 0        | 77         | 34              | 234         | 6         | 0        | 274         | 667         |
| 04:15 PM           | 15                            | 2         | 13         | 0        | 30         | 0               | 208         | 27         | 0        | 235         | 47                         | 2         | 26         | 0        | 75         | 39              | 201         | 11        | 0        | 251         | 591         |
| 04:30 PM           | 19                            | 1         | 9          | 0        | 29         | 0               | 259         | 40         | 0        | 299         | 45                         | 2         | 46         | 0        | 93         | 45              | 225         | 8         | 0        | 278         | 699         |
| 04:45 PM           | 14                            | 1         | 9          | 0        | 24         | 2               | 289         | 31         | 0        | 322         | 38                         | 0         | 33         | 0        | 71         | 32              | 227         | 6         | 0        | 265         | 682         |
| <b>Total</b>       | <b>66</b>                     | <b>5</b>  | <b>38</b>  | <b>0</b> | <b>109</b> | <b>2</b>        | <b>1021</b> | <b>123</b> | <b>0</b> | <b>1146</b> | <b>162</b>                 | <b>6</b>  | <b>148</b> | <b>0</b> | <b>316</b> | <b>150</b>      | <b>887</b>  | <b>31</b> | <b>0</b> | <b>1068</b> | <b>2639</b> |
| 05:00 PM           | 13                            | 3         | 12         | 0        | 28         | 4               | 301         | 24         | 0        | 329         | 46                         | 1         | 47         | 0        | 94         | 26              | 258         | 7         | 0        | 291         | 742         |
| 05:15 PM           | 19                            | 2         | 6          | 0        | 27         | 0               | 338         | 32         | 0        | 370         | 30                         | 3         | 26         | 1        | 60         | 29              | 212         | 8         | 0        | 249         | 706         |
| 05:30 PM           | 18                            | 0         | 12         | 0        | 30         | 0               | 234         | 24         | 1        | 259         | 33                         | 0         | 33         | 0        | 66         | 50              | 256         | 5         | 0        | 311         | 666         |
| 05:45 PM           | 6                             | 3         | 8          | 0        | 17         | 2               | 205         | 32         | 0        | 239         | 46                         | 3         | 34         | 0        | 83         | 35              | 198         | 10        | 0        | 243         | 582         |
| <b>Total</b>       | <b>56</b>                     | <b>8</b>  | <b>38</b>  | <b>0</b> | <b>102</b> | <b>6</b>        | <b>1078</b> | <b>112</b> | <b>1</b> | <b>1197</b> | <b>155</b>                 | <b>7</b>  | <b>140</b> | <b>1</b> | <b>303</b> | <b>140</b>      | <b>924</b>  | <b>30</b> | <b>0</b> | <b>1094</b> | <b>2696</b> |
| <b>Grand Total</b> | <b>200</b>                    | <b>22</b> | <b>104</b> | <b>0</b> | <b>326</b> | <b>15</b>       | <b>3052</b> | <b>347</b> | <b>1</b> | <b>3415</b> | <b>444</b>                 | <b>20</b> | <b>405</b> | <b>2</b> | <b>871</b> | <b>422</b>      | <b>2697</b> | <b>85</b> | <b>0</b> | <b>3204</b> | <b>7816</b> |
| Apprch %           | 61.3                          | 6.7       | 31.9       | 0        |            | 0.4             | 89.4        | 10.2       | 0        |             | 5.1                        | 2.3       | 46.5       | 0.2      |            | 13.2            | 84.2        | 2.7       | 0        |             |             |
| Total %            | 2.6                           | 0.3       | 1.3        | 0        | 4.2        | 0.2             | 39          | 4.4        | 0        | 43.7        | 5.7                        | 0.3       | 5.2        | 0        | 11.1       | 5.4             | 34.5        | 1.1       | 0        | 4.1         |             |

| Start Time   | Warren Newcomb Dr. Southbound |          |           |            | US 11 Westbound |            |           |            | Kroger Driveway Northbound |          |           |            | US 11 Eastbound |            |          |            | Int. Total |
|--|-------------------------------|----------|-----------|------------|-----------------|------------|-----------|------------|----------------------------|----------|-----------|------------|-----------------|------------|----------|------------|------------|
|  | Right                         | Thru     | Left      | App. Total | Right           | Thru       | Left      | App. Total | Right                      | Thru     | Left      | App. Total | Right           | Thru       | Left     | App. Total |            |
| Peak Hour Analysis From 03:00 PM to 05:45 PM - Peak 1 of 1 |                               |          |           |            |                 |            |           |            |                            |          |           |            |                 |            |          |            |            |
| Peak Hour for Entire Intersection Begins at 04:30 PM       |                               |          |           |            |                 |            |           |            |                            |          |           |            |                 |            |          |            |            |
| 04:30 PM   | <b>19</b>                     | 1        | 9         | <b>29</b>  | 0               | 259        | <b>40</b> | 299        | 45                         | 2        | 46        | 93         | <b>45</b>       | 225        | <b>8</b> | 278        | 699        |
| 04:45 PM   | 14                            | 1        | 9         | 24         | 2               | 289        | 31        | 322        | 38                         | 0        | 33        | 71         | 32              | 227        | 6        | 265        | 682        |
| 05:00 PM   | 13                            | <b>3</b> | <b>12</b> | 28         | <b>4</b>        | 301        | 24        | 329        | <b>46</b>                  | 1        | <b>47</b> | <b>94</b>  | 26              | <b>258</b> | 7        | <b>291</b> | <b>742</b> |
| 05:15 PM   | 19                            | 2        | 6         | 27         | 0               | <b>338</b> | 32        | <b>370</b> | 30                         | <b>3</b> | 26        | 59         | 29              | 212        | 8        | 249        | 705        |
| Total Volume   | 65                            | 7        | 36        | 108        | 6               | 1187       | 127       | 1320       | 159                        | 6        | 152       | 317        | 132             | 922        | 29       | 1083       | 2828       |
| % App. Total   | 60.2                          | 6.5      | 33.3      |            | 0.5             | 89.9       | 9.6       |            | 50.2                       | 1.9      | 47.9      |            | 12.2            | 85.1       | 2.7      |            |            |
| PHF  | .855                          | .583     | .750      | .931       | .375            | .878       | .794      | .892       | .864                       | .500     | .809      | .843       | .733            | .893       | .906     | .930       | .953       |

# Peggy Malone & Associates, Inc.

## (888) 247-8602

File Name : TM 6-Kroger Rear Ent and US 11 AM  
 Site Code :  
 Start Date : 2/4/2015  
 Page No : 1

Groups Printed- Car

| Start Time         | US 11 Westbound |      |      |            | Kroger Rear Driveway Northbound |      |      |            | US 11 Eastbound |      |      |            | Int. Total |
|--------------------|-----------------|------|------|------------|---------------------------------|------|------|------------|-----------------|------|------|------------|------------|
|                    | Thru            | Left | Peds | App. Total | Right                           | Left | Peds | App. Total | Right           | Thru | Peds | App. Total |            |
| 07:00 AM           | 0               | 10   | 0    | 10         | 7                               | 0    | 0    | 7          | 0               | 0    | 0    | 0          | 17         |
| 07:15 AM           | 0               | 7    | 0    | 7          | 12                              | 0    | 0    | 12         | 3               | 0    | 0    | 3          | 22         |
| 07:30 AM           | 0               | 12   | 0    | 12         | 12                              | 0    | 0    | 12         | 1               | 0    | 0    | 1          | 25         |
| 07:45 AM           | 0               | 10   | 0    | 10         | 11                              | 0    | 0    | 11         | 1               | 0    | 0    | 1          | 22         |
| <b>Total</b>       | 0               | 39   | 0    | 39         | 42                              | 0    | 0    | 42         | 5               | 0    | 0    | 5          | 86         |
| 08:00 AM           | 0               | 8    | 0    | 8          | 17                              | 2    | 0    | 19         | 1               | 0    | 0    | 1          | 28         |
| 08:15 AM           | 0               | 10   | 0    | 10         | 13                              | 0    | 0    | 13         | 0               | 0    | 0    | 0          | 23         |
| 08:30 AM           | 0               | 8    | 0    | 8          | 15                              | 0    | 1    | 16         | 2               | 0    | 0    | 2          | 26         |
| 08:45 AM           | 0               | 11   | 0    | 11         | 8                               | 2    | 0    | 10         | 2               | 0    | 0    | 2          | 23         |
| <b>Total</b>       | 0               | 37   | 0    | 37         | 53                              | 4    | 1    | 58         | 5               | 0    | 0    | 5          | 100        |
| <b>Grand Total</b> | 0               | 76   | 0    | 76         | 95                              | 4    | 1    | 100        | 10              | 0    | 0    | 10         | 186        |
| Apprch %           | 0               | 100  | 0    |            | 95                              | 4    | 1    |            | 100             | 0    | 0    |            |            |
| Total %            | 0               | 40.9 | 0    | 40.9       | 51.1                            | 2.2  | 0.5  | 53.8       | 5.4             | 0    | 0    | 5.4        |            |

| Start Time   | US 11 Westbound |      |            | Kroger Rear Driveway Northbound |      |            | US 11 Eastbound |      |            | Int. Total |
|--|-----------------|------|------------|---------------------------------|------|------------|-----------------|------|------------|------------|
|  | Thru            | Left | App. Total | Right                           | Left | App. Total | Right           | Thru | App. Total |            |
| Peak Hour Analysis From 07:00 AM to 08:45 AM - Peak 1 of 1 |                 |      |            |                                 |      |            |                 |      |            |            |
| Peak Hour for Entire Intersection Begins at 08:00 AM       |                 |      |            |                                 |      |            |                 |      |            |            |
| 08:00 AM   | 0               | 8    | 8          | 17                              | 2    | 19         | 1               | 0    | 1          | 28         |
| 08:15 AM   | 0               | 10   | 10         | 13                              | 0    | 13         | 0               | 0    | 0          | 23         |
| 08:30 AM   | 0               | 8    | 8          | 15                              | 0    | 15         | 2               | 0    | 2          | 25         |
| 08:45 AM   | 0               | 11   | 11         | 8                               | 2    | 10         | 2               | 0    | 2          | 23         |
| <b>Total Volume</b>  | 0               | 37   | 37         | 53                              | 4    | 57         | 5               | 0    | 5          | 99         |
| <b>% App. Total</b>  | 0               | 100  |            | 93                              | 7    |            | 100             | 0    |            |            |
| <b>PHF</b>   | .000            | .841 | .841       | .779                            | .500 | .750       | .625            | .000 | .625       | .884       |

# Peggy Malone & Associates, Inc.

## (888) 247-8602

File Name : TM 6-Kroger Rear Ent and US 11 AM  
 Site Code :  
 Start Date : 2/4/2015  
 Page No : 1

Groups Printed- Truck

| Start Time         | US 11 Westbound |      |      |            | Kroger Rear Driveway Northbound |      |      |            | US 11 Eastbound |      |      |            | Int. Total |
|--------------------|-----------------|------|------|------------|---------------------------------|------|------|------------|-----------------|------|------|------------|------------|
|                    | Thru            | Left | Peds | App. Total | Right                           | Left | Peds | App. Total | Right           | Thru | Peds | App. Total |            |
| 07:00 AM           | 0               | 0    | 0    | 0          | 0                               | 0    | 0    | 0          | 0               | 0    | 0    | 0          | 0          |
| 07:15 AM           | 0               | 0    | 0    | 0          | 1                               | 0    | 0    | 1          | 0               | 0    | 0    | 0          | 1          |
| 07:30 AM           | 0               | 0    | 0    | 0          | 0                               | 0    | 0    | 0          | 1               | 0    | 0    | 1          | 1          |
| 07:45 AM           | 0               | 0    | 0    | 0          | 0                               | 1    | 0    | 1          | 0               | 0    | 0    | 0          | 1          |
| <b>Total</b>       | 0               | 0    | 0    | 0          | 1                               | 1    | 0    | 2          | 1               | 0    | 0    | 1          | 3          |
| 08:00 AM           | 0               | 0    | 0    | 0          | 0                               | 0    | 0    | 0          | 0               | 0    | 0    | 0          | 0          |
| 08:15 AM           | 0               | 1    | 0    | 1          | 0                               | 0    | 0    | 0          | 1               | 0    | 0    | 1          | 2          |
| 08:30 AM           | 0               | 0    | 0    | 0          | 0                               | 0    | 0    | 0          | 0               | 0    | 0    | 0          | 0          |
| 08:45 AM           | 0               | 0    | 0    | 0          | 1                               | 0    | 0    | 1          | 0               | 0    | 0    | 0          | 1          |
| <b>Total</b>       | 0               | 1    | 0    | 1          | 1                               | 0    | 0    | 1          | 1               | 0    | 0    | 1          | 3          |
| <b>Grand Total</b> | 0               | 1    | 0    | 1          | 2                               | 1    | 0    | 3          | 2               | 0    | 0    | 2          | 6          |
| Apprch %           | 0               | 100  | 0    |            | 66.7                            | 33.3 | 0    |            | 100             | 0    | 0    |            |            |
| Total %            | 0               | 16.7 | 0    | 16.7       | 33.3                            | 16.7 | 0    | 50         | 33.3            | 0    | 0    | 33.3       |            |

| Start Time   | US 11 Westbound |      |            | Kroger Rear Driveway Northbound |      |            | US 11 Eastbound |      |            | Int. Total |
|--|-----------------|------|------------|---------------------------------|------|------------|-----------------|------|------------|------------|
|  | Thru            | Left | App. Total | Right                           | Left | App. Total | Right           | Thru | App. Total |            |
| Peak Hour Analysis From 07:00 AM to 08:45 AM - Peak 1 of 1 |                 |      |            |                                 |      |            |                 |      |            |            |
| Peak Hour for Entire Intersection Begins at 07:30 AM       |                 |      |            |                                 |      |            |                 |      |            |            |
| 07:30 AM   | 0               | 0    | 0          | 0                               | 0    | 0          | 1               | 0    | 1          | 1          |
| 07:45 AM   | 0               | 0    | 0          | 0                               | 1    | 1          | 0               | 0    | 0          | 1          |
| 08:00 AM   | 0               | 0    | 0          | 0                               | 0    | 0          | 0               | 0    | 0          | 0          |
| 08:15 AM   | 0               | 1    | 1          | 0                               | 0    | 0          | 1               | 0    | 1          | 2          |
| <b>Total Volume</b>  | 0               | 1    | 1          | 0                               | 1    | 1          | 2               | 0    | 2          | 4          |
| <b>% App. Total</b>  | 0               | 100  |            | 0                               | 100  |            | 100             | 0    |            |            |
| <b>PHF</b>   | .000            | .250 | .250       | .000                            | .250 | .250       | .500            | .000 | .500       | .500       |



# Peggy Malone & Associates, Inc.

## (888) 247-8602

File Name : TM 6-Kroger Rear Ent and US 11 AM  
 Site Code :  
 Start Date : 2/4/2015  
 Page No : 1

Groups Printed- Combined

| Start Time         | US 11 Westbound |      |      |            | Kroger Rear Driveway Northbound |      |      |            | US 11 Eastbound |      |      |            | Int. Total |
|--------------------|-----------------|------|------|------------|---------------------------------|------|------|------------|-----------------|------|------|------------|------------|
|                    | Thru            | Left | Peds | App. Total | Right                           | Left | Peds | App. Total | Right           | Thru | Peds | App. Total |            |
| 07:00 AM           | 0               | 10   | 0    | 10         | 7                               | 0    | 0    | 7          | 0               | 0    | 0    | 0          | 17         |
| 07:15 AM           | 0               | 7    | 0    | 7          | 13                              | 0    | 0    | 13         | 3               | 0    | 0    | 3          | 23         |
| 07:30 AM           | 0               | 12   | 0    | 12         | 12                              | 0    | 0    | 12         | 2               | 0    | 0    | 2          | 26         |
| 07:45 AM           | 0               | 10   | 0    | 10         | 11                              | 1    | 0    | 12         | 1               | 0    | 0    | 1          | 23         |
| <b>Total</b>       | 0               | 39   | 0    | 39         | 43                              | 1    | 0    | 44         | 6               | 0    | 0    | 6          | 89         |
| 08:00 AM           | 0               | 8    | 0    | 8          | 17                              | 2    | 0    | 19         | 1               | 0    | 0    | 1          | 28         |
| 08:15 AM           | 0               | 11   | 0    | 11         | 13                              | 0    | 0    | 13         | 1               | 0    | 0    | 1          | 25         |
| 08:30 AM           | 0               | 8    | 0    | 8          | 15                              | 0    | 1    | 16         | 2               | 0    | 0    | 2          | 26         |
| 08:45 AM           | 0               | 11   | 0    | 11         | 9                               | 2    | 0    | 11         | 2               | 0    | 0    | 2          | 24         |
| <b>Total</b>       | 0               | 38   | 0    | 38         | 54                              | 4    | 1    | 59         | 6               | 0    | 0    | 6          | 103        |
| <b>Grand Total</b> | 0               | 77   | 0    | 77         | 97                              | 5    | 1    | 103        | 12              | 0    | 0    | 12         | 192        |
| Apprch %           | 0               | 100  | 0    |            | 94.2                            | 4.9  | 1    |            | 100             | 0    | 0    |            |            |
| Total %            | 0               | 40.1 | 0    | 40.1       | 50.5                            | 2.6  | 0.5  | 53.6       | 6.2             | 0    | 0    | 6.2        |            |

| Start Time   | US 11 Westbound |      |            | Kroger Rear Driveway Northbound |      |            | US 11 Eastbound |      |            | Int. Total |
|--|-----------------|------|------------|---------------------------------|------|------------|-----------------|------|------------|------------|
|  | Thru            | Left | App. Total | Right                           | Left | App. Total | Right           | Thru | App. Total |            |
| Peak Hour Analysis From 07:00 AM to 08:45 AM - Peak 1 of 1 |                 |      |            |                                 |      |            |                 |      |            |            |
| Peak Hour for Entire Intersection Begins at 07:30 AM       |                 |      |            |                                 |      |            |                 |      |            |            |
| 07:30 AM   | 0               | 12   | 12         | 12                              | 0    | 12         | 2               | 0    | 2          | 26         |
| 07:45 AM   | 0               | 10   | 10         | 11                              | 1    | 12         | 1               | 0    | 1          | 23         |
| 08:00 AM   | 0               | 8    | 8          | 17                              | 2    | 19         | 1               | 0    | 1          | 28         |
| 08:15 AM   | 0               | 11   | 11         | 13                              | 0    | 13         | 1               | 0    | 1          | 25         |
| <b>Total Volume</b>  | 0               | 41   | 41         | 53                              | 3    | 56         | 5               | 0    | 5          | 102        |
| <b>% App. Total</b>  | 0               | 100  |            | 94.6                            | 5.4  |            | 100             | 0    |            |            |
| <b>PHF</b>   | .000            | .854 | .854       | .779                            | .375 | .737       | .625            | .000 | .625       | .911       |

# Peggy Malone & Associates, Inc.

## (888) 247-8602

File Name : TM 6-Kroger Rear Ent and US 11 MID

Site Code :

Start Date : 2/4/2015

Page No : 1

### Groups Printed- Car

| Start Time         | US 11 Westbound |      |      |            | Kroger Rear Driveway Northbound |      |      |            | US 11 Eastbound |      |      |            | Int. Total |
|--------------------|-----------------|------|------|------------|---------------------------------|------|------|------------|-----------------|------|------|------------|------------|
|                    | Thru            | Left | Peds | App. Total | Right                           | Left | Peds | App. Total | Right           | Thru | Peds | App. Total |            |
| 11:00 AM           | 0               | 10   | 0    | 10         | 7                               | 1    | 0    | 8          | 0               | 0    | 0    | 0          | 18         |
| 11:15 AM           | 0               | 14   | 0    | 14         | 8                               | 0    | 0    | 8          | 1               | 0    | 0    | 1          | 23         |
| 11:30 AM           | 0               | 17   | 0    | 17         | 8                               | 0    | 0    | 8          | 2               | 0    | 0    | 2          | 27         |
| 11:45 AM           | 0               | 9    | 0    | 9          | 12                              | 1    | 0    | 13         | 0               | 0    | 0    | 0          | 22         |
| <b>Total</b>       | 0               | 50   | 0    | 50         | 35                              | 2    | 0    | 37         | 3               | 0    | 0    | 3          | 90         |
| 12:00 PM           | 0               | 10   | 0    | 10         | 10                              | 0    | 0    | 10         | 0               | 0    | 0    | 0          | 20         |
| 12:15 PM           | 0               | 9    | 0    | 9          | 9                               | 0    | 4    | 13         | 0               | 0    | 0    | 0          | 22         |
| 12:30 PM           | 0               | 11   | 0    | 11         | 21                              | 0    | 0    | 21         | 0               | 0    | 0    | 0          | 32         |
| 12:45 PM           | 0               | 8    | 0    | 8          | 16                              | 0    | 0    | 16         | 1               | 0    | 0    | 1          | 25         |
| <b>Total</b>       | 0               | 38   | 0    | 38         | 56                              | 0    | 4    | 60         | 1               | 0    | 0    | 1          | 99         |
| <b>Grand Total</b> | 0               | 88   | 0    | 88         | 91                              | 2    | 4    | 97         | 4               | 0    | 0    | 4          | 189        |
| Apprch %           | 0               | 100  | 0    |            | 93.8                            | 2.1  | 4.1  |            | 100             | 0    | 0    |            |            |
| Total %            | 0               | 46.6 | 0    | 46.6       | 48.1                            | 1.1  | 2.1  | 51.3       | 2.1             | 0    | 0    | 2.1        |            |

| Start Time   | US 11 Westbound |      |            | Kroger Rear Driveway Northbound |      |            | US 11 Eastbound |      |            | Int. Total |
|--|-----------------|------|------------|---------------------------------|------|------------|-----------------|------|------------|------------|
|  | Thru            | Left | App. Total | Right                           | Left | App. Total | Right           | Thru | App. Total |            |
| Peak Hour Analysis From 11:00 AM to 12:45 PM - Peak 1 of 1 |                 |      |            |                                 |      |            |                 |      |            |            |
| Peak Hour for Entire Intersection Begins at 12:00 PM       |                 |      |            |                                 |      |            |                 |      |            |            |
| 12:00 PM   | 0               | 10   | 10         | 10                              | 0    | 10         | 0               | 0    | 0          | 20         |
| 12:15 PM   | 0               | 9    | 9          | 9                               | 0    | 9          | 0               | 0    | 0          | 18         |
| 12:30 PM   | 0               | 11   | 11         | 21                              | 0    | 21         | 0               | 0    | 0          | 32         |
| 12:45 PM   | 0               | 8    | 8          | 16                              | 0    | 16         | 1               | 0    | 1          | 25         |
| <b>Total Volume</b>  | 0               | 38   | 38         | 56                              | 0    | 56         | 1               | 0    | 1          | 95         |
| <b>% App. Total</b>  | 0               | 100  |            | 100                             | 0    |            | 100             | 0    |            |            |
| PHF  | .000            | .864 | .864       | .667                            | .000 | .667       | .250            | .000 | .250       | .742       |



# Peggy Malone & Associates, Inc.

## (888) 247-8602

File Name : TM 6-Kroger Rear Ent and US 11 MID  
 Site Code :  
 Start Date : 2/4/2015  
 Page No : 1

Groups Printed- Combined

| Start Time         | US 11 Westbound |      |      |            | Kroger Rear Driveway Northbound |      |      |            | US 11 Eastbound |      |      |            | Int. Total |
|--------------------|-----------------|------|------|------------|---------------------------------|------|------|------------|-----------------|------|------|------------|------------|
|                    | Thru            | Left | Peds | App. Total | Right                           | Left | Peds | App. Total | Right           | Thru | Peds | App. Total |            |
| 11:00 AM           | 0               | 10   | 0    | 10         | 7                               | 1    | 0    | 8          | 0               | 0    | 0    | 0          | 18         |
| 11:15 AM           | 0               | 14   | 0    | 14         | 8                               | 0    | 0    | 8          | 1               | 0    | 0    | 1          | 23         |
| 11:30 AM           | 0               | 17   | 0    | 17         | 8                               | 0    | 0    | 8          | 2               | 0    | 0    | 2          | 27         |
| 11:45 AM           | 0               | 9    | 0    | 9          | 12                              | 1    | 0    | 13         | 0               | 0    | 0    | 0          | 22         |
| <b>Total</b>       | 0               | 50   | 0    | 50         | 35                              | 2    | 0    | 37         | 3               | 0    | 0    | 3          | 90         |
| 12:00 PM           | 0               | 10   | 0    | 10         | 10                              | 0    | 0    | 10         | 0               | 0    | 0    | 0          | 20         |
| 12:15 PM           | 0               | 10   | 0    | 10         | 9                               | 0    | 4    | 13         | 0               | 0    | 0    | 0          | 23         |
| 12:30 PM           | 0               | 11   | 0    | 11         | 21                              | 0    | 0    | 21         | 0               | 0    | 0    | 0          | 32         |
| 12:45 PM           | 0               | 8    | 0    | 8          | 16                              | 0    | 0    | 16         | 1               | 0    | 0    | 1          | 25         |
| <b>Total</b>       | 0               | 39   | 0    | 39         | 56                              | 0    | 4    | 60         | 1               | 0    | 0    | 1          | 100        |
| <b>Grand Total</b> | 0               | 89   | 0    | 89         | 91                              | 2    | 4    | 97         | 4               | 0    | 0    | 4          | 190        |
| Apprch %           | 0               | 100  | 0    |            | 93.8                            | 2.1  | 4.1  |            | 100             | 0    | 0    |            |            |
| Total %            | 0               | 46.8 | 0    | 46.8       | 47.9                            | 1.1  | 2.1  | 51.1       | 2.1             | 0    | 0    | 2.1        |            |

| Start Time   | US 11 Westbound |           |            | Kroger Rear Driveway Northbound |      |            | US 11 Eastbound |      |            | Int. Total |
|--|-----------------|-----------|------------|---------------------------------|------|------------|-----------------|------|------------|------------|
|  | Thru            | Left      | App. Total | Right                           | Left | App. Total | Right           | Thru | App. Total |            |
| Peak Hour Analysis From 11:00 AM to 12:45 PM - Peak 1 of 1 |                 |           |            |                                 |      |            |                 |      |            |            |
| Peak Hour for Entire Intersection Begins at 12:00 PM       |                 |           |            |                                 |      |            |                 |      |            |            |
| 12:00 PM   | 0               | 10        | 10         | 10                              | 0    | 10         | 0               | 0    | 0          | 20         |
| 12:15 PM   | 0               | 10        | 10         | 9                               | 0    | 9          | 0               | 0    | 0          | 19         |
| 12:30 PM   | 0               | <b>11</b> | <b>11</b>  | <b>21</b>                       | 0    | <b>21</b>  | 0               | 0    | 0          | <b>32</b>  |
| 12:45 PM   | 0               | 8         | 8          | 16                              | 0    | 16         | <b>1</b>        | 0    | <b>1</b>   | 25         |
| Total Volume   | 0               | 39        | 39         | 56                              | 0    | 56         | 1               | 0    | 1          | 96         |
| % App. Total   | 0               | 100       |            | 100                             | 0    |            | 100             | 0    |            |            |
| PHF  | .000            | .886      | .886       | .667                            | .000 | .667       | .250            | .000 | .250       | .750       |

# Peggy Malone & Associates, Inc.

## (888) 247-8602

File Name : TM 6-Kroger Rear Ent and US 11 PM  
 Site Code :  
 Start Date : 2/4/2015  
 Page No : 1

Groups Printed- Car

| Start Time         | US 11 Westbound |      |      |            | Kroger Rear Driveway Northbound |      |      |            | US 11 Eastbound |      |      |            | Int. Total |
|--------------------|-----------------|------|------|------------|---------------------------------|------|------|------------|-----------------|------|------|------------|------------|
|                    | Thru            | Left | Peds | App. Total | Right                           | Left | Peds | App. Total | Right           | Thru | Peds | App. Total |            |
| 03:00 PM           | 0               | 14   | 0    | 14         | 6                               | 1    | 0    | 7          | 0               | 0    | 0    | 0          | 21         |
| 03:15 PM           | 0               | 6    | 0    | 6          | 7                               | 0    | 0    | 7          | 1               | 0    | 0    | 1          | 14         |
| 03:30 PM           | 0               | 4    | 0    | 4          | 11                              | 0    | 0    | 11         | 2               | 0    | 0    | 2          | 17         |
| 03:45 PM           | 0               | 10   | 0    | 10         | 7                               | 0    | 1    | 8          | 1               | 0    | 0    | 1          | 19         |
| <b>Total</b>       | 0               | 34   | 0    | 34         | 31                              | 1    | 1    | 33         | 4               | 0    | 0    | 4          | 71         |
| 04:00 PM           | 0               | 10   | 0    | 10         | 10                              | 0    | 0    | 10         | 2               | 0    | 0    | 2          | 22         |
| 04:15 PM           | 0               | 8    | 0    | 8          | 9                               | 0    | 0    | 9          | 1               | 0    | 0    | 1          | 18         |
| 04:30 PM           | 0               | 9    | 0    | 9          | 10                              | 0    | 0    | 10         | 2               | 0    | 0    | 2          | 21         |
| 04:45 PM           | 0               | 10   | 0    | 10         | 15                              | 2    | 0    | 17         | 0               | 0    | 0    | 0          | 27         |
| <b>Total</b>       | 0               | 37   | 0    | 37         | 44                              | 2    | 0    | 46         | 5               | 0    | 0    | 5          | 88         |
| 05:00 PM           | 0               | 13   | 0    | 13         | 14                              | 0    | 0    | 14         | 0               | 0    | 0    | 0          | 27         |
| 05:15 PM           | 0               | 14   | 0    | 14         | 5                               | 0    | 0    | 5          | 2               | 0    | 0    | 2          | 21         |
| 05:30 PM           | 0               | 11   | 0    | 11         | 15                              | 1    | 0    | 16         | 0               | 0    | 0    | 0          | 27         |
| 05:45 PM           | 0               | 10   | 0    | 10         | 5                               | 1    | 0    | 6          | 0               | 0    | 0    | 0          | 16         |
| <b>Total</b>       | 0               | 48   | 0    | 48         | 39                              | 2    | 0    | 41         | 2               | 0    | 0    | 2          | 91         |
| <b>Grand Total</b> | 0               | 119  | 0    | 119        | 114                             | 5    | 1    | 120        | 11              | 0    | 0    | 11         | 250        |
| Apprch %           | 0               | 100  | 0    |            | 95                              | 4.2  | 0.8  |            | 100             | 0    | 0    |            |            |
| Total %            | 0               | 47.6 | 0    | 47.6       | 45.6                            | 2    | 0.4  | 48         | 4.4             | 0    | 0    | 4.4        |            |

| Start Time   | US 11 Westbound |      |            | Kroger Rear Driveway Northbound |      |            | US 11 Eastbound |      |            | Int. Total |
|--|-----------------|------|------------|---------------------------------|------|------------|-----------------|------|------------|------------|
|  | Thru            | Left | App. Total | Right                           | Left | App. Total | Right           | Thru | App. Total |            |
| Peak Hour Analysis From 03:00 PM to 05:45 PM - Peak 1 of 1 |                 |      |            |                                 |      |            |                 |      |            |            |
| Peak Hour for Entire Intersection Begins at 04:45 PM       |                 |      |            |                                 |      |            |                 |      |            |            |
| 04:45 PM   | 0               | 10   | 10         | 15                              | 2    | 17         | 0               | 0    | 0          | 27         |
| 05:00 PM   | 0               | 13   | 13         | 14                              | 0    | 14         | 0               | 0    | 0          | 27         |
| 05:15 PM   | 0               | 14   | 14         | 5                               | 0    | 5          | 2               | 0    | 2          | 21         |
| 05:30 PM   | 0               | 11   | 11         | 15                              | 1    | 16         | 0               | 0    | 0          | 27         |
| Total Volume   | 0               | 48   | 48         | 49                              | 3    | 52         | 2               | 0    | 2          | 102        |
| % App. Total   | 0               | 100  |            | 94.2                            | 5.8  |            | 100             | 0    |            |            |
| PHF  | .000            | .857 | .857       | .817                            | .375 | .765       | .250            | .000 | .250       | .944       |

# Peggy Malone & Associates, Inc.

## (888) 247-8602

File Name : TM 6-Kroger Rear Ent and US 11 PM  
 Site Code :  
 Start Date : 2/4/2015  
 Page No : 1

Groups Printed- Truck

| Start Time         | US 11 Westbound |      |      |            | Kroger Rear Driveway Northbound |      |      |            | US 11 Eastbound |      |      |            | Int. Total |
|--------------------|-----------------|------|------|------------|---------------------------------|------|------|------------|-----------------|------|------|------------|------------|
|                    | Thru            | Left | Peds | App. Total | Right                           | Left | Peds | App. Total | Right           | Thru | Peds | App. Total |            |
| 03:00 PM           | 0               | 0    | 0    | 0          | 0                               | 0    | 0    | 0          | 0               | 0    | 0    | 0          | 0          |
| 03:15 PM           | 0               | 0    | 0    | 0          | 0                               | 0    | 0    | 0          | 0               | 0    | 0    | 0          | 0          |
| 03:30 PM           | 0               | 0    | 0    | 0          | 0                               | 0    | 0    | 0          | 0               | 0    | 0    | 0          | 0          |
| 03:45 PM           | 0               | 0    | 0    | 0          | 0                               | 0    | 0    | 0          | 0               | 0    | 0    | 0          | 0          |
| <b>Total</b>       | 0               | 0    | 0    | 0          | 0                               | 0    | 0    | 0          | 0               | 0    | 0    | 0          | 0          |
| 04:00 PM           | 0               | 0    | 0    | 0          | 0                               | 0    | 0    | 0          | 0               | 0    | 0    | 0          | 0          |
| 04:15 PM           | 0               | 0    | 0    | 0          | 0                               | 0    | 0    | 0          | 0               | 0    | 0    | 0          | 0          |
| 04:30 PM           | 0               | 0    | 0    | 0          | 0                               | 0    | 0    | 0          | 0               | 0    | 0    | 0          | 0          |
| 04:45 PM           | 0               | 1    | 0    | 1          | 0                               | 0    | 0    | 0          | 0               | 0    | 0    | 0          | 1          |
| <b>Total</b>       | 0               | 1    | 0    | 1          | 0                               | 0    | 0    | 0          | 0               | 0    | 0    | 0          | 1          |
| 05:00 PM           | 0               | 0    | 0    | 0          | 0                               | 0    | 0    | 0          | 0               | 0    | 0    | 0          | 0          |
| 05:15 PM           | 0               | 0    | 0    | 0          | 0                               | 1    | 0    | 1          | 0               | 0    | 0    | 0          | 1          |
| 05:30 PM           | 0               | 0    | 0    | 0          | 0                               | 0    | 0    | 0          | 0               | 0    | 0    | 0          | 0          |
| 05:45 PM           | 0               | 0    | 0    | 0          | 0                               | 0    | 0    | 0          | 0               | 0    | 0    | 0          | 0          |
| <b>Total</b>       | 0               | 0    | 0    | 0          | 0                               | 1    | 0    | 1          | 0               | 0    | 0    | 0          | 1          |
| <b>Grand Total</b> | 0               | 1    | 0    | 1          | 0                               | 1    | 0    | 1          | 0               | 0    | 0    | 0          | 2          |
| Apprch %           | 0               | 100  | 0    |            | 0                               | 100  | 0    |            | 0               | 0    | 0    |            |            |
| Total %            | 0               | 50   | 0    | 50         | 0                               | 50   | 0    | 50         | 0               | 0    | 0    | 0          |            |

| Start Time   | US 11 Westbound |      |            | Kroger Rear Driveway Northbound |      |            | US 11 Eastbound |      |            | Int. Total |
|--|-----------------|------|------------|---------------------------------|------|------------|-----------------|------|------------|------------|
|  | Thru            | Left | App. Total | Right                           | Left | App. Total | Right           | Thru | App. Total |            |
| Peak Hour Analysis From 03:00 PM to 05:45 PM - Peak 1 of 1 |                 |      |            |                                 |      |            |                 |      |            |            |
| Peak Hour for Entire Intersection Begins at 04:30 PM       |                 |      |            |                                 |      |            |                 |      |            |            |
| 04:30 PM   | 0               | 0    | 0          | 0                               | 0    | 0          | 0               | 0    | 0          | 0          |
| 04:45 PM   | 0               | 1    | 1          | 0                               | 0    | 0          | 0               | 0    | 0          | 1          |
| 05:00 PM   | 0               | 0    | 0          | 0                               | 0    | 0          | 0               | 0    | 0          | 0          |
| 05:15 PM   | 0               | 0    | 0          | 0                               | 1    | 1          | 0               | 0    | 0          | 1          |
| <b>Total Volume</b>  | 0               | 1    | 1          | 0                               | 1    | 1          | 0               | 0    | 0          | 2          |
| <b>% App. Total</b>  | 0               | 100  |            | 0                               | 100  |            | 0               | 0    |            |            |
| PHF  | .000            | .250 | .250       | .000                            | .250 | .250       | .000            | .000 | .000       | .500       |

# Peggy Malone & Associates, Inc.

## (888) 247-8602

File Name : TM 6-Kroger Rear Ent and US 11 PM  
 Site Code :  
 Start Date : 2/4/2015  
 Page No : 1

Groups Printed- Combined

| Start Time         | US 11 Westbound |      |      |            | Kroger Rear Driveway Northbound |      |      |            | US 11 Eastbound |      |      |            | Int. Total |
|--------------------|-----------------|------|------|------------|---------------------------------|------|------|------------|-----------------|------|------|------------|------------|
|                    | Thru            | Left | Peds | App. Total | Right                           | Left | Peds | App. Total | Right           | Thru | Peds | App. Total |            |
| 03:00 PM           | 0               | 14   | 0    | 14         | 6                               | 1    | 0    | 7          | 0               | 0    | 0    | 0          | 21         |
| 03:15 PM           | 0               | 6    | 0    | 6          | 7                               | 0    | 0    | 7          | 1               | 0    | 0    | 1          | 14         |
| 03:30 PM           | 0               | 4    | 0    | 4          | 11                              | 0    | 0    | 11         | 2               | 0    | 0    | 2          | 17         |
| 03:45 PM           | 0               | 10   | 0    | 10         | 7                               | 0    | 1    | 8          | 1               | 0    | 0    | 1          | 19         |
| <b>Total</b>       | 0               | 34   | 0    | 34         | 31                              | 1    | 1    | 33         | 4               | 0    | 0    | 4          | 71         |
| 04:00 PM           | 0               | 10   | 0    | 10         | 10                              | 0    | 0    | 10         | 2               | 0    | 0    | 2          | 22         |
| 04:15 PM           | 0               | 8    | 0    | 8          | 9                               | 0    | 0    | 9          | 1               | 0    | 0    | 1          | 18         |
| 04:30 PM           | 0               | 9    | 0    | 9          | 10                              | 0    | 0    | 10         | 2               | 0    | 0    | 2          | 21         |
| 04:45 PM           | 0               | 11   | 0    | 11         | 15                              | 2    | 0    | 17         | 0               | 0    | 0    | 0          | 28         |
| <b>Total</b>       | 0               | 38   | 0    | 38         | 44                              | 2    | 0    | 46         | 5               | 0    | 0    | 5          | 89         |
| 05:00 PM           | 0               | 13   | 0    | 13         | 14                              | 0    | 0    | 14         | 0               | 0    | 0    | 0          | 27         |
| 05:15 PM           | 0               | 14   | 0    | 14         | 5                               | 1    | 0    | 6          | 2               | 0    | 0    | 2          | 22         |
| 05:30 PM           | 0               | 11   | 0    | 11         | 15                              | 1    | 0    | 16         | 0               | 0    | 0    | 0          | 27         |
| 05:45 PM           | 0               | 10   | 0    | 10         | 5                               | 1    | 0    | 6          | 0               | 0    | 0    | 0          | 16         |
| <b>Total</b>       | 0               | 48   | 0    | 48         | 39                              | 3    | 0    | 42         | 2               | 0    | 0    | 2          | 92         |
| <b>Grand Total</b> | 0               | 120  | 0    | 120        | 114                             | 6    | 1    | 121        | 11              | 0    | 0    | 11         | 252        |
| Apprch %           | 0               | 100  | 0    |            | 94.2                            | 5    | 0.8  |            | 100             | 0    | 0    |            |            |
| Total %            | 0               | 47.6 | 0    | 47.6       | 45.2                            | 2.4  | 0.4  | 48         | 4.4             | 0    | 0    | 4.4        |            |

| Start Time   | US 11 Westbound |      |            | Kroger Rear Driveway Northbound |      |            | US 11 Eastbound |      |            | Int. Total |
|--|-----------------|------|------------|---------------------------------|------|------------|-----------------|------|------------|------------|
|  | Thru            | Left | App. Total | Right                           | Left | App. Total | Right           | Thru | App. Total |            |
| Peak Hour Analysis From 03:00 PM to 05:45 PM - Peak 1 of 1 |                 |      |            |                                 |      |            |                 |      |            |            |
| Peak Hour for Entire Intersection Begins at 04:45 PM       |                 |      |            |                                 |      |            |                 |      |            |            |
| 04:45 PM   | 0               | 11   | 11         | 15                              | 2    | 17         | 0               | 0    | 0          | 28         |
| 05:00 PM   | 0               | 13   | 13         | 14                              | 0    | 14         | 0               | 0    | 0          | 27         |
| 05:15 PM   | 0               | 14   | 14         | 5                               | 1    | 6          | 2               | 0    | 2          | 22         |
| 05:30 PM   | 0               | 11   | 11         | 15                              | 1    | 16         | 0               | 0    | 0          | 27         |
| <b>Total Volume</b>  | 0               | 49   | 49         | 49                              | 4    | 53         | 2               | 0    | 2          | 104        |
| <b>% App. Total</b>  | 0               | 100  |            | 92.5                            | 7.5  |            | 100             | 0    |            |            |
| PHF  | .000            | .875 | .875       | .817                            | .500 | .779       | .250            | .000 | .250       | .929       |

# Peggy Malone & Associates, Inc.

## (888) 247-8602

File Name : TM 7-Business Drway and US 11 AM  
 Site Code :  
 Start Date : 2/4/2015  
 Page No : 1

Groups Printed- Car

| Start Time         | US 11 Westbound |      |      |            | Business Driveway Northbound |      |      |            | US 11 Eastbound |      |      |            | Int. Total |
|--------------------|-----------------|------|------|------------|------------------------------|------|------|------------|-----------------|------|------|------------|------------|
|                    | Thru            | Left | Peds | App. Total | Right                        | Left | Peds | App. Total | Right           | Thru | Peds | App. Total |            |
| 07:00 AM           | 0               | 0    | 0    | 0          | 0                            | 1    | 0    | 1          | 1               | 0    | 0    | 1          | 2          |
| 07:15 AM           | 0               | 0    | 0    | 0          | 1                            | 1    | 0    | 2          | 2               | 0    | 0    | 2          | 4          |
| 07:30 AM           | 0               | 1    | 0    | 1          | 0                            | 0    | 0    | 0          | 1               | 0    | 0    | 1          | 2          |
| 07:45 AM           | 0               | 0    | 0    | 0          | 1                            | 1    | 0    | 2          | 1               | 0    | 0    | 1          | 3          |
| <b>Total</b>       | 0               | 1    | 0    | 1          | 2                            | 3    | 0    | 5          | 5               | 0    | 0    | 5          | 11         |
| 08:00 AM           | 0               | 0    | 0    | 0          | 0                            | 1    | 0    | 1          | 0               | 0    | 0    | 0          | 1          |
| 08:15 AM           | 0               | 0    | 0    | 0          | 1                            | 1    | 0    | 2          | 1               | 0    | 0    | 1          | 3          |
| 08:30 AM           | 0               | 0    | 0    | 0          | 0                            | 1    | 2    | 3          | 0               | 0    | 0    | 0          | 3          |
| 08:45 AM           | 0               | 0    | 0    | 0          | 0                            | 0    | 0    | 0          | 4               | 0    | 0    | 4          | 4          |
| <b>Total</b>       | 0               | 0    | 0    | 0          | 1                            | 3    | 2    | 6          | 5               | 0    | 0    | 5          | 11         |
| <b>Grand Total</b> | 0               | 1    | 0    | 1          | 3                            | 6    | 2    | 11         | 10              | 0    | 0    | 10         | 22         |
| Apprch %           | 0               | 100  | 0    |            | 27.3                         | 54.5 | 18.2 |            | 100             | 0    | 0    |            |            |
| Total %            | 0               | 4.5  | 0    | 4.5        | 13.6                         | 27.3 | 9.1  | 50         | 45.5            | 0    | 0    | 45.5       |            |

| Start Time   | US 11 Westbound |      |            | Business Driveway Northbound |      |            | US 11 Eastbound |      |            | Int. Total |
|--|-----------------|------|------------|------------------------------|------|------------|-----------------|------|------------|------------|
|  | Thru            | Left | App. Total | Right                        | Left | App. Total | Right           | Thru | App. Total |            |
| Peak Hour Analysis From 07:00 AM to 08:45 AM - Peak 1 of 1 |                 |      |            |                              |      |            |                 |      |            |            |
| Peak Hour for Entire Intersection Begins at 07:00 AM       |                 |      |            |                              |      |            |                 |      |            |            |
| 07:00 AM   | 0               | 0    | 0          | 0                            | 1    | 1          | 1               | 0    | 1          | 2          |
| 07:15 AM   | 0               | 0    | 0          | 1                            | 1    | 2          | 2               | 0    | 2          | 4          |
| 07:30 AM   | 0               | 1    | 1          | 0                            | 0    | 0          | 1               | 0    | 1          | 2          |
| 07:45 AM   | 0               | 0    | 0          | 1                            | 1    | 2          | 1               | 0    | 1          | 3          |
| <b>Total Volume</b>  | 0               | 1    | 1          | 2                            | 3    | 5          | 5               | 0    | 5          | 11         |
| <b>% App. Total</b>  | 0               | 100  |            | 40                           | 60   |            | 100             | 0    |            |            |
| <b>PHF</b>   | .000            | .250 | .250       | .500                         | .750 | .625       | .625            | .000 | .625       | .688       |



# Peggy Malone & Associates, Inc.

## (888) 247-8602

File Name : TM 7-Business Drway and US 11 AM  
 Site Code :  
 Start Date : 2/4/2015  
 Page No : 1

Groups Printed- Truck

| Start Time         | US 11 Westbound |      |      |            | Business Driveway Northbound |      |      |            | US 11 Eastbound |      |      |            | Int. Total |
|--------------------|-----------------|------|------|------------|------------------------------|------|------|------------|-----------------|------|------|------------|------------|
|                    | Thru            | Left | Peds | App. Total | Right                        | Left | Peds | App. Total | Right           | Thru | Peds | App. Total |            |
| 07:00 AM           | 0               | 0    | 0    | 0          | 0                            | 0    | 0    | 0          | 0               | 0    | 0    | 0          | 0          |
| 07:15 AM           | 0               | 0    | 0    | 0          | 0                            | 0    | 0    | 0          | 0               | 0    | 0    | 0          | 0          |
| 07:30 AM           | 0               | 0    | 0    | 0          | 0                            | 0    | 0    | 0          | 0               | 0    | 0    | 0          | 0          |
| 07:45 AM           | 0               | 0    | 0    | 0          | 0                            | 0    | 0    | 0          | 0               | 0    | 0    | 0          | 0          |
| <b>Total</b>       | 0               | 0    | 0    | 0          | 0                            | 0    | 0    | 0          | 0               | 0    | 0    | 0          | 0          |
| 08:00 AM           | 0               | 0    | 0    | 0          | 1                            | 0    | 0    | 1          | 1               | 0    | 0    | 1          | 2          |
| 08:15 AM           | 0               | 0    | 0    | 0          | 0                            | 0    | 0    | 0          | 0               | 0    | 0    | 0          | 0          |
| 08:30 AM           | 0               | 0    | 0    | 0          | 0                            | 0    | 0    | 0          | 0               | 0    | 0    | 0          | 0          |
| 08:45 AM           | 0               | 0    | 0    | 0          | 0                            | 0    | 0    | 0          | 0               | 0    | 0    | 0          | 0          |
| <b>Total</b>       | 0               | 0    | 0    | 0          | 1                            | 0    | 0    | 1          | 1               | 0    | 0    | 1          | 2          |
| <b>Grand Total</b> | 0               | 0    | 0    | 0          | 1                            | 0    | 0    | 1          | 1               | 0    | 0    | 1          | 2          |
| Apprch %           | 0               | 0    | 0    | 0          | 100                          | 0    | 0    | 100        | 100             | 0    | 0    | 50         |            |
| Total %            | 0               | 0    | 0    | 0          | 50                           | 0    | 0    | 50         | 50              | 0    | 0    | 50         |            |

| Start Time   | US 11 Westbound |      |            | Business Driveway Northbound |      |            | US 11 Eastbound |      |            | Int. Total |
|--|-----------------|------|------------|------------------------------|------|------------|-----------------|------|------------|------------|
|  | Thru            | Left | App. Total | Right                        | Left | App. Total | Right           | Thru | App. Total |            |
| Peak Hour Analysis From 07:00 AM to 08:45 AM - Peak 1 of 1 |                 |      |            |                              |      |            |                 |      |            |            |
| Peak Hour for Entire Intersection Begins at 07:15 AM       |                 |      |            |                              |      |            |                 |      |            |            |
| 07:15 AM   | 0               | 0    | 0          | 0                            | 0    | 0          | 0               | 0    | 0          | 0          |
| 07:30 AM   | 0               | 0    | 0          | 0                            | 0    | 0          | 0               | 0    | 0          | 0          |
| 07:45 AM   | 0               | 0    | 0          | 0                            | 0    | 0          | 0               | 0    | 0          | 0          |
| 08:00 AM   | 0               | 0    | 0          | 1                            | 0    | 1          | 1               | 0    | 1          | 2          |
| <b>Total Volume</b>  | 0               | 0    | 0          | 1                            | 0    | 1          | 1               | 0    | 1          | 2          |
| <b>% App. Total</b>  | 0               | 0    | 0          | 100                          | 0    | 100        | 100             | 0    | 50         |            |
| PHF  | .000            | .000 | .000       | .250                         | .000 | .250       | .250            | .000 | .250       | .250       |

# Peggy Malone & Associates, Inc.

## (888) 247-8602

File Name : TM 7-Business Drway and US 11 AM  
 Site Code :  
 Start Date : 2/4/2015  
 Page No : 1

Groups Printed- Combined

| Start Time         | US 11 Westbound |          |          |            | Business Driveway Northbound |          |          |            | US 11 Eastbound |          |          |            | Int. Total |
|--------------------|-----------------|----------|----------|------------|------------------------------|----------|----------|------------|-----------------|----------|----------|------------|------------|
|                    | Thru            | Left     | Peds     | App. Total | Right                        | Left     | Peds     | App. Total | Right           | Thru     | Peds     | App. Total |            |
| 07:00 AM           | 0               | 0        | 0        | 0          | 0                            | 1        | 0        | 1          | 1               | 0        | 0        | 1          | 2          |
| 07:15 AM           | 0               | 0        | 0        | 0          | 1                            | 1        | 0        | 2          | 2               | 0        | 0        | 2          | 4          |
| 07:30 AM           | 0               | 1        | 0        | 1          | 0                            | 0        | 0        | 0          | 1               | 0        | 0        | 1          | 2          |
| 07:45 AM           | 0               | 0        | 0        | 0          | 1                            | 1        | 0        | 2          | 1               | 0        | 0        | 1          | 3          |
| <b>Total</b>       | <b>0</b>        | <b>1</b> | <b>0</b> | <b>1</b>   | <b>2</b>                     | <b>3</b> | <b>0</b> | <b>5</b>   | <b>5</b>        | <b>0</b> | <b>0</b> | <b>5</b>   | <b>11</b>  |
| 08:00 AM           | 0               | 0        | 0        | 0          | 1                            | 1        | 0        | 2          | 1               | 0        | 0        | 1          | 3          |
| 08:15 AM           | 0               | 0        | 0        | 0          | 1                            | 1        | 0        | 2          | 1               | 0        | 0        | 1          | 3          |
| 08:30 AM           | 0               | 0        | 0        | 0          | 0                            | 1        | 2        | 3          | 0               | 0        | 0        | 0          | 3          |
| 08:45 AM           | 0               | 0        | 0        | 0          | 0                            | 0        | 0        | 0          | 4               | 0        | 0        | 4          | 4          |
| <b>Total</b>       | <b>0</b>        | <b>0</b> | <b>0</b> | <b>0</b>   | <b>2</b>                     | <b>3</b> | <b>2</b> | <b>7</b>   | <b>6</b>        | <b>0</b> | <b>0</b> | <b>6</b>   | <b>13</b>  |
| <b>Grand Total</b> | <b>0</b>        | <b>1</b> | <b>0</b> | <b>1</b>   | <b>4</b>                     | <b>6</b> | <b>2</b> | <b>12</b>  | <b>11</b>       | <b>0</b> | <b>0</b> | <b>11</b>  | <b>24</b>  |
| Apprch %           | 0               | 100      | 0        |            | 33.3                         | 50       | 16.7     |            | 100             | 0        | 0        |            |            |
| Total %            | 0               | 4.2      | 0        | 4.2        | 16.7                         | 25       | 8.3      | 50         | 45.8            | 0        | 0        | 45.8       |            |

| Start Time   | US 11 Westbound |            |            | Business Driveway Northbound |           |            | US 11 Eastbound |          |            | Int. Total |
|--|-----------------|------------|------------|------------------------------|-----------|------------|-----------------|----------|------------|------------|
|  | Thru            | Left       | App. Total | Right                        | Left      | App. Total | Right           | Thru     | App. Total |            |
| Peak Hour Analysis From 07:00 AM to 08:45 AM - Peak 1 of 1 |                 |            |            |                              |           |            |                 |          |            |            |
| Peak Hour for Entire Intersection Begins at 07:15 AM       |                 |            |            |                              |           |            |                 |          |            |            |
| 07:15 AM   | 0               | 0          | 0          | 1                            | 1         | 2          | 2               | 0        | 2          | 4          |
| 07:30 AM   | 0               | 1          | 1          | 0                            | 0         | 0          | 1               | 0        | 1          | 2          |
| 07:45 AM   | 0               | 0          | 0          | 1                            | 1         | 2          | 1               | 0        | 1          | 3          |
| 08:00 AM   | 0               | 0          | 0          | 1                            | 1         | 2          | 1               | 0        | 1          | 3          |
| <b>Total Volume</b>  | <b>0</b>        | <b>1</b>   | <b>1</b>   | <b>3</b>                     | <b>3</b>  | <b>6</b>   | <b>5</b>        | <b>0</b> | <b>5</b>   | <b>12</b>  |
| <b>% App. Total</b>  | <b>0</b>        | <b>100</b> |            | <b>50</b>                    | <b>50</b> |            | <b>100</b>      | <b>0</b> |            |            |
| PHF  | .000            | .250       | .250       | .750                         | .750      | .750       | .625            | .000     | .625       | .750       |

Peggy Malone & Associates, Inc.  
(888) 247-8602

File Name : TM 7-Business Drway and US 11 MID  
Site Code :  
Start Date : 2/4/2015  
Page No : 1

Groups Printed- Car

| Start Time  | US 11 Westbound |      |      |            | Business Driveway Northbound |      |      |            | US 11 Eastbound |      |      |            | Int. Total |
|-------------|-----------------|------|------|------------|------------------------------|------|------|------------|-----------------|------|------|------------|------------|
|             | Thru            | Left | Peds | App. Total | Right                        | Left | Peds | App. Total | Right           | Thru | Peds | App. Total |            |
| 11:00 AM    | 0               | 2    | 0    | 2          | 1                            | 3    | 0    | 4          | 2               | 0    | 0    | 2          | 8          |
| 11:15 AM    | 0               | 0    | 0    | 0          | 1                            | 3    | 0    | 4          | 2               | 0    | 0    | 2          | 6          |
| 11:30 AM    | 0               | 1    | 0    | 1          | 2                            | 0    | 0    | 2          | 4               | 0    | 0    | 4          | 7          |
| 11:45 AM    | 0               | 0    | 0    | 0          | 1                            | 1    | 0    | 2          | 1               | 0    | 0    | 1          | 3          |
| Total       | 0               | 3    | 0    | 3          | 5                            | 7    | 0    | 12         | 9               | 0    | 0    | 9          | 24         |
| 12:00 PM    | 0               | 2    | 0    | 2          | 2                            | 0    | 1    | 3          | 0               | 0    | 0    | 0          | 5          |
| 12:15 PM    | 0               | 1    | 0    | 1          | 2                            | 1    | 3    | 6          | 0               | 0    | 0    | 0          | 7          |
| 12:30 PM    | 0               | 3    | 0    | 3          | 3                            | 1    | 3    | 7          | 5               | 0    | 0    | 5          | 15         |
| 12:45 PM    | 0               | 2    | 0    | 2          | 0                            | 4    | 2    | 6          | 4               | 0    | 0    | 4          | 12         |
| Total       | 0               | 8    | 0    | 8          | 7                            | 6    | 9    | 22         | 9               | 0    | 0    | 9          | 39         |
| Grand Total | 0               | 11   | 0    | 11         | 12                           | 13   | 9    | 34         | 18              | 0    | 0    | 18         | 63         |
| Apprch %    | 0               | 100  | 0    |            | 35.3                         | 38.2 | 26.5 |            | 100             | 0    | 0    |            |            |
| Total %     | 0               | 17.5 | 0    | 17.5       | 19                           | 20.6 | 14.3 | 54         | 28.6            | 0    | 0    | 28.6       |            |

| Start Time   | US 11 Westbound |      |            | Business Driveway Northbound |      |            | US 11 Eastbound |      |            | Int. Total |
|--|-----------------|------|------------|------------------------------|------|------------|-----------------|------|------------|------------|
|  | Thru            | Left | App. Total | Right                        | Left | App. Total | Right           | Thru | App. Total |            |
| Peak Hour Analysis From 11:00 AM to 12:45 PM - Peak 1 of 1 |                 |      |            |                              |      |            |                 |      |            |            |
| Peak Hour for Entire Intersection Begins at 12:00 PM       |                 |      |            |                              |      |            |                 |      |            |            |
| 12:00 PM   | 0               | 2    | 2          | 2                            | 0    | 2          | 0               | 0    | 0          | 4          |
| 12:15 PM   | 0               | 1    | 1          | 2                            | 1    | 3          | 0               | 0    | 0          | 4          |
| 12:30 PM   | 0               | 3    | 3          | 3                            | 1    | 4          | 5               | 0    | 5          | 12         |
| 12:45 PM   | 0               | 2    | 2          | 0                            | 4    | 4          | 4               | 0    | 4          | 10         |
| Total Volume   | 0               | 8    | 8          | 7                            | 6    | 13         | 9               | 0    | 9          | 30         |
| % App. Total   | 0               | 100  |            | 53.8                         | 46.2 |            | 100             | 0    |            |            |
| PHF  | .000            | .667 | .667       | .583                         | .375 | .813       | .450            | .000 | .450       | .625       |

Peggy Malone & Associates, Inc.  
(888) 247-8602

File Name : TM 7-Business Drway and US 11 MID  
Site Code :  
Start Date : 2/4/2015  
Page No : 1

Groups Printed- Truck

| Start Time  | US 11 Westbound |      |      |            | Business Driveway Northbound |      |      |            | US 11 Eastbound |      |      |            | Int. Total |
|-------------|-----------------|------|------|------------|------------------------------|------|------|------------|-----------------|------|------|------------|------------|
|             | Thru            | Left | Peds | App. Total | Right                        | Left | Peds | App. Total | Right           | Thru | Peds | App. Total |            |
| 11:00 AM    | 0               | 0    | 0    | 0          | 0                            | 0    | 0    | 0          | 0               | 0    | 0    | 0          | 0          |
| 11:15 AM    | 0               | 0    | 0    | 0          | 0                            | 0    | 0    | 0          | 0               | 0    | 0    | 0          | 0          |
| 11:30 AM    | 0               | 0    | 0    | 0          | 0                            | 0    | 0    | 0          | 0               | 0    | 0    | 0          | 0          |
| 11:45 AM    | 0               | 0    | 0    | 0          | 0                            | 0    | 0    | 0          | 0               | 0    | 0    | 0          | 0          |
| Total       | 0               | 0    | 0    | 0          | 0                            | 0    | 0    | 0          | 0               | 0    | 0    | 0          | 0          |
| 12:00 PM    | 0               | 0    | 0    | 0          | 0                            | 0    | 0    | 0          | 0               | 0    | 0    | 0          | 0          |
| 12:15 PM    | 0               | 0    | 0    | 0          | 0                            | 0    | 0    | 0          | 0               | 0    | 0    | 0          | 0          |
| 12:30 PM    | 0               | 0    | 0    | 0          | 0                            | 0    | 0    | 0          | 0               | 0    | 0    | 0          | 0          |
| 12:45 PM    | 0               | 0    | 0    | 0          | 0                            | 1    | 0    | 1          | 0               | 0    | 0    | 0          | 1          |
| Total       | 0               | 0    | 0    | 0          | 0                            | 1    | 0    | 1          | 0               | 0    | 0    | 0          | 1          |
| Grand Total | 0               | 0    | 0    | 0          | 0                            | 1    | 0    | 1          | 0               | 0    | 0    | 0          | 1          |
| Apprch %    | 0               | 0    | 0    | 0          | 0                            | 100  | 0    | 100        | 0               | 0    | 0    | 0          | 0          |
| Total %     | 0               | 0    | 0    | 0          | 0                            | 100  | 0    | 100        | 0               | 0    | 0    | 0          | 0          |

| Start Time   | US 11 Westbound |      |            | Business Driveway Northbound |      |            | US 11 Eastbound |      |            | Int. Total |
|--|-----------------|------|------------|------------------------------|------|------------|-----------------|------|------------|------------|
|  | Thru            | Left | App. Total | Right                        | Left | App. Total | Right           | Thru | App. Total |            |
| Peak Hour Analysis From 11:00 AM to 12:45 PM - Peak 1 of 1 |                 |      |            |                              |      |            |                 |      |            |            |
| Peak Hour for Entire Intersection Begins at 12:00 PM       |                 |      |            |                              |      |            |                 |      |            |            |
| 12:00 PM   | 0               | 0    | 0          | 0                            | 0    | 0          | 0               | 0    | 0          | 0          |
| 12:15 PM   | 0               | 0    | 0          | 0                            | 0    | 0          | 0               | 0    | 0          | 0          |
| 12:30 PM   | 0               | 0    | 0          | 0                            | 0    | 0          | 0               | 0    | 0          | 0          |
| 12:45 PM   | 0               | 0    | 0          | 0                            | 1    | 1          | 0               | 0    | 0          | 1          |
| Total Volume   | 0               | 0    | 0          | 0                            | 1    | 1          | 0               | 0    | 0          | 1          |
| % App. Total   | 0               | 0    | 0          | 0                            | 100  | 100        | 0               | 0    | 0          | 0          |
| PHF  | .000            | .000 | .000       | .000                         | .250 | .250       | .000            | .000 | .000       | .250       |

Peggy Malone & Associates, Inc.  
(888) 247-8602

File Name : TM 7-Business Drway and US 11 MID  
Site Code :  
Start Date : 2/4/2015  
Page No : 1

Groups Printed- Combined

| Start Time  | US 11 Westbound |      |      |            | Business Driveway Northbound |      |      |            | US 11 Eastbound |      |      |            | Int. Total |
|-------------|-----------------|------|------|------------|------------------------------|------|------|------------|-----------------|------|------|------------|------------|
|             | Thru            | Left | Peds | App. Total | Right                        | Left | Peds | App. Total | Right           | Thru | Peds | App. Total |            |
| 11:00 AM    | 0               | 2    | 0    | 2          | 1                            | 3    | 0    | 4          | 2               | 0    | 0    | 2          | 8          |
| 11:15 AM    | 0               | 0    | 0    | 0          | 1                            | 3    | 0    | 4          | 2               | 0    | 0    | 2          | 6          |
| 11:30 AM    | 0               | 1    | 0    | 1          | 2                            | 0    | 0    | 2          | 4               | 0    | 0    | 4          | 7          |
| 11:45 AM    | 0               | 0    | 0    | 0          | 1                            | 1    | 0    | 2          | 1               | 0    | 0    | 1          | 3          |
| Total       | 0               | 3    | 0    | 3          | 5                            | 7    | 0    | 12         | 9               | 0    | 0    | 9          | 24         |
| 12:00 PM    | 0               | 2    | 0    | 2          | 2                            | 0    | 1    | 3          | 0               | 0    | 0    | 0          | 5          |
| 12:15 PM    | 0               | 1    | 0    | 1          | 2                            | 1    | 3    | 6          | 0               | 0    | 0    | 0          | 7          |
| 12:30 PM    | 0               | 3    | 0    | 3          | 3                            | 1    | 3    | 7          | 5               | 0    | 0    | 5          | 15         |
| 12:45 PM    | 0               | 2    | 0    | 2          | 0                            | 5    | 2    | 7          | 4               | 0    | 0    | 4          | 13         |
| Total       | 0               | 8    | 0    | 8          | 7                            | 7    | 9    | 23         | 9               | 0    | 0    | 9          | 40         |
| Grand Total | 0               | 11   | 0    | 11         | 12                           | 14   | 9    | 35         | 18              | 0    | 0    | 18         | 64         |
| Apprch %    | 0               | 100  | 0    |            | 34.3                         | 40   | 25.7 |            | 100             | 0    | 0    |            |            |
| Total %     | 0               | 17.2 | 0    | 17.2       | 18.8                         | 21.9 | 14.1 | 54.7       | 28.1            | 0    | 0    | 28.1       |            |

| Start Time   | US 11 Westbound |      |            | Business Driveway Northbound |      |            | US 11 Eastbound |      |            | Int. Total |
|--|-----------------|------|------------|------------------------------|------|------------|-----------------|------|------------|------------|
|  | Thru            | Left | App. Total | Right                        | Left | App. Total | Right           | Thru | App. Total |            |
| Peak Hour Analysis From 11:00 AM to 12:45 PM - Peak 1 of 1 |                 |      |            |                              |      |            |                 |      |            |            |
| Peak Hour for Entire Intersection Begins at 12:00 PM       |                 |      |            |                              |      |            |                 |      |            |            |
| 12:00 PM   | 0               | 2    | 2          | 2                            | 0    | 2          | 0               | 0    | 0          | 4          |
| 12:15 PM   | 0               | 1    | 1          | 2                            | 1    | 3          | 0               | 0    | 0          | 4          |
| 12:30 PM   | 0               | 3    | 3          | 3                            | 1    | 4          | 5               | 0    | 5          | 12         |
| 12:45 PM   | 0               | 2    | 2          | 0                            | 5    | 5          | 4               | 0    | 4          | 11         |
| Total Volume   | 0               | 8    | 8          | 7                            | 7    | 14         | 9               | 0    | 9          | 31         |
| % App. Total   | 0               | 100  |            | 50                           | 50   |            | 100             | 0    |            |            |
| PHF  | .000            | .667 | .667       | .583                         | .350 | .700       | .450            | .000 | .450       | .646       |

# Peggy Malone & Associates, Inc.

## (888) 247-8602

File Name : TM 7-Business Drway and US 11 PM  
 Site Code :  
 Start Date : 2/4/2015  
 Page No : 1

Groups Printed- Car

| Start Time         | US 11 Westbound |      |      |            | Business Driveway Northbound |      |      |            | US 11 Eastbound |      |      |            | Int. Total |
|--------------------|-----------------|------|------|------------|------------------------------|------|------|------------|-----------------|------|------|------------|------------|
|                    | Thru            | Left | Peds | App. Total | Right                        | Left | Peds | App. Total | Right           | Thru | Peds | App. Total |            |
| 03:00 PM           | 0               | 1    | 0    | 1          | 0                            | 1    | 4    | 5          | 1               | 0    | 0    | 1          | 7          |
| 03:15 PM           | 0               | 2    | 0    | 2          | 5                            | 1    | 0    | 6          | 0               | 0    | 0    | 0          | 8          |
| 03:30 PM           | 0               | 2    | 0    | 2          | 0                            | 2    | 1    | 3          | 3               | 0    | 0    | 3          | 8          |
| 03:45 PM           | 0               | 2    | 0    | 2          | 1                            | 3    | 1    | 5          | 0               | 0    | 0    | 0          | 7          |
| <b>Total</b>       | 0               | 7    | 0    | 7          | 6                            | 7    | 6    | 19         | 4               | 0    | 0    | 4          | 30         |
| 04:00 PM           | 0               | 2    | 0    | 2          | 1                            | 1    | 0    | 2          | 3               | 0    | 0    | 3          | 7          |
| 04:15 PM           | 0               | 1    | 0    | 1          | 1                            | 1    | 0    | 2          | 0               | 0    | 0    | 0          | 3          |
| 04:30 PM           | 0               | 0    | 0    | 0          | 0                            | 3    | 1    | 4          | 0               | 0    | 0    | 0          | 4          |
| 04:45 PM           | 0               | 2    | 0    | 2          | 1                            | 1    | 0    | 2          | 0               | 0    | 0    | 0          | 4          |
| <b>Total</b>       | 0               | 5    | 0    | 5          | 3                            | 6    | 1    | 10         | 3               | 0    | 0    | 3          | 18         |
| 05:00 PM           | 0               | 1    | 0    | 1          | 2                            | 1    | 0    | 3          | 3               | 0    | 0    | 3          | 7          |
| 05:15 PM           | 0               | 0    | 0    | 0          | 2                            | 0    | 0    | 2          | 1               | 0    | 0    | 1          | 3          |
| 05:30 PM           | 0               | 0    | 0    | 0          | 1                            | 1    | 0    | 2          | 2               | 0    | 0    | 2          | 4          |
| 05:45 PM           | 0               | 2    | 0    | 2          | 3                            | 1    | 0    | 4          | 5               | 0    | 0    | 5          | 11         |
| <b>Total</b>       | 0               | 3    | 0    | 3          | 8                            | 3    | 0    | 11         | 11              | 0    | 0    | 11         | 25         |
| <b>Grand Total</b> | 0               | 15   | 0    | 15         | 17                           | 16   | 7    | 40         | 18              | 0    | 0    | 18         | 73         |
| Apprch %           | 0               | 100  | 0    |            | 42.5                         | 40   | 17.5 |            | 100             | 0    | 0    |            |            |
| Total %            | 0               | 20.5 | 0    | 20.5       | 23.3                         | 21.9 | 9.6  | 54.8       | 24.7            | 0    | 0    | 24.7       |            |

| Start Time   | US 11 Westbound |      |            | Business Driveway Northbound |      |            | US 11 Eastbound |      |            | Int. Total |
|--|-----------------|------|------------|------------------------------|------|------------|-----------------|------|------------|------------|
|  | Thru            | Left | App. Total | Right                        | Left | App. Total | Right           | Thru | App. Total |            |
| Peak Hour Analysis From 03:00 PM to 05:45 PM - Peak 1 of 1 |                 |      |            |                              |      |            |                 |      |            |            |
| Peak Hour for Entire Intersection Begins at 03:15 PM       |                 |      |            |                              |      |            |                 |      |            |            |
| 03:15 PM   | 0               | 2    | 2          | 5                            | 1    | 6          | 0               | 0    | 0          | 8          |
| 03:30 PM   | 0               | 2    | 2          | 0                            | 2    | 2          | 3               | 0    | 3          | 7          |
| 03:45 PM   | 0               | 2    | 2          | 1                            | 3    | 4          | 0               | 0    | 0          | 6          |
| 04:00 PM   | 0               | 2    | 2          | 1                            | 1    | 2          | 3               | 0    | 3          | 7          |
| Total Volume   | 0               | 8    | 8          | 7                            | 7    | 14         | 6               | 0    | 6          | 28         |
| % App. Total   | 0               | 100  |            | 50                           | 50   |            | 100             | 0    |            |            |
| PHF  | .000            | 1.00 | 1.00       | .350                         | .583 | .583       | .500            | .000 | .500       | .875       |

# Peggy Malone & Associates, Inc.

## (888) 247-8602

File Name : TM 7-Business Drway and US 11 PM  
 Site Code :  
 Start Date : 2/4/2015  
 Page No : 1

Groups Printed- Truck

| Start Time         | US 11 Westbound |      |      |            | Business Driveway Northbound |      |      |            | US 11 Eastbound |      |      |            | Int. Total |
|--------------------|-----------------|------|------|------------|------------------------------|------|------|------------|-----------------|------|------|------------|------------|
|                    | Thru            | Left | Peds | App. Total | Right                        | Left | Peds | App. Total | Right           | Thru | Peds | App. Total |            |
| 03:00 PM           | 0               | 0    | 0    | 0          | 1                            | 0    | 0    | 1          | 1               | 0    | 0    | 1          | 2          |
| 03:15 PM           | 0               | 0    | 0    | 0          | 0                            | 0    | 0    | 0          | 0               | 0    | 0    | 0          | 0          |
| 03:30 PM           | 0               | 0    | 0    | 0          | 0                            | 0    | 0    | 0          | 0               | 0    | 0    | 0          | 0          |
| 03:45 PM           | 0               | 0    | 0    | 0          | 0                            | 0    | 0    | 0          | 1               | 0    | 0    | 1          | 1          |
| <b>Total</b>       | 0               | 0    | 0    | 0          | 1                            | 0    | 0    | 1          | 2               | 0    | 0    | 2          | 3          |
| 04:00 PM           | 0               | 0    | 0    | 0          | 0                            | 0    | 0    | 0          | 0               | 0    | 0    | 0          | 0          |
| 04:15 PM           | 0               | 0    | 0    | 0          | 0                            | 0    | 0    | 0          | 0               | 0    | 0    | 0          | 0          |
| 04:30 PM           | 0               | 0    | 0    | 0          | 0                            | 0    | 0    | 0          | 0               | 0    | 0    | 0          | 0          |
| 04:45 PM           | 0               | 0    | 0    | 0          | 0                            | 0    | 0    | 0          | 0               | 0    | 0    | 0          | 0          |
| <b>Total</b>       | 0               | 0    | 0    | 0          | 0                            | 0    | 0    | 0          | 0               | 0    | 0    | 0          | 0          |
| 05:00 PM           | 0               | 0    | 0    | 0          | 0                            | 0    | 0    | 0          | 0               | 0    | 0    | 0          | 0          |
| 05:15 PM           | 0               | 0    | 0    | 0          | 0                            | 0    | 0    | 0          | 0               | 0    | 0    | 0          | 0          |
| 05:30 PM           | 0               | 0    | 0    | 0          | 0                            | 0    | 0    | 0          | 0               | 0    | 0    | 0          | 0          |
| 05:45 PM           | 0               | 0    | 0    | 0          | 0                            | 0    | 0    | 0          | 0               | 0    | 0    | 0          | 0          |
| <b>Total</b>       | 0               | 0    | 0    | 0          | 0                            | 0    | 0    | 0          | 0               | 0    | 0    | 0          | 0          |
| <b>Grand Total</b> | 0               | 0    | 0    | 0          | 1                            | 0    | 0    | 1          | 2               | 0    | 0    | 2          | 3          |
| Apprch %           | 0               | 0    | 0    | 0          | 100                          | 0    | 0    | 100        | 100             | 0    | 0    | 100        | 100        |
| Total %            | 0               | 0    | 0    | 0          | 33.3                         | 0    | 0    | 33.3       | 66.7            | 0    | 0    | 66.7       | 66.7       |

| Start Time   | US 11 Westbound |      |            | Business Driveway Northbound |      |            | US 11 Eastbound |      |            | Int. Total |
|--|-----------------|------|------------|------------------------------|------|------------|-----------------|------|------------|------------|
|  | Thru            | Left | App. Total | Right                        | Left | App. Total | Right           | Thru | App. Total |            |
| Peak Hour Analysis From 03:00 PM to 05:45 PM - Peak 1 of 1 |                 |      |            |                              |      |            |                 |      |            |            |
| Peak Hour for Entire Intersection Begins at 03:00 PM       |                 |      |            |                              |      |            |                 |      |            |            |
| 03:00 PM   | 0               | 0    | 0          | 1                            | 0    | 1          | 1               | 0    | 1          | 2          |
| 03:15 PM   | 0               | 0    | 0          | 0                            | 0    | 0          | 0               | 0    | 0          | 0          |
| 03:30 PM   | 0               | 0    | 0          | 0                            | 0    | 0          | 0               | 0    | 0          | 0          |
| 03:45 PM   | 0               | 0    | 0          | 0                            | 0    | 0          | 1               | 0    | 1          | 1          |
| <b>Total Volume</b>  | 0               | 0    | 0          | 1                            | 0    | 1          | 2               | 0    | 2          | 3          |
| <b>% App. Total</b>  | 0               | 0    | 0          | 100                          | 0    | 100        | 100             | 0    | 100        | 100        |
| PHF  | .000            | .000 | .000       | .250                         | .000 | .250       | .500            | .000 | .500       | .375       |

# Peggy Malone & Associates, Inc.

## (888) 247-8602

File Name : TM 7-Business Drway and US 11 PM  
 Site Code :  
 Start Date : 2/4/2015  
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Groups Printed- Combined

| Start Time         | US 11 Westbound |      |      |            | Business Driveway Northbound |      |      |            | US 11 Eastbound |      |      |            | Int. Total |
|--------------------|-----------------|------|------|------------|------------------------------|------|------|------------|-----------------|------|------|------------|------------|
|                    | Thru            | Left | Peds | App. Total | Right                        | Left | Peds | App. Total | Right           | Thru | Peds | App. Total |            |
| 03:00 PM           | 0               | 1    | 0    | 1          | 1                            | 1    | 4    | 6          | 2               | 0    | 0    | 2          | 9          |
| 03:15 PM           | 0               | 2    | 0    | 2          | 5                            | 1    | 0    | 6          | 0               | 0    | 0    | 0          | 8          |
| 03:30 PM           | 0               | 2    | 0    | 2          | 0                            | 2    | 1    | 3          | 3               | 0    | 0    | 3          | 8          |
| 03:45 PM           | 0               | 2    | 0    | 2          | 1                            | 3    | 1    | 5          | 1               | 0    | 0    | 1          | 8          |
| <b>Total</b>       | 0               | 7    | 0    | 7          | 7                            | 7    | 6    | 20         | 6               | 0    | 0    | 6          | 33         |
| 04:00 PM           | 0               | 2    | 0    | 2          | 1                            | 1    | 0    | 2          | 3               | 0    | 0    | 3          | 7          |
| 04:15 PM           | 0               | 1    | 0    | 1          | 1                            | 1    | 0    | 2          | 0               | 0    | 0    | 0          | 3          |
| 04:30 PM           | 0               | 0    | 0    | 0          | 0                            | 3    | 1    | 4          | 0               | 0    | 0    | 0          | 4          |
| 04:45 PM           | 0               | 2    | 0    | 2          | 1                            | 1    | 0    | 2          | 0               | 0    | 0    | 0          | 4          |
| <b>Total</b>       | 0               | 5    | 0    | 5          | 3                            | 6    | 1    | 10         | 3               | 0    | 0    | 3          | 18         |
| 05:00 PM           | 0               | 1    | 0    | 1          | 2                            | 1    | 0    | 3          | 3               | 0    | 0    | 3          | 7          |
| 05:15 PM           | 0               | 0    | 0    | 0          | 2                            | 0    | 0    | 2          | 1               | 0    | 0    | 1          | 3          |
| 05:30 PM           | 0               | 0    | 0    | 0          | 1                            | 1    | 0    | 2          | 2               | 0    | 0    | 2          | 4          |
| 05:45 PM           | 0               | 2    | 0    | 2          | 3                            | 1    | 0    | 4          | 5               | 0    | 0    | 5          | 11         |
| <b>Total</b>       | 0               | 3    | 0    | 3          | 8                            | 3    | 0    | 11         | 11              | 0    | 0    | 11         | 25         |
| <b>Grand Total</b> | 0               | 15   | 0    | 15         | 18                           | 16   | 7    | 41         | 20              | 0    | 0    | 20         | 76         |
| Apprch %           | 0               | 100  | 0    |            | 43.9                         | 39   | 17.1 |            | 100             | 0    | 0    |            |            |
| Total %            | 0               | 19.7 | 0    | 19.7       | 23.7                         | 21.1 | 9.2  | 53.9       | 26.3            | 0    | 0    | 26.3       |            |

| Start Time   | US 11 Westbound |      |            | Business Driveway Northbound |      |            | US 11 Eastbound |      |            | Int. Total |
|--|-----------------|------|------------|------------------------------|------|------------|-----------------|------|------------|------------|
|  | Thru            | Left | App. Total | Right                        | Left | App. Total | Right           | Thru | App. Total |            |
| Peak Hour Analysis From 03:00 PM to 05:45 PM - Peak 1 of 1 |                 |      |            |                              |      |            |                 |      |            |            |
| Peak Hour for Entire Intersection Begins at 03:15 PM       |                 |      |            |                              |      |            |                 |      |            |            |
| 03:15 PM   | 0               | 2    | 2          | 5                            | 1    | 6          | 0               | 0    | 0          | 8          |
| 03:30 PM   | 0               | 2    | 2          | 0                            | 2    | 2          | 3               | 0    | 3          | 7          |
| 03:45 PM   | 0               | 2    | 2          | 1                            | 3    | 4          | 1               | 0    | 1          | 7          |
| 04:00 PM   | 0               | 2    | 2          | 1                            | 1    | 2          | 3               | 0    | 3          | 7          |
| <b>Total Volume</b>  | 0               | 8    | 8          | 7                            | 7    | 14         | 7               | 0    | 7          | 29         |
| <b>% App. Total</b>  | 0               | 100  |            | 50                           | 50   |            | 100             | 0    |            |            |
| PHF  | .000            | 1.00 | 1.00       | .350                         | .583 | .583       | .583            | .000 | .583       | .906       |



# Peggy Malone & Associates, Inc.

## (888) 247-8602

File Name : TM 8-University Park and US 11 AM  
 Site Code :  
 Start Date : 2/4/2015  
 Page No : 1

Groups Printed- Car

| Start Time         | Restaurant Parking Southbound |      |      |      |            | US 11 Westbound |      |      |      |            | University Park Dr. Northbound |      |      |      |            | US 11 Eastbound |      |      |      |            | Int. Total |
|--------------------|-------------------------------|------|------|------|------------|-----------------|------|------|------|------------|--------------------------------|------|------|------|------------|-----------------|------|------|------|------------|------------|
|                    | Right                         | Thru | Left | Peds | App. Total | Right           | Thru | Left | Peds | App. Total | Right                          | Thru | Left | Peds | App. Total | Right           | Thru | Left | Peds | App. Total |            |
| 07:00 AM           | 0                             | 0    | 0    | 0    | 0          | 0               | 0    | 0    | 0    | 0          | 0                              | 0    | 0    | 0    | 0          | 1               | 0    | 0    | 0    | 0          | 1          |
| 07:15 AM           | 0                             | 0    | 0    | 0    | 0          | 0               | 0    | 1    | 0    | 1          | 0                              | 0    | 0    | 0    | 0          | 4               | 0    | 0    | 0    | 0          | 4          |
| 07:30 AM           | 0                             | 0    | 0    | 0    | 0          | 0               | 0    | 2    | 0    | 2          | 1                              | 0    | 0    | 0    | 1          | 3               | 0    | 0    | 0    | 0          | 3          |
| 07:45 AM           | 0                             | 0    | 0    | 0    | 0          | 0               | 0    | 1    | 0    | 1          | 0                              | 0    | 0    | 0    | 0          | 0               | 0    | 0    | 0    | 0          | 0          |
| <b>Total</b>       | 0                             | 0    | 0    | 0    | 0          | 0               | 0    | 4    | 0    | 4          | 1                              | 0    | 0    | 0    | 1          | 8               | 0    | 0    | 0    | 0          | 8          |
| 08:00 AM           | 0                             | 0    | 0    | 0    | 0          | 0               | 0    | 5    | 0    | 5          | 0                              | 0    | 1    | 0    | 1          | 2               | 0    | 0    | 0    | 0          | 2          |
| 08:15 AM           | 0                             | 0    | 0    | 0    | 0          | 0               | 0    | 0    | 0    | 0          | 0                              | 0    | 0    | 0    | 0          | 0               | 0    | 0    | 0    | 0          | 0          |
| 08:30 AM           | 0                             | 0    | 0    | 0    | 0          | 0               | 0    | 4    | 0    | 4          | 0                              | 0    | 0    | 1    | 1          | 2               | 0    | 0    | 0    | 0          | 2          |
| 08:45 AM           | 0                             | 0    | 0    | 0    | 0          | 0               | 0    | 0    | 0    | 0          | 1                              | 0    | 0    | 0    | 1          | 1               | 0    | 2    | 0    | 0          | 3          |
| <b>Total</b>       | 0                             | 0    | 0    | 0    | 0          | 0               | 0    | 9    | 0    | 9          | 1                              | 0    | 1    | 1    | 3          | 5               | 0    | 2    | 0    | 0          | 7          |
| <b>Grand Total</b> | 0                             | 0    | 0    | 0    | 0          | 0               | 0    | 13   | 0    | 13         | 2                              | 0    | 1    | 1    | 4          | 13              | 0    | 2    | 0    | 0          | 15         |
| Apprch %           | 0                             | 0    | 0    | 0    | 0          | 0               | 0    | 100  | 0    | 100        | 50                             | 0    | 25   | 25   | 50         | 86.7            | 0    | 13.3 | 0    | 0          | 86.7       |
| Total %            | 0                             | 0    | 0    | 0    | 0          | 0               | 0    | 40.6 | 0    | 40.6       | 6.2                            | 0    | 3.1  | 3.1  | 12.5       | 40.6            | 0    | 6.2  | 0    | 0          | 46.9       |

| Start Time   | Restaurant Parking Southbound |      |      |            | US 11 Westbound |      |      |            | University Park Dr. Northbound |      |      |            | US 11 Eastbound |      |      |            | Int. Total |
|--|-------------------------------|------|------|------------|-----------------|------|------|------------|--------------------------------|------|------|------------|-----------------|------|------|------------|------------|
|  | Right                         | Thru | Left | App. Total | Right           | Thru | Left | App. Total | Right                          | Thru | Left | App. Total | Right           | Thru | Left | App. Total |            |
| Peak Hour Analysis From 07:00 AM to 08:45 AM - Peak 1 of 1 |                               |      |      |            |                 |      |      |            |                                |      |      |            |                 |      |      |            |            |
| Peak Hour for Entire Intersection Begins at 07:15 AM       |                               |      |      |            |                 |      |      |            |                                |      |      |            |                 |      |      |            |            |
| 07:15 AM   | 0                             | 0    | 0    | 0          | 0               | 0    | 1    | 1          | 0                              | 0    | 0    | 0          | 4               | 0    | 0    | 0          | 4          |
| 07:30 AM   | 0                             | 0    | 0    | 0          | 0               | 0    | 2    | 2          | 1                              | 0    | 0    | 1          | 3               | 0    | 0    | 0          | 3          |
| 07:45 AM   | 0                             | 0    | 0    | 0          | 0               | 0    | 1    | 1          | 0                              | 0    | 0    | 0          | 0               | 0    | 0    | 0          | 0          |
| 08:00 AM   | 0                             | 0    | 0    | 0          | 0               | 0    | 5    | 5          | 0                              | 0    | 1    | 1          | 2               | 0    | 0    | 0          | 2          |
| <b>Total Volume</b>  | 0                             | 0    | 0    | 0          | 0               | 0    | 9    | 9          | 1                              | 0    | 1    | 2          | 9               | 0    | 0    | 0          | 9          |
| % App. Total   | 0                             | 0    | 0    | 0          | 0               | 0    | 100  | 100        | 50                             | 0    | 50   | 50         | 100             | 0    | 0    | 0          | 100        |
| PHF  | .000                          | .000 | .000 | .000       | .000            | .000 | .450 | .450       | .250                           | .000 | .250 | .500       | .563            | .000 | .000 | .563       | .625       |

# Peggy Malone & Associates, Inc.

## (888) 247-8602

File Name : TM 8-University Park and US 11 AM  
 Site Code :  
 Start Date : 2/4/2015  
 Page No : 1

Groups Printed- Truck

| Start Time         | Restaurant Parking Southbound |      |      |      |            | US 11 Westbound |      |      |      |            | University Park Dr. Northbound |      |      |      |            | US 11 Eastbound |      |      |      |            | Int. Total |
|--------------------|-------------------------------|------|------|------|------------|-----------------|------|------|------|------------|--------------------------------|------|------|------|------------|-----------------|------|------|------|------------|------------|
|                    | Right                         | Thru | Left | Peds | App. Total | Right           | Thru | Left | Peds | App. Total | Right                          | Thru | Left | Peds | App. Total | Right           | Thru | Left | Peds | App. Total |            |
| 07:00 AM           | 0                             | 0    | 0    | 0    | 0          | 0               | 0    | 0    | 0    | 0          | 0                              | 0    | 0    | 0    | 0          | 0               | 0    | 0    | 0    | 0          | 0          |
| 07:15 AM           | 0                             | 0    | 0    | 0    | 0          | 0               | 0    | 0    | 0    | 0          | 0                              | 0    | 0    | 0    | 0          | 0               | 0    | 0    | 0    | 0          | 0          |
| 07:30 AM           | 0                             | 0    | 0    | 0    | 0          | 0               | 0    | 0    | 0    | 0          | 0                              | 0    | 0    | 0    | 0          | 0               | 0    | 0    | 0    | 0          | 0          |
| 07:45 AM           | 0                             | 0    | 0    | 0    | 0          | 0               | 0    | 0    | 0    | 0          | 0                              | 0    | 0    | 0    | 0          | 0               | 0    | 0    | 0    | 0          | 0          |
| <b>Total</b>       | 0                             | 0    | 0    | 0    | 0          | 0               | 0    | 0    | 0    | 0          | 0                              | 0    | 0    | 0    | 0          | 0               | 0    | 0    | 0    | 0          | 0          |
| 08:00 AM           | 0                             | 0    | 0    | 0    | 0          | 0               | 0    | 0    | 0    | 0          | 0                              | 0    | 0    | 0    | 0          | 0               | 0    | 1    | 0    | 1          | 1          |
| 08:15 AM           | 0                             | 0    | 0    | 0    | 0          | 0               | 0    | 0    | 0    | 0          | 0                              | 0    | 0    | 0    | 0          | 0               | 0    | 0    | 0    | 0          | 0          |
| 08:30 AM           | 0                             | 0    | 0    | 0    | 0          | 0               | 0    | 0    | 0    | 0          | 0                              | 0    | 0    | 0    | 0          | 0               | 0    | 0    | 0    | 0          | 0          |
| 08:45 AM           | 0                             | 0    | 0    | 0    | 0          | 0               | 0    | 0    | 0    | 0          | 0                              | 0    | 0    | 0    | 0          | 0               | 0    | 0    | 0    | 0          | 0          |
| <b>Total</b>       | 0                             | 0    | 0    | 0    | 0          | 0               | 0    | 0    | 0    | 0          | 0                              | 0    | 0    | 0    | 0          | 0               | 0    | 1    | 0    | 1          | 1          |
| <b>Grand Total</b> | 0                             | 0    | 0    | 0    | 0          | 0               | 0    | 0    | 0    | 0          | 0                              | 0    | 0    | 0    | 0          | 0               | 0    | 1    | 0    | 1          | 1          |
| Apprch %           | 0                             | 0    | 0    | 0    |            | 0               | 0    | 0    | 0    |            | 0                              | 0    | 0    | 0    |            | 0               | 0    | 100  | 0    |            |            |
| Total %            | 0                             | 0    | 0    | 0    |            | 0               | 0    | 0    | 0    |            | 0                              | 0    | 0    | 0    |            | 0               | 0    | 100  | 0    | 100        |            |

| Start Time   | Restaurant Parking Southbound |      |      |            | US 11 Westbound |      |      |            | University Park Dr. Northbound |      |      |            | US 11 Eastbound |      |      |            | Int. Total |
|--|-------------------------------|------|------|------------|-----------------|------|------|------------|--------------------------------|------|------|------------|-----------------|------|------|------------|------------|
|  | Right                         | Thru | Left | App. Total | Right           | Thru | Left | App. Total | Right                          | Thru | Left | App. Total | Right           | Thru | Left | App. Total |            |
| Peak Hour Analysis From 07:00 AM to 08:45 AM - Peak 1 of 1 |                               |      |      |            |                 |      |      |            |                                |      |      |            |                 |      |      |            |            |
| Peak Hour for Entire Intersection Begins at 07:15 AM       |                               |      |      |            |                 |      |      |            |                                |      |      |            |                 |      |      |            |            |
| 07:15 AM   | 0                             | 0    | 0    | 0          | 0               | 0    | 0    | 0          | 0                              | 0    | 0    | 0          | 0               | 0    | 0    | 0          | 0          |
| 07:30 AM   | 0                             | 0    | 0    | 0          | 0               | 0    | 0    | 0          | 0                              | 0    | 0    | 0          | 0               | 0    | 0    | 0          | 0          |
| 07:45 AM   | 0                             | 0    | 0    | 0          | 0               | 0    | 0    | 0          | 0                              | 0    | 0    | 0          | 0               | 0    | 0    | 0          | 0          |
| 08:00 AM   | 0                             | 0    | 0    | 0          | 0               | 0    | 0    | 0          | 0                              | 0    | 0    | 0          | 0               | 0    | 1    | 1          | 1          |
| <b>Total Volume</b>  | 0                             | 0    | 0    | 0          | 0               | 0    | 0    | 0          | 0                              | 0    | 0    | 0          | 0               | 0    | 1    | 1          | 1          |
| <b>% App. Total</b>  | 0                             | 0    | 0    |            | 0               | 0    | 0    |            | 0                              | 0    | 0    |            | 0               | 0    | 100  |            |            |
| PHF  | .000                          | .000 | .000 | .000       | .000            | .000 | .000 | .000       | .000                           | .000 | .000 | .000       | .000            | .000 | .250 | .250       | .250       |

# Peggy Malone & Associates, Inc.

## (888) 247-8602

File Name : TM 8-University Park and US 11 AM  
 Site Code :  
 Start Date : 2/4/2015  
 Page No : 1

Groups Printed- Combined

| Start Time         | Restaurant Parking Southbound |      |      |      |            | US 11 Westbound |      |      |      |            | University Park Dr. Northbound |      |      |      |            | US 11 Eastbound |      |      |      |            | Int. Total |
|--------------------|-------------------------------|------|------|------|------------|-----------------|------|------|------|------------|--------------------------------|------|------|------|------------|-----------------|------|------|------|------------|------------|
|                    | Right                         | Thru | Left | Peds | App. Total | Right           | Thru | Left | Peds | App. Total | Right                          | Thru | Left | Peds | App. Total | Right           | Thru | Left | Peds | App. Total |            |
| 07:00 AM           | 0                             | 0    | 0    | 0    | 0          | 0               | 0    | 0    | 0    | 0          | 0                              | 0    | 0    | 0    | 0          | 1               | 0    | 0    | 0    | 0          | 1          |
| 07:15 AM           | 0                             | 0    | 0    | 0    | 0          | 0               | 0    | 1    | 0    | 1          | 0                              | 0    | 0    | 0    | 0          | 4               | 0    | 0    | 0    | 0          | 4          |
| 07:30 AM           | 0                             | 0    | 0    | 0    | 0          | 0               | 0    | 2    | 0    | 2          | 1                              | 0    | 0    | 0    | 1          | 3               | 0    | 0    | 0    | 0          | 3          |
| 07:45 AM           | 0                             | 0    | 0    | 0    | 0          | 0               | 0    | 1    | 0    | 1          | 0                              | 0    | 0    | 0    | 0          | 0               | 0    | 0    | 0    | 0          | 0          |
| <b>Total</b>       | 0                             | 0    | 0    | 0    | 0          | 0               | 0    | 4    | 0    | 4          | 1                              | 0    | 0    | 0    | 1          | 8               | 0    | 0    | 0    | 0          | 8          |
| 08:00 AM           | 0                             | 0    | 0    | 0    | 0          | 0               | 0    | 5    | 0    | 5          | 0                              | 0    | 1    | 0    | 1          | 2               | 0    | 1    | 0    | 0          | 3          |
| 08:15 AM           | 0                             | 0    | 0    | 0    | 0          | 0               | 0    | 0    | 0    | 0          | 0                              | 0    | 0    | 0    | 0          | 0               | 0    | 0    | 0    | 0          | 0          |
| 08:30 AM           | 0                             | 0    | 0    | 0    | 0          | 0               | 0    | 4    | 0    | 4          | 0                              | 0    | 0    | 1    | 1          | 2               | 0    | 0    | 0    | 0          | 2          |
| 08:45 AM           | 0                             | 0    | 0    | 0    | 0          | 0               | 0    | 0    | 0    | 0          | 1                              | 0    | 0    | 0    | 1          | 1               | 0    | 2    | 0    | 0          | 3          |
| <b>Total</b>       | 0                             | 0    | 0    | 0    | 0          | 0               | 0    | 9    | 0    | 9          | 1                              | 0    | 1    | 1    | 3          | 5               | 0    | 3    | 0    | 0          | 8          |
| <b>Grand Total</b> | 0                             | 0    | 0    | 0    | 0          | 0               | 0    | 13   | 0    | 13         | 2                              | 0    | 1    | 1    | 4          | 13              | 0    | 3    | 0    | 0          | 16         |
| Apprch %           | 0                             | 0    | 0    | 0    | 0          | 0               | 0    | 100  | 0    | 0          | 50                             | 0    | 25   | 25   | 0          | 81.2            | 0    | 18.8 | 0    | 0          | 0          |
| Total %            | 0                             | 0    | 0    | 0    | 0          | 0               | 0    | 39.4 | 0    | 39.4       | 6.1                            | 0    | 3    | 3    | 12.1       | 39.4            | 0    | 9.1  | 0    | 0          | 48.5       |

| Start Time   | Restaurant Parking Southbound |      |      |            | US 11 Westbound |      |      |            | University Park Dr. Northbound |      |      |            | US 11 Eastbound |      |      |            | Int. Total |
|--|-------------------------------|------|------|------------|-----------------|------|------|------------|--------------------------------|------|------|------------|-----------------|------|------|------------|------------|
|  | Right                         | Thru | Left | App. Total | Right           | Thru | Left | App. Total | Right                          | Thru | Left | App. Total | Right           | Thru | Left | App. Total |            |
| Peak Hour Analysis From 07:00 AM to 08:45 AM - Peak 1 of 1 |                               |      |      |            |                 |      |      |            |                                |      |      |            |                 |      |      |            |            |
| Peak Hour for Entire Intersection Begins at 07:15 AM       |                               |      |      |            |                 |      |      |            |                                |      |      |            |                 |      |      |            |            |
| 07:15 AM   | 0                             | 0    | 0    | 0          | 0               | 0    | 1    | 1          | 0                              | 0    | 0    | 0          | 4               | 0    | 0    | 4          | 5          |
| 07:30 AM   | 0                             | 0    | 0    | 0          | 0               | 0    | 2    | 2          | 1                              | 0    | 0    | 1          | 3               | 0    | 0    | 3          | 6          |
| 07:45 AM   | 0                             | 0    | 0    | 0          | 0               | 0    | 1    | 1          | 0                              | 0    | 0    | 0          | 0               | 0    | 0    | 0          | 1          |
| 08:00 AM   | 0                             | 0    | 0    | 0          | 0               | 0    | 5    | 5          | 0                              | 0    | 1    | 1          | 2               | 0    | 1    | 3          | 9          |
| <b>Total Volume</b>  | 0                             | 0    | 0    | 0          | 0               | 0    | 9    | 9          | 1                              | 0    | 1    | 2          | 9               | 0    | 1    | 10         | 21         |
| <b>% App. Total</b>  | 0                             | 0    | 0    | 0          | 0               | 0    | 100  | 0          | 50                             | 0    | 50   | 0          | 90              | 0    | 10   | 0          | 0          |
| PHF  | .000                          | .000 | .000 | .000       | .000            | .000 | .450 | .450       | .250                           | .000 | .250 | .500       | .563            | .000 | .250 | .625       | .583       |

# Peggy Malone & Associates, Inc.

## (888) 247-8602

File Name : TM 8-University Park and US 11 MID  
 Site Code :  
 Start Date : 2/4/2015  
 Page No : 1

Groups Printed- Car

| Start Time         | Restaurant Parking Southbound |          |          |          |            | US 11 Westbound |          |           |          |            | University Park Dr. Northbound |          |           |          |            | US 11 Eastbound |          |          |          |            | Int. Total |
|--------------------|-------------------------------|----------|----------|----------|------------|-----------------|----------|-----------|----------|------------|--------------------------------|----------|-----------|----------|------------|-----------------|----------|----------|----------|------------|------------|
|                    | Right                         | Thru     | Left     | Peds     | App. Total | Right           | Thru     | Left      | Peds     | App. Total | Right                          | Thru     | Left      | Peds     | App. Total | Right           | Thru     | Left     | Peds     | App. Total |            |
| 11:00 AM           | 0                             | 0        | 0        | 1        | 1          | 0               | 0        | 3         | 0        | 3          | 0                              | 0        | 1         | 0        | 1          | 2               | 0        | 0        | 0        | 2          | 7          |
| 11:15 AM           | 0                             | 0        | 0        | 0        | 0          | 0               | 0        | 1         | 0        | 1          | 3                              | 0        | 4         | 0        | 7          | 2               | 0        | 0        | 0        | 2          | 10         |
| 11:30 AM           | 1                             | 0        | 0        | 0        | 1          | 0               | 0        | 1         | 0        | 1          | 1                              | 0        | 1         | 0        | 2          | 1               | 0        | 1        | 0        | 2          | 6          |
| 11:45 AM           | 0                             | 0        | 0        | 0        | 0          | 0               | 0        | 1         | 0        | 1          | 2                              | 0        | 0         | 0        | 2          | 1               | 0        | 3        | 0        | 4          | 7          |
| <b>Total</b>       | <b>1</b>                      | <b>0</b> | <b>0</b> | <b>1</b> | <b>2</b>   | <b>0</b>        | <b>0</b> | <b>6</b>  | <b>0</b> | <b>6</b>   | <b>6</b>                       | <b>0</b> | <b>6</b>  | <b>0</b> | <b>12</b>  | <b>6</b>        | <b>0</b> | <b>4</b> | <b>0</b> | <b>10</b>  | <b>30</b>  |
| 12:00 PM           | 1                             | 0        | 0        | 0        | 1          | 0               | 0        | 0         | 0        | 0          | 2                              | 0        | 3         | 0        | 5          | 2               | 0        | 0        | 0        | 2          | 8          |
| 12:15 PM           | 0                             | 0        | 0        | 1        | 1          | 1               | 0        | 1         | 0        | 2          | 0                              | 0        | 3         | 0        | 3          | 0               | 0        | 1        | 0        | 1          | 7          |
| 12:30 PM           | 0                             | 0        | 1        | 0        | 1          | 0               | 0        | 0         | 0        | 0          | 0                              | 0        | 1         | 0        | 1          | 1               | 0        | 0        | 0        | 1          | 3          |
| 12:45 PM           | 2                             | 0        | 1        | 1        | 4          | 0               | 0        | 4         | 0        | 4          | 1                              | 0        | 0         | 0        | 1          | 1               | 0        | 0        | 0        | 1          | 10         |
| <b>Total</b>       | <b>3</b>                      | <b>0</b> | <b>2</b> | <b>2</b> | <b>7</b>   | <b>1</b>        | <b>0</b> | <b>5</b>  | <b>0</b> | <b>6</b>   | <b>3</b>                       | <b>0</b> | <b>7</b>  | <b>0</b> | <b>10</b>  | <b>4</b>        | <b>0</b> | <b>1</b> | <b>0</b> | <b>5</b>   | <b>28</b>  |
| <b>Grand Total</b> | <b>4</b>                      | <b>0</b> | <b>2</b> | <b>3</b> | <b>9</b>   | <b>1</b>        | <b>0</b> | <b>11</b> | <b>0</b> | <b>12</b>  | <b>9</b>                       | <b>0</b> | <b>13</b> | <b>0</b> | <b>22</b>  | <b>10</b>       | <b>0</b> | <b>5</b> | <b>0</b> | <b>15</b>  | <b>58</b>  |
| Apprch %           | 44.4                          | 0        | 22.2     | 33.3     |            | 8.3             | 0        | 91.7      | 0        |            | 40.9                           | 0        | 59.1      | 0        |            | 66.7            | 0        | 33.3     | 0        |            |            |
| Total %            | 6.9                           | 0        | 3.4      | 5.2      | 15.5       | 1.7             | 0        | 19        | 0        | 20.7       | 15.5                           | 0        | 22.4      | 0        | 37.9       | 17.2            | 0        | 8.6      | 0        | 25.9       |            |

| Start Time   | Restaurant Parking Southbound |          |          |            | US 11 Westbound |          |          |            | University Park Dr. Northbound |          |          |            | US 11 Eastbound |          |          |            | Int. Total |
|--|-------------------------------|----------|----------|------------|-----------------|----------|----------|------------|--------------------------------|----------|----------|------------|-----------------|----------|----------|------------|------------|
|  | Right                         | Thru     | Left     | App. Total | Right           | Thru     | Left     | App. Total | Right                          | Thru     | Left     | App. Total | Right           | Thru     | Left     | App. Total |            |
| Peak Hour Analysis From 11:00 AM to 12:45 PM - Peak 1 of 1 |                               |          |          |            |                 |          |          |            |                                |          |          |            |                 |          |          |            |            |
| Peak Hour for Entire Intersection Begins at 11:15 AM       |                               |          |          |            |                 |          |          |            |                                |          |          |            |                 |          |          |            |            |
| 11:15 AM   | 0                             | 0        | 0        | 0          | 0               | 0        | 1        | 1          | 3                              | 0        | 4        | 7          | 2               | 0        | 0        | 2          | 10         |
| 11:30 AM   | 1                             | 0        | 0        | 1          | 0               | 0        | 1        | 1          | 1                              | 0        | 1        | 2          | 1               | 0        | 1        | 2          | 6          |
| 11:45 AM   | 0                             | 0        | 0        | 0          | 0               | 0        | 1        | 1          | 2                              | 0        | 0        | 2          | 1               | 0        | 3        | 4          | 7          |
| 12:00 PM   | 1                             | 0        | 0        | 1          | 0               | 0        | 0        | 0          | 2                              | 0        | 3        | 5          | 2               | 0        | 0        | 2          | 8          |
| <b>Total Volume</b>  | <b>2</b>                      | <b>0</b> | <b>0</b> | <b>2</b>   | <b>0</b>        | <b>0</b> | <b>3</b> | <b>3</b>   | <b>8</b>                       | <b>0</b> | <b>8</b> | <b>16</b>  | <b>6</b>        | <b>0</b> | <b>4</b> | <b>10</b>  | <b>31</b>  |
| % App. Total   | 100                           | 0        | 0        |            | 0               | 0        | 100      |            | 50                             | 0        | 50       |            | 60              | 0        | 40       |            |            |
| PHF  | .500                          | .000     | .000     | .500       | .000            | .000     | .750     | .750       | .667                           | .000     | .500     | .571       | .750            | .000     | .333     | .625       | .775       |

# Peggy Malone & Associates, Inc.

## (888) 247-8602

File Name : TM 8-University Park and US 11 MID  
 Site Code :  
 Start Date : 2/4/2015  
 Page No : 1

Groups Printed- Truck

| Start Time         | Restaurant Parking Southbound |      |      |      |            | US 11 Westbound |      |      |      |            | University Park Dr. Northbound |      |      |      |            | US 11 Eastbound |      |      |      |            | Int. Total |
|--------------------|-------------------------------|------|------|------|------------|-----------------|------|------|------|------------|--------------------------------|------|------|------|------------|-----------------|------|------|------|------------|------------|
|                    | Right                         | Thru | Left | Peds | App. Total | Right           | Thru | Left | Peds | App. Total | Right                          | Thru | Left | Peds | App. Total | Right           | Thru | Left | Peds | App. Total |            |
| 11:00 AM           | 0                             | 0    | 0    | 0    | 0          | 0               | 0    | 0    | 0    | 0          | 0                              | 0    | 1    | 0    | 1          | 0               | 0    | 0    | 0    | 0          | 0          |
| 11:15 AM           | 0                             | 0    | 0    | 0    | 0          | 0               | 0    | 2    | 0    | 2          | 0                              | 0    | 0    | 0    | 0          | 0               | 0    | 0    | 0    | 0          | 0          |
| 11:30 AM           | 0                             | 0    | 0    | 0    | 0          | 0               | 0    | 0    | 0    | 0          | 0                              | 0    | 1    | 0    | 1          | 0               | 0    | 0    | 0    | 0          | 0          |
| 11:45 AM           | 0                             | 0    | 0    | 0    | 0          | 0               | 0    | 0    | 0    | 0          | 0                              | 0    | 0    | 0    | 0          | 0               | 0    | 0    | 0    | 0          | 0          |
| <b>Total</b>       | 0                             | 0    | 0    | 0    | 0          | 0               | 0    | 2    | 0    | 2          | 0                              | 0    | 2    | 0    | 2          | 0               | 0    | 0    | 0    | 0          | 0          |
| 12:00 PM           | 0                             | 0    | 0    | 0    | 0          | 0               | 0    | 0    | 0    | 0          | 0                              | 0    | 0    | 0    | 0          | 0               | 0    | 0    | 0    | 0          | 0          |
| 12:15 PM           | 0                             | 0    | 0    | 0    | 0          | 0               | 0    | 0    | 0    | 0          | 1                              | 0    | 0    | 0    | 1          | 0               | 0    | 0    | 0    | 0          | 0          |
| 12:30 PM           | 0                             | 0    | 0    | 0    | 0          | 0               | 0    | 0    | 0    | 0          | 0                              | 0    | 0    | 0    | 0          | 0               | 0    | 0    | 0    | 0          | 0          |
| 12:45 PM           | 0                             | 0    | 0    | 0    | 0          | 0               | 0    | 0    | 0    | 0          | 0                              | 0    | 0    | 0    | 0          | 0               | 0    | 0    | 0    | 0          | 0          |
| <b>Total</b>       | 0                             | 0    | 0    | 0    | 0          | 0               | 0    | 0    | 0    | 0          | 1                              | 0    | 0    | 0    | 1          | 0               | 0    | 0    | 0    | 0          | 0          |
| <b>Grand Total</b> | 0                             | 0    | 0    | 0    | 0          | 0               | 0    | 2    | 0    | 2          | 1                              | 0    | 2    | 0    | 3          | 0               | 0    | 0    | 0    | 0          | 5          |
| Apprch %           | 0                             | 0    | 0    | 0    |            | 0               | 0    | 100  | 0    |            | 33.3                           | 0    | 66.7 | 0    |            | 0               | 0    | 0    | 0    |            |            |
| Total %            | 0                             | 0    | 0    | 0    |            | 0               | 0    | 40   | 0    | 40         | 20                             | 0    | 40   | 0    | 60         | 0               | 0    | 0    | 0    |            |            |

| Start Time   | Restaurant Parking Southbound |      |      |            | US 11 Westbound |      |      |            | University Park Dr. Northbound |      |      |            | US 11 Eastbound |      |      |            | Int. Total |
|--|-------------------------------|------|------|------------|-----------------|------|------|------------|--------------------------------|------|------|------------|-----------------|------|------|------------|------------|
|  | Right                         | Thru | Left | App. Total | Right           | Thru | Left | App. Total | Right                          | Thru | Left | App. Total | Right           | Thru | Left | App. Total |            |
| Peak Hour Analysis From 11:00 AM to 12:45 PM - Peak 1 of 1 |                               |      |      |            |                 |      |      |            |                                |      |      |            |                 |      |      |            |            |
| Peak Hour for Entire Intersection Begins at 11:00 AM       |                               |      |      |            |                 |      |      |            |                                |      |      |            |                 |      |      |            |            |
| 11:00 AM   | 0                             | 0    | 0    | 0          | 0               | 0    | 0    | 0          | 0                              | 0    | 1    | 1          | 0               | 0    | 0    | 0          | 0          |
| 11:15 AM   | 0                             | 0    | 0    | 0          | 0               | 0    | 2    | 2          | 0                              | 0    | 0    | 0          | 0               | 0    | 0    | 0          | 0          |
| 11:30 AM   | 0                             | 0    | 0    | 0          | 0               | 0    | 0    | 0          | 0                              | 0    | 1    | 1          | 0               | 0    | 0    | 0          | 0          |
| 11:45 AM   | 0                             | 0    | 0    | 0          | 0               | 0    | 0    | 0          | 0                              | 0    | 0    | 0          | 0               | 0    | 0    | 0          | 0          |
| <b>Total Volume</b>  | 0                             | 0    | 0    | 0          | 0               | 0    | 2    | 2          | 0                              | 0    | 2    | 2          | 0               | 0    | 0    | 0          | 0          |
| <b>% App. Total</b>  | 0                             | 0    | 0    |            | 0               | 0    | 100  |            | 0                              | 0    | 100  |            | 0               | 0    | 0    |            |            |
| PHF  | .000                          | .000 | .000 | .000       | .000            | .000 | .250 | .250       | .000                           | .000 | .500 | .500       | .000            | .000 | .000 | .000       | .500       |

# Peggy Malone & Associates, Inc.

## (888) 247-8602

File Name : TM 8-University Park and US 11 MID  
 Site Code :  
 Start Date : 2/4/2015  
 Page No : 1

Groups Printed- Combined

| Start Time         | Restaurant Parking Southbound |          |          |          |            | US 11 Westbound |          |           |          |            | University Park Dr. Northbound |          |           |          |            | US 11 Eastbound |          |          |          |            | Int. Total |
|--------------------|-------------------------------|----------|----------|----------|------------|-----------------|----------|-----------|----------|------------|--------------------------------|----------|-----------|----------|------------|-----------------|----------|----------|----------|------------|------------|
|                    | Right                         | Thru     | Left     | Peds     | App. Total | Right           | Thru     | Left      | Peds     | App. Total | Right                          | Thru     | Left      | Peds     | App. Total | Right           | Thru     | Left     | Peds     | App. Total |            |
| 11:00 AM           | 0                             | 0        | 0        | 1        | 1          | 0               | 0        | 3         | 0        | 3          | 0                              | 0        | 2         | 0        | 2          | 2               | 0        | 0        | 0        | 2          | 8          |
| 11:15 AM           | 0                             | 0        | 0        | 0        | 0          | 0               | 0        | 3         | 0        | 3          | 3                              | 0        | 4         | 0        | 7          | 2               | 0        | 0        | 0        | 2          | 12         |
| 11:30 AM           | 1                             | 0        | 0        | 0        | 1          | 0               | 0        | 1         | 0        | 1          | 1                              | 0        | 2         | 0        | 3          | 1               | 0        | 1        | 0        | 2          | 7          |
| 11:45 AM           | 0                             | 0        | 0        | 0        | 0          | 0               | 0        | 1         | 0        | 1          | 2                              | 0        | 0         | 0        | 2          | 1               | 0        | 3        | 0        | 4          | 7          |
| <b>Total</b>       | <b>1</b>                      | <b>0</b> | <b>0</b> | <b>1</b> | <b>2</b>   | <b>0</b>        | <b>0</b> | <b>8</b>  | <b>0</b> | <b>8</b>   | <b>6</b>                       | <b>0</b> | <b>8</b>  | <b>0</b> | <b>14</b>  | <b>6</b>        | <b>0</b> | <b>4</b> | <b>0</b> | <b>10</b>  | <b>34</b>  |
| 12:00 PM           | 1                             | 0        | 0        | 0        | 1          | 0               | 0        | 0         | 0        | 0          | 2                              | 0        | 3         | 0        | 5          | 2               | 0        | 0        | 0        | 2          | 8          |
| 12:15 PM           | 0                             | 0        | 0        | 1        | 1          | 1               | 0        | 1         | 0        | 2          | 1                              | 0        | 3         | 0        | 4          | 0               | 0        | 1        | 0        | 1          | 8          |
| 12:30 PM           | 0                             | 0        | 1        | 0        | 1          | 0               | 0        | 0         | 0        | 0          | 0                              | 0        | 1         | 0        | 1          | 1               | 0        | 0        | 0        | 1          | 3          |
| 12:45 PM           | 2                             | 0        | 1        | 1        | 4          | 0               | 0        | 4         | 0        | 4          | 1                              | 0        | 0         | 0        | 1          | 1               | 0        | 0        | 0        | 1          | 10         |
| <b>Total</b>       | <b>3</b>                      | <b>0</b> | <b>2</b> | <b>2</b> | <b>7</b>   | <b>1</b>        | <b>0</b> | <b>5</b>  | <b>0</b> | <b>6</b>   | <b>4</b>                       | <b>0</b> | <b>7</b>  | <b>0</b> | <b>11</b>  | <b>4</b>        | <b>0</b> | <b>1</b> | <b>0</b> | <b>5</b>   | <b>29</b>  |
| <b>Grand Total</b> | <b>4</b>                      | <b>0</b> | <b>2</b> | <b>3</b> | <b>9</b>   | <b>1</b>        | <b>0</b> | <b>13</b> | <b>0</b> | <b>14</b>  | <b>10</b>                      | <b>0</b> | <b>15</b> | <b>0</b> | <b>25</b>  | <b>10</b>       | <b>0</b> | <b>5</b> | <b>0</b> | <b>15</b>  | <b>63</b>  |
| Apprch %           | 44.4                          | 0        | 22.2     | 33.3     |            | 7.1             | 0        | 92.9      | 0        |            | 40                             | 0        | 60        | 0        |            | 66.7            | 0        | 33.3     | 0        |            |            |
| Total %            | 6.3                           | 0        | 3.2      | 4.8      | 14.3       | 1.6             | 0        | 20.6      | 0        | 22.2       | 15.9                           | 0        | 23.8      | 0        | 39.7       | 15.9            | 0        | 7.9      | 0        | 23.8       |            |

| Start Time   | Restaurant Parking Southbound |          |          |            | US 11 Westbound |          |          |            | University Park Dr. Northbound |          |          |            | US 11 Eastbound |          |          |            | Int. Total |
|--|-------------------------------|----------|----------|------------|-----------------|----------|----------|------------|--------------------------------|----------|----------|------------|-----------------|----------|----------|------------|------------|
|  | Right                         | Thru     | Left     | App. Total | Right           | Thru     | Left     | App. Total | Right                          | Thru     | Left     | App. Total | Right           | Thru     | Left     | App. Total |            |
| Peak Hour Analysis From 11:00 AM to 12:45 PM - Peak 1 of 1 |                               |          |          |            |                 |          |          |            |                                |          |          |            |                 |          |          |            |            |
| Peak Hour for Entire Intersection Begins at 11:15 AM       |                               |          |          |            |                 |          |          |            |                                |          |          |            |                 |          |          |            |            |
| 11:15 AM   | 0                             | 0        | 0        | 0          | 0               | 0        | 3        | 3          | 3                              | 0        | 4        | 7          | 2               | 0        | 0        | 2          | 12         |
| 11:30 AM   | 1                             | 0        | 0        | 1          | 0               | 0        | 1        | 1          | 1                              | 0        | 2        | 3          | 1               | 0        | 1        | 2          | 7          |
| 11:45 AM   | 0                             | 0        | 0        | 0          | 0               | 0        | 1        | 1          | 2                              | 0        | 0        | 2          | 1               | 0        | 3        | 4          | 7          |
| 12:00 PM   | 1                             | 0        | 0        | 1          | 0               | 0        | 0        | 0          | 2                              | 0        | 3        | 5          | 2               | 0        | 0        | 2          | 8          |
| <b>Total Volume</b>  | <b>2</b>                      | <b>0</b> | <b>0</b> | <b>2</b>   | <b>0</b>        | <b>0</b> | <b>5</b> | <b>5</b>   | <b>8</b>                       | <b>0</b> | <b>9</b> | <b>17</b>  | <b>6</b>        | <b>0</b> | <b>4</b> | <b>10</b>  | <b>34</b>  |
| % App. Total   | 100                           | 0        | 0        |            | 0               | 0        | 100      |            | 47.1                           | 0        | 52.9     |            | 60              | 0        | 40       |            |            |
| PHF  | .500                          | .000     | .000     | .500       | .000            | .000     | .417     | .417       | .667                           | .000     | .563     | .607       | .750            | .000     | .333     | .625       | .708       |

# Peggy Malone & Associates, Inc.

## (888) 247-8602

File Name : TM 8-University Park and US 11 PM  
 Site Code :  
 Start Date : 2/4/2015  
 Page No : 1

Groups Printed- Car

| Start Time         | Restaurant Parking Southbound |          |          |          |            | US 11 Westbound |          |          |          |            | University Park Dr. Northbound |          |           |          |            | US 11 Eastbound |          |           |          |            | Int. Total |
|--------------------|-------------------------------|----------|----------|----------|------------|-----------------|----------|----------|----------|------------|--------------------------------|----------|-----------|----------|------------|-----------------|----------|-----------|----------|------------|------------|
|                    | Right                         | Thru     | Left     | Peds     | App. Total | Right           | Thru     | Left     | Peds     | App. Total | Right                          | Thru     | Left      | Peds     | App. Total | Right           | Thru     | Left      | Peds     | App. Total |            |
| 03:00 PM           | 4                             | 0        | 0        | 0        | 4          | 0               | 0        | 2        | 0        | 2          | 1                              | 0        | 1         | 0        | 2          | 0               | 0        | 0         | 0        | 0          | 8          |
| 03:15 PM           | 0                             | 0        | 0        | 0        | 0          | 0               | 0        | 0        | 0        | 0          | 0                              | 0        | 0         | 0        | 0          | 0               | 0        | 1         | 0        | 1          | 1          |
| 03:30 PM           | 0                             | 0        | 0        | 1        | 1          | 0               | 0        | 0        | 0        | 0          | 2                              | 0        | 2         | 0        | 4          | 0               | 0        | 0         | 0        | 0          | 5          |
| 03:45 PM           | 0                             | 0        | 2        | 1        | 3          | 0               | 0        | 0        | 0        | 0          | 4                              | 0        | 0         | 1        | 5          | 1               | 0        | 1         | 0        | 2          | 10         |
| <b>Total</b>       | <b>4</b>                      | <b>0</b> | <b>2</b> | <b>2</b> | <b>8</b>   | <b>0</b>        | <b>0</b> | <b>2</b> | <b>0</b> | <b>2</b>   | <b>7</b>                       | <b>0</b> | <b>3</b>  | <b>1</b> | <b>11</b>  | <b>1</b>        | <b>0</b> | <b>2</b>  | <b>0</b> | <b>3</b>   | <b>24</b>  |
| 04:00 PM           | 0                             | 0        | 0        | 0        | 0          | 0               | 0        | 0        | 0        | 0          | 0                              | 0        | 0         | 0        | 0          | 1               | 0        | 2         | 0        | 3          | 3          |
| 04:15 PM           | 0                             | 0        | 1        | 0        | 1          | 0               | 0        | 1        | 0        | 1          | 2                              | 0        | 1         | 0        | 3          | 1               | 0        | 2         | 0        | 3          | 8          |
| 04:30 PM           | 0                             | 0        | 0        | 0        | 0          | 0               | 0        | 1        | 0        | 1          | 0                              | 0        | 2         | 0        | 2          | 0               | 0        | 2         | 0        | 2          | 5          |
| 04:45 PM           | 1                             | 0        | 0        | 3        | 4          | 0               | 0        | 0        | 0        | 0          | 2                              | 0        | 0         | 0        | 2          | 1               | 0        | 3         | 0        | 4          | 10         |
| <b>Total</b>       | <b>1</b>                      | <b>0</b> | <b>1</b> | <b>3</b> | <b>5</b>   | <b>0</b>        | <b>0</b> | <b>2</b> | <b>0</b> | <b>2</b>   | <b>4</b>                       | <b>0</b> | <b>3</b>  | <b>0</b> | <b>7</b>   | <b>3</b>        | <b>0</b> | <b>9</b>  | <b>0</b> | <b>12</b>  | <b>26</b>  |
| 05:00 PM           | 1                             | 0        | 0        | 0        | 1          | 0               | 0        | 2        | 0        | 2          | 4                              | 0        | 2         | 0        | 6          | 2               | 0        | 4         | 0        | 6          | 15         |
| 05:15 PM           | 0                             | 0        | 0        | 0        | 0          | 1               | 0        | 1        | 0        | 2          | 2                              | 0        | 1         | 0        | 3          | 2               | 0        | 1         | 0        | 3          | 8          |
| 05:30 PM           | 0                             | 0        | 0        | 2        | 2          | 0               | 0        | 0        | 0        | 0          | 2                              | 0        | 0         | 0        | 2          | 1               | 0        | 1         | 0        | 2          | 6          |
| 05:45 PM           | 1                             | 0        | 1        | 0        | 2          | 0               | 0        | 0        | 0        | 0          | 0                              | 0        | 2         | 0        | 2          | 1               | 0        | 1         | 0        | 2          | 6          |
| <b>Total</b>       | <b>2</b>                      | <b>0</b> | <b>1</b> | <b>2</b> | <b>5</b>   | <b>1</b>        | <b>0</b> | <b>3</b> | <b>0</b> | <b>4</b>   | <b>8</b>                       | <b>0</b> | <b>5</b>  | <b>0</b> | <b>13</b>  | <b>6</b>        | <b>0</b> | <b>7</b>  | <b>0</b> | <b>13</b>  | <b>35</b>  |
| <b>Grand Total</b> | <b>7</b>                      | <b>0</b> | <b>4</b> | <b>7</b> | <b>18</b>  | <b>1</b>        | <b>0</b> | <b>7</b> | <b>0</b> | <b>8</b>   | <b>19</b>                      | <b>0</b> | <b>11</b> | <b>1</b> | <b>31</b>  | <b>10</b>       | <b>0</b> | <b>18</b> | <b>0</b> | <b>28</b>  | <b>85</b>  |
| Apprch %           | 38.9                          | 0        | 22.2     | 38.9     |            | 12.5            | 0        | 87.5     | 0        |            | 61.3                           | 0        | 35.5      | 3.2      |            | 35.7            | 0        | 64.3      | 0        |            |            |
| Total %            | 8.2                           | 0        | 4.7      | 8.2      | 21.2       | 1.2             | 0        | 8.2      | 0        | 9.4        | 22.4                           | 0        | 12.9      | 1.2      | 36.5       | 11.8            | 0        | 21.2      | 0        | 32.9       |            |

| Start Time   | Restaurant Parking Southbound |          |          |            | US 11 Westbound |          |          |            | University Park Dr. Northbound |          |          |            | US 11 Eastbound |          |           |            | Int. Total |
|--|-------------------------------|----------|----------|------------|-----------------|----------|----------|------------|--------------------------------|----------|----------|------------|-----------------|----------|-----------|------------|------------|
|  | Right                         | Thru     | Left     | App. Total | Right           | Thru     | Left     | App. Total | Right                          | Thru     | Left     | App. Total | Right           | Thru     | Left      | App. Total |            |
| Peak Hour Analysis From 03:00 PM to 05:45 PM - Peak 1 of 1 |                               |          |          |            |                 |          |          |            |                                |          |          |            |                 |          |           |            |            |
| Peak Hour for Entire Intersection Begins at 04:15 PM       |                               |          |          |            |                 |          |          |            |                                |          |          |            |                 |          |           |            |            |
| 04:15 PM   | 0                             | 0        | 1        | 1          | 0               | 0        | 1        | 1          | 2                              | 0        | 1        | 3          | 1               | 0        | 2         | 3          | 8          |
| 04:30 PM   | 0                             | 0        | 0        | 0          | 0               | 0        | 1        | 1          | 0                              | 0        | 2        | 2          | 0               | 0        | 2         | 2          | 5          |
| 04:45 PM   | 1                             | 0        | 0        | 1          | 0               | 0        | 0        | 0          | 2                              | 0        | 0        | 2          | 1               | 0        | 3         | 4          | 7          |
| 05:00 PM   | 1                             | 0        | 0        | 1          | 0               | 0        | 2        | 2          | 4                              | 0        | 2        | 6          | 2               | 0        | 4         | 6          | 15         |
| <b>Total Volume</b>  | <b>2</b>                      | <b>0</b> | <b>1</b> | <b>3</b>   | <b>0</b>        | <b>0</b> | <b>4</b> | <b>4</b>   | <b>8</b>                       | <b>0</b> | <b>5</b> | <b>13</b>  | <b>4</b>        | <b>0</b> | <b>11</b> | <b>15</b>  | <b>35</b>  |
| % App. Total   | 66.7                          | 0        | 33.3     |            | 0               | 0        | 100      |            | 61.5                           | 0        | 38.5     |            | 26.7            | 0        | 73.3      |            |            |
| PHF  | .500                          | .000     | .250     | .750       | .000            | .000     | .500     | .500       | .500                           | .000     | .625     | .542       | .500            | .000     | .688      | .625       | .583       |

# Peggy Malone & Associates, Inc.

## (888) 247-8602

File Name : TM 8-University Park and US 11 PM  
 Site Code :  
 Start Date : 2/4/2015  
 Page No : 1

Groups Printed- Truck

| Start Time         | Restaurant Parking Southbound |      |      |      |            | US 11 Westbound |      |      |      |            | University Park Dr. Northbound |      |      |      |            | US 11 Eastbound |      |      |      |            | Int. Total |
|--------------------|-------------------------------|------|------|------|------------|-----------------|------|------|------|------------|--------------------------------|------|------|------|------------|-----------------|------|------|------|------------|------------|
|                    | Right                         | Thru | Left | Peds | App. Total | Right           | Thru | Left | Peds | App. Total | Right                          | Thru | Left | Peds | App. Total | Right           | Thru | Left | Peds | App. Total |            |
| 03:00 PM           | 0                             | 0    | 0    | 0    | 0          | 0               | 0    | 1    | 0    | 1          | 1                              | 0    | 0    | 0    | 1          | 0               | 0    | 0    | 0    | 0          | 2          |
| 03:15 PM           | 0                             | 0    | 0    | 0    | 0          | 0               | 0    | 0    | 0    | 0          | 0                              | 0    | 0    | 0    | 0          | 0               | 0    | 0    | 0    | 0          | 0          |
| 03:30 PM           | 0                             | 0    | 0    | 0    | 0          | 0               | 0    | 0    | 0    | 0          | 0                              | 0    | 0    | 0    | 0          | 0               | 0    | 0    | 0    | 0          | 0          |
| 03:45 PM           | 0                             | 0    | 0    | 0    | 0          | 0               | 0    | 0    | 0    | 0          | 0                              | 0    | 0    | 0    | 0          | 0               | 0    | 0    | 0    | 0          | 0          |
| <b>Total</b>       | 0                             | 0    | 0    | 0    | 0          | 0               | 0    | 1    | 0    | 1          | 1                              | 0    | 0    | 0    | 1          | 0               | 0    | 0    | 0    | 0          | 2          |
| 04:00 PM           | 0                             | 0    | 0    | 0    | 0          | 0               | 0    | 0    | 0    | 0          | 0                              | 0    | 0    | 0    | 0          | 0               | 0    | 0    | 0    | 0          | 0          |
| 04:15 PM           | 0                             | 0    | 0    | 0    | 0          | 0               | 0    | 0    | 0    | 0          | 0                              | 0    | 0    | 0    | 0          | 0               | 0    | 0    | 0    | 0          | 0          |
| 04:30 PM           | 0                             | 0    | 0    | 0    | 0          | 0               | 0    | 0    | 0    | 0          | 0                              | 0    | 0    | 0    | 0          | 0               | 0    | 0    | 0    | 0          | 0          |
| 04:45 PM           | 0                             | 0    | 0    | 0    | 0          | 0               | 0    | 0    | 0    | 0          | 0                              | 0    | 0    | 0    | 0          | 0               | 0    | 0    | 0    | 0          | 0          |
| <b>Total</b>       | 0                             | 0    | 0    | 0    | 0          | 0               | 0    | 0    | 0    | 0          | 0                              | 0    | 0    | 0    | 0          | 0               | 0    | 0    | 0    | 0          | 0          |
| 05:00 PM           | 0                             | 0    | 0    | 0    | 0          | 0               | 0    | 0    | 0    | 0          | 0                              | 0    | 0    | 0    | 0          | 0               | 0    | 0    | 0    | 0          | 0          |
| 05:15 PM           | 0                             | 0    | 0    | 0    | 0          | 0               | 0    | 0    | 0    | 0          | 0                              | 0    | 0    | 0    | 0          | 0               | 0    | 0    | 0    | 0          | 0          |
| 05:30 PM           | 0                             | 0    | 0    | 0    | 0          | 0               | 0    | 0    | 0    | 0          | 0                              | 0    | 0    | 0    | 0          | 0               | 0    | 0    | 0    | 0          | 0          |
| 05:45 PM           | 0                             | 0    | 0    | 0    | 0          | 0               | 0    | 0    | 0    | 0          | 0                              | 0    | 0    | 0    | 0          | 0               | 0    | 0    | 0    | 0          | 0          |
| <b>Total</b>       | 0                             | 0    | 0    | 0    | 0          | 0               | 0    | 0    | 0    | 0          | 0                              | 0    | 0    | 0    | 0          | 0               | 0    | 0    | 0    | 0          | 0          |
| <b>Grand Total</b> | 0                             | 0    | 0    | 0    | 0          | 0               | 0    | 1    | 0    | 1          | 1                              | 0    | 0    | 0    | 1          | 0               | 0    | 0    | 0    | 0          | 2          |
| Apprch %           | 0                             | 0    | 0    | 0    |            | 0               | 0    | 100  | 0    |            | 100                            | 0    | 0    | 0    |            | 0               | 0    | 0    | 0    |            |            |
| Total %            | 0                             | 0    | 0    | 0    | 0          | 0               | 0    | 50   | 0    | 50         | 50                             | 0    | 0    | 0    | 50         | 0               | 0    | 0    | 0    | 0          |            |

| Start Time   | Restaurant Parking Southbound |      |      |            | US 11 Westbound |      |      |            | University Park Dr. Northbound |      |      |            | US 11 Eastbound |      |      |            | Int. Total |
|--|-------------------------------|------|------|------------|-----------------|------|------|------------|--------------------------------|------|------|------------|-----------------|------|------|------------|------------|
|  | Right                         | Thru | Left | App. Total | Right           | Thru | Left | App. Total | Right                          | Thru | Left | App. Total | Right           | Thru | Left | App. Total |            |
| Peak Hour Analysis From 03:00 PM to 05:45 PM - Peak 1 of 1 |                               |      |      |            |                 |      |      |            |                                |      |      |            |                 |      |      |            |            |
| Peak Hour for Entire Intersection Begins at 03:00 PM       |                               |      |      |            |                 |      |      |            |                                |      |      |            |                 |      |      |            |            |
| 03:00 PM   | 0                             | 0    | 0    | 0          | 0               | 0    | 1    | 1          | 1                              | 0    | 0    | 1          | 0               | 0    | 0    | 0          | 2          |
| 03:15 PM   | 0                             | 0    | 0    | 0          | 0               | 0    | 0    | 0          | 0                              | 0    | 0    | 0          | 0               | 0    | 0    | 0          | 0          |
| 03:30 PM   | 0                             | 0    | 0    | 0          | 0               | 0    | 0    | 0          | 0                              | 0    | 0    | 0          | 0               | 0    | 0    | 0          | 0          |
| 03:45 PM   | 0                             | 0    | 0    | 0          | 0               | 0    | 0    | 0          | 0                              | 0    | 0    | 0          | 0               | 0    | 0    | 0          | 0          |
| <b>Total Volume</b>  | 0                             | 0    | 0    | 0          | 0               | 0    | 1    | 1          | 1                              | 0    | 0    | 1          | 0               | 0    | 0    | 0          | 2          |
| % App. Total   | 0                             | 0    | 0    |            | 0               | 0    | 100  |            | 100                            | 0    | 0    |            | 0               | 0    | 0    |            |            |
| PHF  | .000                          | .000 | .000 | .000       | .000            | .000 | .250 | .250       | .250                           | .000 | .000 | .250       | .000            | .000 | .000 | .000       | .250       |



# Peggy Malone & Associates, Inc.

## (888) 247-8602

File Name : TM 8-University Park and US 11 PM  
 Site Code :  
 Start Date : 2/4/2015  
 Page No : 1

Groups Printed- Combined

| Start Time         | Restaurant Parking Southbound |          |          |          |            | US 11 Westbound |          |          |          |            | University Park Dr. Northbound |          |           |          |            | US 11 Eastbound |          |           |          |            | Int. Total |
|--------------------|-------------------------------|----------|----------|----------|------------|-----------------|----------|----------|----------|------------|--------------------------------|----------|-----------|----------|------------|-----------------|----------|-----------|----------|------------|------------|
|                    | Right                         | Thru     | Left     | Peds     | App. Total | Right           | Thru     | Left     | Peds     | App. Total | Right                          | Thru     | Left      | Peds     | App. Total | Right           | Thru     | Left      | Peds     | App. Total |            |
| 03:00 PM           | 4                             | 0        | 0        | 0        | 4          | 0               | 0        | 3        | 0        | 3          | 2                              | 0        | 1         | 0        | 3          | 0               | 0        | 0         | 0        | 0          | 10         |
| 03:15 PM           | 0                             | 0        | 0        | 0        | 0          | 0               | 0        | 0        | 0        | 0          | 0                              | 0        | 0         | 0        | 0          | 0               | 0        | 1         | 0        | 1          | 1          |
| 03:30 PM           | 0                             | 0        | 0        | 1        | 1          | 0               | 0        | 0        | 0        | 0          | 2                              | 0        | 2         | 0        | 4          | 0               | 0        | 0         | 0        | 0          | 5          |
| 03:45 PM           | 0                             | 0        | 2        | 1        | 3          | 0               | 0        | 0        | 0        | 0          | 4                              | 0        | 0         | 1        | 5          | 1               | 0        | 1         | 0        | 2          | 10         |
| <b>Total</b>       | <b>4</b>                      | <b>0</b> | <b>2</b> | <b>2</b> | <b>8</b>   | <b>0</b>        | <b>0</b> | <b>3</b> | <b>0</b> | <b>3</b>   | <b>8</b>                       | <b>0</b> | <b>3</b>  | <b>1</b> | <b>12</b>  | <b>1</b>        | <b>0</b> | <b>2</b>  | <b>0</b> | <b>3</b>   | <b>26</b>  |
| 04:00 PM           | 0                             | 0        | 0        | 0        | 0          | 0               | 0        | 0        | 0        | 0          | 0                              | 0        | 0         | 0        | 0          | 1               | 0        | 2         | 0        | 3          | 3          |
| 04:15 PM           | 0                             | 0        | 1        | 0        | 1          | 0               | 0        | 1        | 0        | 1          | 2                              | 0        | 1         | 0        | 3          | 1               | 0        | 2         | 0        | 3          | 8          |
| 04:30 PM           | 0                             | 0        | 0        | 0        | 0          | 0               | 0        | 1        | 0        | 1          | 0                              | 0        | 2         | 0        | 2          | 0               | 0        | 2         | 0        | 2          | 5          |
| 04:45 PM           | 1                             | 0        | 0        | 3        | 4          | 0               | 0        | 0        | 0        | 0          | 2                              | 0        | 0         | 0        | 2          | 1               | 0        | 3         | 0        | 4          | 10         |
| <b>Total</b>       | <b>1</b>                      | <b>0</b> | <b>1</b> | <b>3</b> | <b>5</b>   | <b>0</b>        | <b>0</b> | <b>2</b> | <b>0</b> | <b>2</b>   | <b>4</b>                       | <b>0</b> | <b>3</b>  | <b>0</b> | <b>7</b>   | <b>3</b>        | <b>0</b> | <b>9</b>  | <b>0</b> | <b>12</b>  | <b>26</b>  |
| 05:00 PM           | 1                             | 0        | 0        | 0        | 1          | 0               | 0        | 2        | 0        | 2          | 4                              | 0        | 2         | 0        | 6          | 2               | 0        | 4         | 0        | 6          | 15         |
| 05:15 PM           | 0                             | 0        | 0        | 0        | 0          | 1               | 0        | 1        | 0        | 2          | 2                              | 0        | 1         | 0        | 3          | 2               | 0        | 1         | 0        | 3          | 8          |
| 05:30 PM           | 0                             | 0        | 0        | 2        | 2          | 0               | 0        | 0        | 0        | 0          | 2                              | 0        | 0         | 0        | 2          | 1               | 0        | 1         | 0        | 2          | 6          |
| 05:45 PM           | 1                             | 0        | 1        | 0        | 2          | 0               | 0        | 0        | 0        | 0          | 0                              | 0        | 2         | 0        | 2          | 1               | 0        | 1         | 0        | 2          | 6          |
| <b>Total</b>       | <b>2</b>                      | <b>0</b> | <b>1</b> | <b>2</b> | <b>5</b>   | <b>1</b>        | <b>0</b> | <b>3</b> | <b>0</b> | <b>4</b>   | <b>8</b>                       | <b>0</b> | <b>5</b>  | <b>0</b> | <b>13</b>  | <b>6</b>        | <b>0</b> | <b>7</b>  | <b>0</b> | <b>13</b>  | <b>35</b>  |
| <b>Grand Total</b> | <b>7</b>                      | <b>0</b> | <b>4</b> | <b>7</b> | <b>18</b>  | <b>1</b>        | <b>0</b> | <b>8</b> | <b>0</b> | <b>9</b>   | <b>20</b>                      | <b>0</b> | <b>11</b> | <b>1</b> | <b>32</b>  | <b>10</b>       | <b>0</b> | <b>18</b> | <b>0</b> | <b>28</b>  | <b>87</b>  |
| Apprch %           | 38.9                          | 0        | 22.2     | 38.9     |            | 11.1            | 0        | 88.9     | 0        |            | 62.5                           | 0        | 34.4      | 3.1      |            | 35.7            | 0        | 64.3      | 0        |            |            |
| Total %            | 8                             | 0        | 4.6      | 8        | 20.7       | 1.1             | 0        | 9.2      | 0        | 10.3       | 23                             | 0        | 12.6      | 1.1      | 36.8       | 11.5            | 0        | 20.7      | 0        | 32.2       |            |

| Start Time   | Restaurant Parking Southbound |          |          |            | US 11 Westbound |          |          |            | University Park Dr. Northbound |          |          |            | US 11 Eastbound |          |           |            | Int. Total |
|--|-------------------------------|----------|----------|------------|-----------------|----------|----------|------------|--------------------------------|----------|----------|------------|-----------------|----------|-----------|------------|------------|
|  | Right                         | Thru     | Left     | App. Total | Right           | Thru     | Left     | App. Total | Right                          | Thru     | Left     | App. Total | Right           | Thru     | Left      | App. Total |            |
| Peak Hour Analysis From 03:00 PM to 05:45 PM - Peak 1 of 1 |                               |          |          |            |                 |          |          |            |                                |          |          |            |                 |          |           |            |            |
| Peak Hour for Entire Intersection Begins at 04:15 PM       |                               |          |          |            |                 |          |          |            |                                |          |          |            |                 |          |           |            |            |
| 04:15 PM   | 0                             | 0        | 1        | 1          | 0               | 0        | 1        | 1          | 2                              | 0        | 1        | 3          | 1               | 0        | 2         | 3          | 8          |
| 04:30 PM   | 0                             | 0        | 0        | 0          | 0               | 0        | 1        | 1          | 0                              | 0        | 2        | 2          | 0               | 0        | 2         | 2          | 5          |
| 04:45 PM   | 1                             | 0        | 0        | 1          | 0               | 0        | 0        | 0          | 2                              | 0        | 0        | 2          | 1               | 0        | 3         | 4          | 7          |
| 05:00 PM   | 1                             | 0        | 0        | 1          | 0               | 0        | 2        | 2          | 4                              | 0        | 2        | 6          | 2               | 0        | 4         | 6          | 15         |
| <b>Total Volume</b>  | <b>2</b>                      | <b>0</b> | <b>1</b> | <b>3</b>   | <b>0</b>        | <b>0</b> | <b>4</b> | <b>4</b>   | <b>8</b>                       | <b>0</b> | <b>5</b> | <b>13</b>  | <b>4</b>        | <b>0</b> | <b>11</b> | <b>15</b>  | <b>35</b>  |
| % App. Total   | 66.7                          | 0        | 33.3     |            | 0               | 0        | 100      |            | 61.5                           | 0        | 38.5     |            | 26.7            | 0        | 73.3      |            |            |
| PHF  | .500                          | .000     | .250     | .750       | .000            | .000     | .500     | .500       | .500                           | .000     | .625     | .542       | .500            | .000     | .688      | .625       | .583       |

# Peggy Malone & Associates, Inc.

## (888) 247-8602

File Name : TM 9-Nesselrod and US 11 AM  
 Site Code :  
 Start Date : 2/4/2015  
 Page No : 1

Groups Printed- Car

| Start Time         | Nesselrod Bus Drway Southbound |      |      |            | US 11 Westbound |      |      |            | US 11 Eastbound |      |      |            | Int. Total |
|--------------------|--------------------------------|------|------|------------|-----------------|------|------|------------|-----------------|------|------|------------|------------|
|                    | Right                          | Left | Peds | App. Total | Right           | Thru | Peds | App. Total | Thru            | Left | Peds | App. Total |            |
| 07:00 AM           | 0                              | 0    | 0    | 0          | 0               | 0    | 0    | 0          | 0               | 0    | 0    | 0          | 0          |
| 07:15 AM           | 0                              | 0    | 0    | 0          | 0               | 0    | 0    | 0          | 0               | 0    | 0    | 0          | 0          |
| 07:30 AM           | 0                              | 0    | 0    | 0          | 0               | 0    | 0    | 0          | 0               | 0    | 0    | 0          | 0          |
| 07:45 AM           | 0                              | 0    | 0    | 0          | 0               | 0    | 0    | 0          | 0               | 0    | 0    | 0          | 0          |
| <b>Total</b>       | 0                              | 0    | 0    | 0          | 0               | 0    | 0    | 0          | 0               | 0    | 0    | 0          | 0          |
| 08:00 AM           | 0                              | 1    | 0    | 1          | 0               | 0    | 0    | 0          | 0               | 1    | 0    | 1          | 2          |
| 08:15 AM           | 0                              | 0    | 0    | 0          | 0               | 0    | 0    | 0          | 0               | 0    | 0    | 0          | 0          |
| 08:30 AM           | 0                              | 0    | 0    | 0          | 0               | 0    | 0    | 0          | 0               | 0    | 0    | 0          | 0          |
| 08:45 AM           | 0                              | 0    | 0    | 0          | 0               | 0    | 0    | 0          | 0               | 0    | 0    | 0          | 0          |
| <b>Total</b>       | 0                              | 1    | 0    | 1          | 0               | 0    | 0    | 0          | 0               | 1    | 0    | 1          | 2          |
| <b>Grand Total</b> | 0                              | 1    | 0    | 1          | 0               | 0    | 0    | 0          | 0               | 1    | 0    | 1          | 2          |
| Apprch %           | 0                              | 100  | 0    |            | 0               | 0    | 0    |            | 0               | 100  | 0    |            |            |
| Total %            | 0                              | 50   | 0    | 50         | 0               | 0    | 0    | 0          | 0               | 50   | 0    | 50         |            |

| Start Time   | Nesselrod Bus Drway Southbound |      |            | US 11 Westbound |      |            | US 11 Eastbound |      |            | Int. Total |
|--|--------------------------------|------|------------|-----------------|------|------------|-----------------|------|------------|------------|
|  | Right                          | Left | App. Total | Right           | Thru | App. Total | Thru            | Left | App. Total |            |
| Peak Hour Analysis From 07:00 AM to 08:45 AM - Peak 1 of 1 |                                |      |            |                 |      |            |                 |      |            |            |
| Peak Hour for Entire Intersection Begins at 07:15 AM       |                                |      |            |                 |      |            |                 |      |            |            |
| 07:15 AM   | 0                              | 0    | 0          | 0               | 0    | 0          | 0               | 0    | 0          | 0          |
| 07:30 AM   | 0                              | 0    | 0          | 0               | 0    | 0          | 0               | 0    | 0          | 0          |
| 07:45 AM   | 0                              | 0    | 0          | 0               | 0    | 0          | 0               | 0    | 0          | 0          |
| 08:00 AM   | 0                              | 1    | 1          | 0               | 0    | 0          | 0               | 1    | 1          | 2          |
| <b>Total Volume</b>  | 0                              | 1    | 1          | 0               | 0    | 0          | 0               | 1    | 1          | 2          |
| <b>% App. Total</b>  | 0                              | 100  |            | 0               | 0    |            | 0               | 100  |            |            |
| PHF  | .000                           | .250 | .250       | .000            | .000 | .000       | .000            | .250 | .250       | .250       |



# Peggy Malone & Associates, Inc.

## (888) 247-8602

File Name : TM 9-Nesselrod and US 11 AM  
 Site Code :  
 Start Date : 2/4/2015  
 Page No : 1

### Groups Printed- Combined

| Start Time         | Nesselrod Bus Drway Southbound |      |      |            | US 11 Westbound |      |      |            | US 11 Eastbound |      |      |            | Int. Total |
|--------------------|--------------------------------|------|------|------------|-----------------|------|------|------------|-----------------|------|------|------------|------------|
|                    | Right                          | Left | Peds | App. Total | Right           | Thru | Peds | App. Total | Thru            | Left | Peds | App. Total |            |
| 07:00 AM           | 0                              | 0    | 0    | 0          | 0               | 0    | 0    | 0          | 0               | 0    | 0    | 0          | 0          |
| 07:15 AM           | 0                              | 0    | 0    | 0          | 0               | 0    | 0    | 0          | 0               | 0    | 0    | 0          | 0          |
| 07:30 AM           | 0                              | 0    | 0    | 0          | 0               | 0    | 0    | 0          | 0               | 0    | 0    | 0          | 0          |
| 07:45 AM           | 0                              | 0    | 0    | 0          | 0               | 0    | 0    | 0          | 0               | 0    | 0    | 0          | 0          |
| <b>Total</b>       | 0                              | 0    | 0    | 0          | 0               | 0    | 0    | 0          | 0               | 0    | 0    | 0          | 0          |
| 08:00 AM           | 0                              | 1    | 0    | 1          | 0               | 0    | 0    | 0          | 0               | 1    | 0    | 1          | 2          |
| 08:15 AM           | 0                              | 0    | 0    | 0          | 0               | 0    | 0    | 0          | 0               | 0    | 0    | 0          | 0          |
| 08:30 AM           | 0                              | 0    | 0    | 0          | 0               | 0    | 0    | 0          | 0               | 0    | 0    | 0          | 0          |
| 08:45 AM           | 0                              | 0    | 0    | 0          | 0               | 0    | 0    | 0          | 0               | 0    | 0    | 0          | 0          |
| <b>Total</b>       | 0                              | 1    | 0    | 1          | 0               | 0    | 0    | 0          | 0               | 1    | 0    | 1          | 2          |
| <b>Grand Total</b> | 0                              | 1    | 0    | 1          | 0               | 0    | 0    | 0          | 0               | 1    | 0    | 1          | 2          |
| Apprch %           | 0                              | 100  | 0    |            | 0               | 0    | 0    |            | 0               | 100  | 0    |            |            |
| Total %            | 0                              | 50   | 0    | 50         | 0               | 0    | 0    | 0          | 0               | 50   | 0    | 50         |            |

| Start Time   | Nesselrod Bus Drway Southbound |      |            | US 11 Westbound |      |            | US 11 Eastbound |      |            | Int. Total |
|--|--------------------------------|------|------------|-----------------|------|------------|-----------------|------|------------|------------|
|  | Right                          | Left | App. Total | Right           | Thru | App. Total | Thru            | Left | App. Total |            |
| Peak Hour Analysis From 07:00 AM to 08:45 AM - Peak 1 of 1 |                                |      |            |                 |      |            |                 |      |            |            |
| Peak Hour for Entire Intersection Begins at 07:15 AM       |                                |      |            |                 |      |            |                 |      |            |            |
| 07:15 AM   | 0                              | 0    | 0          | 0               | 0    | 0          | 0               | 0    | 0          | 0          |
| 07:30 AM   | 0                              | 0    | 0          | 0               | 0    | 0          | 0               | 0    | 0          | 0          |
| 07:45 AM   | 0                              | 0    | 0          | 0               | 0    | 0          | 0               | 0    | 0          | 0          |
| 08:00 AM   | 0                              | 1    | 1          | 0               | 0    | 0          | 0               | 1    | 1          | 2          |
| <b>Total Volume</b>  | 0                              | 1    | 1          | 0               | 0    | 0          | 0               | 1    | 1          | 2          |
| <b>% App. Total</b>  | 0                              | 100  |            | 0               | 0    |            | 0               | 100  |            |            |
| PHF  | .000                           | .250 | .250       | .000            | .000 | .000       | .000            | .250 | .250       | .250       |

# Peggy Malone & Associates, Inc.

## (888) 247-8602

File Name : TM 9-Nesselrod and US 11 MID  
 Site Code :  
 Start Date : 2/4/2015  
 Page No : 1

Groups Printed- Car

| Start Time         | Nesselrod Bus Drway Southbound |      |      |            | US 11 Westbound |      |      |            | US 11 Eastbound |      |      |            | Int. Total |
|--------------------|--------------------------------|------|------|------------|-----------------|------|------|------------|-----------------|------|------|------------|------------|
|                    | Right                          | Left | Peds | App. Total | Right           | Thru | Peds | App. Total | Thru            | Left | Peds | App. Total |            |
| 11:00 AM           | 0                              | 1    | 1    | 2          | 0               | 0    | 0    | 0          | 0               | 0    | 0    | 0          | 2          |
| 11:15 AM           | 0                              | 0    | 0    | 0          | 0               | 0    | 0    | 0          | 0               | 0    | 0    | 0          | 0          |
| 11:30 AM           | 1                              | 0    | 0    | 1          | 1               | 0    | 0    | 1          | 0               | 1    | 0    | 1          | 3          |
| 11:45 AM           | 0                              | 0    | 0    | 0          | 0               | 0    | 0    | 0          | 0               | 0    | 0    | 0          | 0          |
| <b>Total</b>       | 1                              | 1    | 1    | 3          | 1               | 0    | 0    | 1          | 0               | 1    | 0    | 1          | 5          |
| 12:00 PM           | 0                              | 0    | 0    | 0          | 0               | 0    | 0    | 0          | 0               | 2    | 0    | 2          | 2          |
| 12:15 PM           | 0                              | 0    | 0    | 0          | 0               | 0    | 0    | 0          | 0               | 0    | 0    | 0          | 0          |
| 12:30 PM           | 0                              | 0    | 1    | 1          | 0               | 0    | 0    | 0          | 0               | 0    | 0    | 0          | 1          |
| 12:45 PM           | 0                              | 0    | 1    | 1          | 0               | 0    | 0    | 0          | 0               | 0    | 0    | 0          | 1          |
| <b>Total</b>       | 0                              | 0    | 2    | 2          | 0               | 0    | 0    | 0          | 0               | 2    | 0    | 2          | 4          |
| <b>Grand Total</b> | 1                              | 1    | 3    | 5          | 1               | 0    | 0    | 1          | 0               | 3    | 0    | 3          | 9          |
| Apprch %           | 20                             | 20   | 60   |            | 100             | 0    | 0    |            | 0               | 100  | 0    |            |            |
| Total %            | 11.1                           | 11.1 | 33.3 | 55.6       | 11.1            | 0    | 0    | 11.1       | 0               | 33.3 | 0    | 33.3       |            |

| Start Time   | Nesselrod Bus Drway Southbound |      |            | US 11 Westbound |      |            | US 11 Eastbound |      |            | Int. Total |
|--|--------------------------------|------|------------|-----------------|------|------------|-----------------|------|------------|------------|
|  | Right                          | Left | App. Total | Right           | Thru | App. Total | Thru            | Left | App. Total |            |
| Peak Hour Analysis From 11:00 AM to 12:45 PM - Peak 1 of 1 |                                |      |            |                 |      |            |                 |      |            |            |
| Peak Hour for Entire Intersection Begins at 11:15 AM       |                                |      |            |                 |      |            |                 |      |            |            |
| 11:15 AM   | 0                              | 0    | 0          | 0               | 0    | 0          | 0               | 0    | 0          | 0          |
| 11:30 AM   | 1                              | 0    | 1          | 1               | 0    | 1          | 0               | 1    | 1          | 3          |
| 11:45 AM   | 0                              | 0    | 0          | 0               | 0    | 0          | 0               | 0    | 0          | 0          |
| 12:00 PM   | 0                              | 0    | 0          | 0               | 0    | 0          | 0               | 2    | 2          | 2          |
| <b>Total Volume</b>  | 1                              | 0    | 1          | 1               | 0    | 1          | 0               | 3    | 3          | 5          |
| % App. Total   | 100                            | 0    |            | 100             | 0    |            | 0               | 100  |            |            |
| PHF  | .250                           | .000 | .250       | .250            | .000 | .250       | .000            | .375 | .375       | .417       |



# Peggy Malone & Associates, Inc.

(888) 247-8602

File Name : TM 9-Nesselrod and US 11 MID  
 Site Code :  
 Start Date : 2/4/2015  
 Page No : 1

Groups Printed- Combined

| Start Time         | Nesselrod Bus Drway Southbound |      |      |            | US 11 Westbound |      |      |            | US 11 Eastbound |      |      |            | Int. Total |
|--------------------|--------------------------------|------|------|------------|-----------------|------|------|------------|-----------------|------|------|------------|------------|
|                    | Right                          | Left | Peds | App. Total | Right           | Thru | Peds | App. Total | Thru            | Left | Peds | App. Total |            |
| 11:00 AM           | 0                              | 1    | 1    | 2          | 0               | 0    | 0    | 0          | 0               | 0    | 0    | 0          | 2          |
| 11:15 AM           | 0                              | 0    | 0    | 0          | 0               | 0    | 0    | 0          | 0               | 0    | 0    | 0          | 0          |
| 11:30 AM           | 1                              | 0    | 0    | 1          | 1               | 0    | 0    | 1          | 0               | 1    | 0    | 1          | 3          |
| 11:45 AM           | 0                              | 0    | 0    | 0          | 0               | 0    | 0    | 0          | 0               | 0    | 0    | 0          | 0          |
| <b>Total</b>       | 1                              | 1    | 1    | 3          | 1               | 0    | 0    | 1          | 0               | 1    | 0    | 1          | 5          |
| 12:00 PM           | 0                              | 0    | 0    | 0          | 0               | 0    | 0    | 0          | 0               | 2    | 0    | 2          | 2          |
| 12:15 PM           | 0                              | 0    | 0    | 0          | 0               | 0    | 0    | 0          | 0               | 0    | 0    | 0          | 0          |
| 12:30 PM           | 0                              | 0    | 1    | 1          | 0               | 0    | 0    | 0          | 0               | 0    | 0    | 0          | 1          |
| 12:45 PM           | 0                              | 0    | 1    | 1          | 0               | 0    | 0    | 0          | 0               | 0    | 0    | 0          | 1          |
| <b>Total</b>       | 0                              | 0    | 2    | 2          | 0               | 0    | 0    | 0          | 0               | 2    | 0    | 2          | 4          |
| <b>Grand Total</b> | 1                              | 1    | 3    | 5          | 1               | 0    | 0    | 1          | 0               | 3    | 0    | 3          | 9          |
| Apprch %           | 20                             | 20   | 60   |            | 100             | 0    | 0    |            | 0               | 100  | 0    |            |            |
| Total %            | 11.1                           | 11.1 | 33.3 | 55.6       | 11.1            | 0    | 0    | 11.1       | 0               | 33.3 | 0    | 33.3       |            |

| Start Time   | Nesselrod Bus Drway Southbound |      |            | US 11 Westbound |      |            | US 11 Eastbound |      |            | Int. Total |
|--|--------------------------------|------|------------|-----------------|------|------------|-----------------|------|------------|------------|
|  | Right                          | Left | App. Total | Right           | Thru | App. Total | Thru            | Left | App. Total |            |
| Peak Hour Analysis From 11:00 AM to 12:45 PM - Peak 1 of 1 |                                |      |            |                 |      |            |                 |      |            |            |
| Peak Hour for Entire Intersection Begins at 11:15 AM       |                                |      |            |                 |      |            |                 |      |            |            |
| 11:15 AM   | 0                              | 0    | 0          | 0               | 0    | 0          | 0               | 0    | 0          | 0          |
| 11:30 AM   | 1                              | 0    | 1          | 1               | 0    | 1          | 0               | 1    | 1          | 3          |
| 11:45 AM   | 0                              | 0    | 0          | 0               | 0    | 0          | 0               | 0    | 0          | 0          |
| 12:00 PM   | 0                              | 0    | 0          | 0               | 0    | 0          | 0               | 2    | 2          | 2          |
| <b>Total Volume</b>  | 1                              | 0    | 1          | 1               | 0    | 1          | 0               | 3    | 3          | 5          |
| % App. Total   | 100                            | 0    |            | 100             | 0    |            | 0               | 100  |            |            |
| PHF  | .250                           | .000 | .250       | .250            | .000 | .250       | .000            | .375 | .375       | .417       |

# Peggy Malone & Associates, Inc.

## (888) 247-8602

File Name : TM 9-Nesselrod and US 11 PM  
 Site Code :  
 Start Date : 2/4/2015  
 Page No : 1

### Groups Printed- Car

| Start Time         | Nesselrod Bus Drway Southbound |          |          |            | US 11 Westbound |          |          |            | US 11 Eastbound |          |          |            | Int. Total |
|--------------------|--------------------------------|----------|----------|------------|-----------------|----------|----------|------------|-----------------|----------|----------|------------|------------|
|                    | Right                          | Left     | Peds     | App. Total | Right           | Thru     | Peds     | App. Total | Thru            | Left     | Peds     | App. Total |            |
| 03:00 PM           | 1                              | 0        | 0        | 1          | 0               | 0        | 0        | 0          | 0               | 0        | 0        | 0          | 1          |
| 03:15 PM           | 0                              | 1        | 0        | 1          | 1               | 0        | 0        | 1          | 0               | 0        | 0        | 0          | 2          |
| 03:30 PM           | 0                              | 1        | 0        | 1          | 0               | 0        | 0        | 0          | 0               | 0        | 0        | 0          | 1          |
| 03:45 PM           | 0                              | 2        | 0        | 2          | 1               | 0        | 0        | 1          | 0               | 0        | 0        | 0          | 3          |
| <b>Total</b>       | <b>1</b>                       | <b>4</b> | <b>0</b> | <b>5</b>   | <b>2</b>        | <b>0</b> | <b>0</b> | <b>2</b>   | <b>0</b>        | <b>0</b> | <b>0</b> | <b>0</b>   | <b>7</b>   |
| 04:00 PM           | 0                              | 0        | 0        | 0          | 1               | 0        | 0        | 1          | 0               | 0        | 0        | 0          | 1          |
| 04:15 PM           | 0                              | 2        | 0        | 2          | 0               | 0        | 0        | 0          | 0               | 0        | 0        | 0          | 2          |
| 04:30 PM           | 0                              | 2        | 0        | 2          | 0               | 0        | 0        | 0          | 0               | 0        | 0        | 0          | 2          |
| 04:45 PM           | 0                              | 0        | 0        | 0          | 0               | 0        | 0        | 0          | 0               | 0        | 0        | 0          | 0          |
| <b>Total</b>       | <b>0</b>                       | <b>4</b> | <b>0</b> | <b>4</b>   | <b>1</b>        | <b>0</b> | <b>0</b> | <b>1</b>   | <b>0</b>        | <b>0</b> | <b>0</b> | <b>0</b>   | <b>5</b>   |
| 05:00 PM           | 1                              | 0        | 0        | 1          | 0               | 0        | 0        | 0          | 0               | 2        | 0        | 2          | 3          |
| 05:15 PM           | 1                              | 0        | 0        | 1          | 1               | 0        | 0        | 1          | 0               | 6        | 0        | 6          | 8          |
| 05:30 PM           | 0                              | 0        | 0        | 0          | 1               | 0        | 0        | 1          | 0               | 0        | 0        | 0          | 1          |
| 05:45 PM           | 0                              | 0        | 0        | 0          | 0               | 0        | 0        | 0          | 0               | 1        | 0        | 1          | 1          |
| <b>Total</b>       | <b>2</b>                       | <b>0</b> | <b>0</b> | <b>2</b>   | <b>2</b>        | <b>0</b> | <b>0</b> | <b>2</b>   | <b>0</b>        | <b>9</b> | <b>0</b> | <b>9</b>   | <b>13</b>  |
| <b>Grand Total</b> | <b>3</b>                       | <b>8</b> | <b>0</b> | <b>11</b>  | <b>5</b>        | <b>0</b> | <b>0</b> | <b>5</b>   | <b>0</b>        | <b>9</b> | <b>0</b> | <b>9</b>   | <b>25</b>  |
| Apprch %           | 27.3                           | 72.7     | 0        |            | 100             | 0        | 0        |            | 0               | 100      | 0        |            |            |
| Total %            | 12                             | 32       | 0        | 44         | 20              | 0        | 0        | 20         | 0               | 36       | 0        | 36         |            |

| Start Time   | Nesselrod Bus Drway Southbound |          |            | US 11 Westbound |          |            | US 11 Eastbound |          |            | Int. Total |
|--|--------------------------------|----------|------------|-----------------|----------|------------|-----------------|----------|------------|------------|
|  | Right                          | Left     | App. Total | Right           | Thru     | App. Total | Thru            | Left     | App. Total |            |
| Peak Hour Analysis From 03:00 PM to 05:45 PM - Peak 1 of 1 |                                |          |            |                 |          |            |                 |          |            |            |
| Peak Hour for Entire Intersection Begins at 04:30 PM       |                                |          |            |                 |          |            |                 |          |            |            |
| 04:30 PM   | 0                              | 2        | 2          | 0               | 0        | 0          | 0               | 0        | 0          | 2          |
| 04:45 PM   | 0                              | 0        | 0          | 0               | 0        | 0          | 0               | 0        | 0          | 0          |
| 05:00 PM   | 1                              | 0        | 1          | 0               | 0        | 0          | 0               | 2        | 2          | 3          |
| 05:15 PM   | 1                              | 0        | 1          | 1               | 0        | 1          | 0               | 6        | 6          | 8          |
| <b>Total Volume</b>  | <b>2</b>                       | <b>2</b> | <b>4</b>   | <b>1</b>        | <b>0</b> | <b>1</b>   | <b>0</b>        | <b>8</b> | <b>8</b>   | <b>13</b>  |
| % App. Total   | 50                             | 50       |            | 100             | 0        |            | 0               | 100      |            |            |
| PHF  | .500                           | .250     | .500       | .250            | .000     | .250       | .000            | .333     | .333       | .406       |



# Peggy Malone & Associates, Inc.

## (888) 247-8602

File Name : TM 9-Nesselrod and US 11 PM  
 Site Code :  
 Start Date : 2/4/2015  
 Page No : 1

Groups Printed- Truck

| Start Time  | Nesselrod Bus Drway Southbound |      |      |            | US 11 Westbound |      |      |            | US 11 Eastbound |      |      |            | Int. Total |
|-------------|--------------------------------|------|------|------------|-----------------|------|------|------------|-----------------|------|------|------------|------------|
|             | Right                          | Left | Peds | App. Total | Right           | Thru | Peds | App. Total | Thru            | Left | Peds | App. Total |            |
| 03:00 PM    | 0                              | 0    | 0    | 0          | 0               | 0    | 0    | 0          | 0               | 0    | 0    | 0          | 0          |
| 03:15 PM    | 0                              | 0    | 0    | 0          | 0               | 0    | 0    | 0          | 0               | 0    | 0    | 0          | 0          |
| 03:30 PM    | 0                              | 0    | 0    | 0          | 0               | 0    | 0    | 0          | 0               | 0    | 0    | 0          | 0          |
| 03:45 PM    | 0                              | 0    | 0    | 0          | 0               | 0    | 0    | 0          | 0               | 0    | 0    | 0          | 0          |
| Total       | 0                              | 0    | 0    | 0          | 0               | 0    | 0    | 0          | 0               | 0    | 0    | 0          | 0          |
| 04:00 PM    | 0                              | 0    | 0    | 0          | 0               | 0    | 0    | 0          | 0               | 0    | 0    | 0          | 0          |
| 04:15 PM    | 0                              | 0    | 0    | 0          | 0               | 0    | 0    | 0          | 0               | 0    | 0    | 0          | 0          |
| 04:30 PM    | 0                              | 0    | 0    | 0          | 0               | 0    | 0    | 0          | 0               | 0    | 0    | 0          | 0          |
| 04:45 PM    | 0                              | 0    | 0    | 0          | 0               | 0    | 0    | 0          | 0               | 0    | 0    | 0          | 0          |
| Total       | 0                              | 0    | 0    | 0          | 0               | 0    | 0    | 0          | 0               | 0    | 0    | 0          | 0          |
| 05:00 PM    | 0                              | 0    | 0    | 0          | 0               | 0    | 0    | 0          | 0               | 0    | 0    | 0          | 0          |
| 05:15 PM    | 0                              | 0    | 0    | 0          | 0               | 0    | 0    | 0          | 0               | 0    | 0    | 0          | 0          |
| 05:30 PM    | 0                              | 0    | 0    | 0          | 0               | 0    | 0    | 0          | 0               | 0    | 0    | 0          | 0          |
| 05:45 PM    | 0                              | 0    | 0    | 0          | 0               | 0    | 0    | 0          | 0               | 0    | 0    | 0          | 0          |
| Total       | 0                              | 0    | 0    | 0          | 0               | 0    | 0    | 0          | 0               | 0    | 0    | 0          | 0          |
| Grand Total | 0                              | 0    | 0    | 0          | 0               | 0    | 0    | 0          | 0               | 0    | 0    | 0          | 0          |
| Apprch %    | 0                              | 0    | 0    |            | 0               | 0    | 0    |            | 0               | 0    | 0    |            |            |
| Total %     |                                |      |      |            |                 |      |      |            |                 |      |      |            |            |

| Start Time   | Nesselrod Bus Drway Southbound |      |            | US 11 Westbound |      |            | US 11 Eastbound |      |            | Int. Total |
|--|--------------------------------|------|------------|-----------------|------|------------|-----------------|------|------------|------------|
|  | Right                          | Left | App. Total | Right           | Thru | App. Total | Thru            | Left | App. Total |            |
| Peak Hour Analysis From 03:00 PM to 05:45 PM - Peak 1 of 1 |                                |      |            |                 |      |            |                 |      |            |            |
| Peak Hour for Entire Intersection Begins at 03:00 PM       |                                |      |            |                 |      |            |                 |      |            |            |
| 03:00 PM   | 0                              | 0    | 0          | 0               | 0    | 0          | 0               | 0    | 0          | 0          |
| 03:15 PM   | 0                              | 0    | 0          | 0               | 0    | 0          | 0               | 0    | 0          | 0          |
| 03:30 PM   | 0                              | 0    | 0          | 0               | 0    | 0          | 0               | 0    | 0          | 0          |
| 03:45 PM   | 0                              | 0    | 0          | 0               | 0    | 0          | 0               | 0    | 0          | 0          |
| Total Volume   | 0                              | 0    | 0          | 0               | 0    | 0          | 0               | 0    | 0          | 0          |
| % App. Total   | 0                              | 0    |            | 0               | 0    |            | 0               | 0    |            |            |
| PHF  | .000                           | .000 | .000       | .000            | .000 | .000       | .000            | .000 | .000       | .000       |

# Peggy Malone & Associates, Inc.

## (888) 247-8602

File Name : TM 9-Nesselrod and US 11 PM  
 Site Code :  
 Start Date : 2/4/2015  
 Page No : 1

### Groups Printed- Combined

| Start Time         | Nesselrod Bus Drway Southbound |          |          |            | US 11 Westbound |          |          |            | US 11 Eastbound |          |          |            | Int. Total |
|--------------------|--------------------------------|----------|----------|------------|-----------------|----------|----------|------------|-----------------|----------|----------|------------|------------|
|                    | Right                          | Left     | Peds     | App. Total | Right           | Thru     | Peds     | App. Total | Thru            | Left     | Peds     | App. Total |            |
| 03:00 PM           | 1                              | 0        | 0        | 1          | 0               | 0        | 0        | 0          | 0               | 0        | 0        | 0          | 1          |
| 03:15 PM           | 0                              | 1        | 0        | 1          | 1               | 0        | 0        | 1          | 0               | 0        | 0        | 0          | 2          |
| 03:30 PM           | 0                              | 1        | 0        | 1          | 0               | 0        | 0        | 0          | 0               | 0        | 0        | 0          | 1          |
| 03:45 PM           | 0                              | 2        | 0        | 2          | 1               | 0        | 0        | 1          | 0               | 0        | 0        | 0          | 3          |
| <b>Total</b>       | <b>1</b>                       | <b>4</b> | <b>0</b> | <b>5</b>   | <b>2</b>        | <b>0</b> | <b>0</b> | <b>2</b>   | <b>0</b>        | <b>0</b> | <b>0</b> | <b>0</b>   | <b>7</b>   |
| 04:00 PM           | 0                              | 0        | 0        | 0          | 1               | 0        | 0        | 1          | 0               | 0        | 0        | 0          | 1          |
| 04:15 PM           | 0                              | 2        | 0        | 2          | 0               | 0        | 0        | 0          | 0               | 0        | 0        | 0          | 2          |
| 04:30 PM           | 0                              | 2        | 0        | 2          | 0               | 0        | 0        | 0          | 0               | 0        | 0        | 0          | 2          |
| 04:45 PM           | 0                              | 0        | 0        | 0          | 0               | 0        | 0        | 0          | 0               | 0        | 0        | 0          | 0          |
| <b>Total</b>       | <b>0</b>                       | <b>4</b> | <b>0</b> | <b>4</b>   | <b>1</b>        | <b>0</b> | <b>0</b> | <b>1</b>   | <b>0</b>        | <b>0</b> | <b>0</b> | <b>0</b>   | <b>5</b>   |
| 05:00 PM           | 1                              | 0        | 0        | 1          | 0               | 0        | 0        | 0          | 0               | 2        | 0        | 2          | 3          |
| 05:15 PM           | 1                              | 0        | 0        | 1          | 1               | 0        | 0        | 1          | 0               | 6        | 0        | 6          | 8          |
| 05:30 PM           | 0                              | 0        | 0        | 0          | 1               | 0        | 0        | 1          | 0               | 0        | 0        | 0          | 1          |
| 05:45 PM           | 0                              | 0        | 0        | 0          | 0               | 0        | 0        | 0          | 0               | 1        | 0        | 1          | 1          |
| <b>Total</b>       | <b>2</b>                       | <b>0</b> | <b>0</b> | <b>2</b>   | <b>2</b>        | <b>0</b> | <b>0</b> | <b>2</b>   | <b>0</b>        | <b>9</b> | <b>0</b> | <b>9</b>   | <b>13</b>  |
| <b>Grand Total</b> | <b>3</b>                       | <b>8</b> | <b>0</b> | <b>11</b>  | <b>5</b>        | <b>0</b> | <b>0</b> | <b>5</b>   | <b>0</b>        | <b>9</b> | <b>0</b> | <b>9</b>   | <b>25</b>  |
| Apprch %           | 27.3                           | 72.7     | 0        |            | 100             | 0        | 0        |            | 0               | 100      | 0        |            |            |
| Total %            | 12                             | 32       | 0        | 44         | 20              | 0        | 0        | 20         | 0               | 36       | 0        | 36         |            |

| Start Time   | Nesselrod Bus Drway Southbound |          |            | US 11 Westbound |          |            | US 11 Eastbound |          |            | Int. Total |
|--|--------------------------------|----------|------------|-----------------|----------|------------|-----------------|----------|------------|------------|
|  | Right                          | Left     | App. Total | Right           | Thru     | App. Total | Thru            | Left     | App. Total |            |
| Peak Hour Analysis From 03:00 PM to 05:45 PM - Peak 1 of 1 |                                |          |            |                 |          |            |                 |          |            |            |
| Peak Hour for Entire Intersection Begins at 04:30 PM       |                                |          |            |                 |          |            |                 |          |            |            |
| 04:30 PM   | 0                              | 2        | 2          | 0               | 0        | 0          | 0               | 0        | 0          | 2          |
| 04:45 PM   | 0                              | 0        | 0          | 0               | 0        | 0          | 0               | 0        | 0          | 0          |
| 05:00 PM   | 1                              | 0        | 1          | 0               | 0        | 0          | 0               | 2        | 2          | 3          |
| 05:15 PM   | 1                              | 0        | 1          | 1               | 0        | 1          | 0               | 6        | 6          | 8          |
| <b>Total Volume</b>  | <b>2</b>                       | <b>2</b> | <b>4</b>   | <b>1</b>        | <b>0</b> | <b>1</b>   | <b>0</b>        | <b>8</b> | <b>8</b>   | <b>13</b>  |
| % App. Total   | 50                             | 50       |            | 100             | 0        |            | 0               | 100      |            |            |
| PHF  | .500                           | .250     | .500       | .250            | .000     | .250       | .000            | .333     | .333       | .406       |

# Peggy Malone & Associates, Inc.

## (888) 247-8602

File Name : TM 10-Hazel Hollow and US 11 AM  
 Site Code :  
 Start Date : 2/4/2015  
 Page No : 1

Groups Printed- Car

| Start Time         | US 11 Westbound |      |      |            | Hazel Hollow Rd. Northbound |      |      |            | US 11 Eastbound |      |      |            | Int. Total |
|--------------------|-----------------|------|------|------------|-----------------------------|------|------|------------|-----------------|------|------|------------|------------|
|                    | Thru            | Left | Peds | App. Total | Right                       | Left | Peds | App. Total | Right           | Thru | Peds | App. Total |            |
| 07:00 AM           | 126             | 0    | 0    | 126        | 6                           | 12   | 0    | 18         | 3               | 115  | 0    | 118        | 262        |
| 07:15 AM           | 141             | 0    | 0    | 141        | 7                           | 8    | 0    | 15         | 4               | 186  | 0    | 190        | 346        |
| 07:30 AM           | 171             | 2    | 0    | 173        | 7                           | 7    | 0    | 14         | 2               | 222  | 0    | 224        | 411        |
| 07:45 AM           | 174             | 0    | 0    | 174        | 11                          | 4    | 0    | 15         | 0               | 245  | 0    | 245        | 434        |
| <b>Total</b>       | 612             | 2    | 0    | 614        | 31                          | 31   | 0    | 62         | 9               | 768  | 0    | 777        | 1453       |
| 08:00 AM           | 157             | 7    | 0    | 164        | 2                           | 7    | 0    | 9          | 1               | 204  | 0    | 205        | 378        |
| 08:15 AM           | 163             | 1    | 0    | 164        | 8                           | 3    | 0    | 11         | 1               | 200  | 0    | 201        | 376        |
| 08:30 AM           | 168             | 2    | 0    | 170        | 5                           | 4    | 1    | 10         | 2               | 162  | 0    | 164        | 344        |
| 08:45 AM           | 163             | 1    | 0    | 164        | 9                           | 4    | 0    | 13         | 4               | 166  | 0    | 170        | 347        |
| <b>Total</b>       | 651             | 11   | 0    | 662        | 24                          | 18   | 1    | 43         | 8               | 732  | 0    | 740        | 1445       |
| <b>Grand Total</b> | 1263            | 13   | 0    | 1276       | 55                          | 49   | 1    | 105        | 17              | 1500 | 0    | 1517       | 2898       |
| Apprch %           | 99              | 1    | 0    |            | 52.4                        | 46.7 | 1    |            | 1.1             | 98.9 | 0    |            |            |
| Total %            | 43.6            | 0.4  | 0    | 44         | 1.9                         | 1.7  | 0    | 3.6        | 0.6             | 51.8 | 0    | 52.3       |            |

| Start Time   | US 11 Westbound |      |            | Hazel Hollow Rd. Northbound |      |            | US 11 Eastbound |            |            | Int. Total |
|--|-----------------|------|------------|-----------------------------|------|------------|-----------------|------------|------------|------------|
|  | Thru            | Left | App. Total | Right                       | Left | App. Total | Right           | Thru       | App. Total |            |
| Peak Hour Analysis From 07:00 AM to 08:45 AM - Peak 1 of 1 |                 |      |            |                             |      |            |                 |            |            |            |
| Peak Hour for Entire Intersection Begins at 07:30 AM       |                 |      |            |                             |      |            |                 |            |            |            |
| 07:30 AM   | 171             | 2    | 173        | 7                           | 7    | 14         | 2               | 222        | 224        | 411        |
| 07:45 AM   | <b>174</b>      | 0    | <b>174</b> | <b>11</b>                   | 4    | <b>15</b>  | 0               | <b>245</b> | <b>245</b> | <b>434</b> |
| 08:00 AM   | 157             | 7    | 164        | 2                           | 7    | 9          | 1               | 204        | 205        | 378        |
| 08:15 AM   | 163             | 1    | 164        | 8                           | 3    | 11         | 1               | 200        | 201        | 376        |
| <b>Total Volume</b>  | 665             | 10   | 675        | 28                          | 21   | 49         | 4               | 871        | 875        | 1599       |
| % App. Total   | 98.5            | 1.5  |            | 57.1                        | 42.9 |            | 0.5             | 99.5       |            |            |
| PHF  | .955            | .357 | .970       | .636                        | .750 | .817       | .500            | .889       | .893       | .921       |

# Peggy Malone & Associates, Inc.

## (888) 247-8602

File Name : TM 10-Hazel Hollow and US 11 AM  
 Site Code :  
 Start Date : 2/4/2015  
 Page No : 1

Groups Printed- Truck

| Start Time         | US 11 Westbound |          |          |            | Hazel Hollow Rd. Northbound |          |          |            | US 11 Eastbound |           |          |            | Int. Total |
|--------------------|-----------------|----------|----------|------------|-----------------------------|----------|----------|------------|-----------------|-----------|----------|------------|------------|
|                    | Thru            | Left     | Peds     | App. Total | Right                       | Left     | Peds     | App. Total | Right           | Thru      | Peds     | App. Total |            |
| 07:00 AM           | 1               | 0        | 0        | 1          | 0                           | 0        | 0        | 0          | 0               | 1         | 0        | 1          | 2          |
| 07:15 AM           | 1               | 0        | 0        | 1          | 0                           | 0        | 0        | 0          | 0               | 3         | 0        | 3          | 4          |
| 07:30 AM           | 5               | 0        | 0        | 5          | 0                           | 0        | 0        | 0          | 0               | 1         | 0        | 1          | 6          |
| 07:45 AM           | 2               | 0        | 0        | 2          | 0                           | 0        | 0        | 0          | 0               | 1         | 0        | 1          | 3          |
| <b>Total</b>       | <b>9</b>        | <b>0</b> | <b>0</b> | <b>9</b>   | <b>0</b>                    | <b>0</b> | <b>0</b> | <b>0</b>   | <b>0</b>        | <b>6</b>  | <b>0</b> | <b>6</b>   | <b>15</b>  |
| 08:00 AM           | 5               | 0        | 0        | 5          | 0                           | 0        | 0        | 0          | 0               | 2         | 0        | 2          | 7          |
| 08:15 AM           | 6               | 2        | 0        | 8          | 0                           | 0        | 0        | 0          | 0               | 3         | 0        | 3          | 11         |
| 08:30 AM           | 3               | 1        | 0        | 4          | 0                           | 0        | 0        | 0          | 0               | 3         | 0        | 3          | 7          |
| 08:45 AM           | 5               | 0        | 0        | 5          | 1                           | 0        | 0        | 1          | 0               | 9         | 0        | 9          | 15         |
| <b>Total</b>       | <b>19</b>       | <b>3</b> | <b>0</b> | <b>22</b>  | <b>1</b>                    | <b>0</b> | <b>0</b> | <b>1</b>   | <b>0</b>        | <b>17</b> | <b>0</b> | <b>17</b>  | <b>40</b>  |
| <b>Grand Total</b> | <b>28</b>       | <b>3</b> | <b>0</b> | <b>31</b>  | <b>1</b>                    | <b>0</b> | <b>0</b> | <b>1</b>   | <b>0</b>        | <b>23</b> | <b>0</b> | <b>23</b>  | <b>55</b>  |
| Apprch %           | 90.3            | 9.7      | 0        |            | 100                         | 0        | 0        |            | 0               | 100       | 0        |            |            |
| Total %            | 50.9            | 5.5      | 0        | 56.4       | 1.8                         | 0        | 0        | 1.8        | 0               | 41.8      | 0        | 41.8       |            |

| Start Time   | US 11 Westbound |          |            | Hazel Hollow Rd. Northbound |          |            | US 11 Eastbound |           |            | Int. Total |
|--|-----------------|----------|------------|-----------------------------|----------|------------|-----------------|-----------|------------|------------|
|  | Thru            | Left     | App. Total | Right                       | Left     | App. Total | Right           | Thru      | App. Total |            |
| Peak Hour Analysis From 07:00 AM to 08:45 AM - Peak 1 of 1 |                 |          |            |                             |          |            |                 |           |            |            |
| Peak Hour for Entire Intersection Begins at 08:00 AM       |                 |          |            |                             |          |            |                 |           |            |            |
| 08:00 AM   | 5               | 0        | 5          | 0                           | 0        | 0          | 0               | 2         | 2          | 7          |
| 08:15 AM   | 6               | 2        | 8          | 0                           | 0        | 0          | 0               | 3         | 3          | 11         |
| 08:30 AM   | 3               | 1        | 4          | 0                           | 0        | 0          | 0               | 3         | 3          | 7          |
| 08:45 AM   | 5               | 0        | 5          | 1                           | 0        | 1          | 0               | 9         | 9          | 15         |
| <b>Total Volume</b>  | <b>19</b>       | <b>3</b> | <b>22</b>  | <b>1</b>                    | <b>0</b> | <b>1</b>   | <b>0</b>        | <b>17</b> | <b>17</b>  | <b>40</b>  |
| % App. Total   | 86.4            | 13.6     |            | 100                         | 0        |            | 0               | 100       |            |            |
| PHF  | .792            | .375     | .688       | .250                        | .000     | .250       | .000            | .472      | .472       | .667       |

# Peggy Malone & Associates, Inc.

## (888) 247-8602

File Name : TM 10-Hazel Hollow and US 11 AM  
 Site Code :  
 Start Date : 2/4/2015  
 Page No : 1

### Groups Printed- Combined

| Start Time         | US 11 Westbound |           |          |             | Hazel Hollow Rd. Northbound |           |          |            | US 11 Eastbound |             |          |             | Int. Total  |
|--------------------|-----------------|-----------|----------|-------------|-----------------------------|-----------|----------|------------|-----------------|-------------|----------|-------------|-------------|
|                    | Thru            | Left      | Peds     | App. Total  | Right                       | Left      | Peds     | App. Total | Right           | Thru        | Peds     | App. Total  |             |
| 07:00 AM           | 127             | 0         | 0        | 127         | 6                           | 12        | 0        | 18         | 3               | 116         | 0        | 119         | 264         |
| 07:15 AM           | 142             | 0         | 0        | 142         | 7                           | 8         | 0        | 15         | 4               | 189         | 0        | 193         | 350         |
| 07:30 AM           | 176             | 2         | 0        | 178         | 7                           | 7         | 0        | 14         | 2               | 223         | 0        | 225         | 417         |
| 07:45 AM           | 176             | 0         | 0        | 176         | 11                          | 4         | 0        | 15         | 0               | 246         | 0        | 246         | 437         |
| <b>Total</b>       | <b>621</b>      | <b>2</b>  | <b>0</b> | <b>623</b>  | <b>31</b>                   | <b>31</b> | <b>0</b> | <b>62</b>  | <b>9</b>        | <b>774</b>  | <b>0</b> | <b>783</b>  | <b>1468</b> |
| 08:00 AM           | 162             | 7         | 0        | 169         | 2                           | 7         | 0        | 9          | 1               | 206         | 0        | 207         | 385         |
| 08:15 AM           | 169             | 3         | 0        | 172         | 8                           | 3         | 0        | 11         | 1               | 203         | 0        | 204         | 387         |
| 08:30 AM           | 171             | 3         | 0        | 174         | 5                           | 4         | 1        | 10         | 2               | 165         | 0        | 167         | 351         |
| 08:45 AM           | 168             | 1         | 0        | 169         | 10                          | 4         | 0        | 14         | 4               | 175         | 0        | 179         | 362         |
| <b>Total</b>       | <b>670</b>      | <b>14</b> | <b>0</b> | <b>684</b>  | <b>25</b>                   | <b>18</b> | <b>1</b> | <b>44</b>  | <b>8</b>        | <b>749</b>  | <b>0</b> | <b>757</b>  | <b>1485</b> |
| <b>Grand Total</b> | <b>1291</b>     | <b>16</b> | <b>0</b> | <b>1307</b> | <b>56</b>                   | <b>49</b> | <b>1</b> | <b>106</b> | <b>17</b>       | <b>1523</b> | <b>0</b> | <b>1540</b> | <b>2953</b> |
| Apprch %           | 98.8            | 1.2       | 0        |             | 52.8                        | 46.2      | 0.9      |            | 1.1             | 98.9        | 0        |             |             |
| Total %            | 43.7            | 0.5       | 0        | 44.3        | 1.9                         | 1.7       | 0        | 3.6        | 0.6             | 51.6        | 0        | 52.2        |             |

| Start Time   | US 11 Westbound |           |            | Hazel Hollow Rd. Northbound |           |            | US 11 Eastbound |            |            | Int. Total  |
|--|-----------------|-----------|------------|-----------------------------|-----------|------------|-----------------|------------|------------|-------------|
|  | Thru            | Left      | App. Total | Right                       | Left      | App. Total | Right           | Thru       | App. Total |             |
| Peak Hour Analysis From 07:00 AM to 08:45 AM - Peak 1 of 1 |                 |           |            |                             |           |            |                 |            |            |             |
| Peak Hour for Entire Intersection Begins at 07:30 AM       |                 |           |            |                             |           |            |                 |            |            |             |
| 07:30 AM   | <b>176</b>      | 2         | <b>178</b> | 7                           | <b>7</b>  | 14         | <b>2</b>        | 223        | 225        | 417         |
| 07:45 AM   | 176             | 0         | 176        | <b>11</b>                   | 4         | <b>15</b>  | 0               | <b>246</b> | <b>246</b> | <b>437</b>  |
| 08:00 AM   | 162             | <b>7</b>  | 169        | 2                           | 7         | 9          | 1               | 206        | 207        | 385         |
| 08:15 AM   | 169             | 3         | 172        | 8                           | 3         | 11         | 1               | 203        | 204        | 387         |
| <b>Total Volume</b>  | <b>683</b>      | <b>12</b> | <b>695</b> | <b>28</b>                   | <b>21</b> | <b>49</b>  | <b>4</b>        | <b>878</b> | <b>882</b> | <b>1626</b> |
| % App. Total   | 98.3            | 1.7       |            | 57.1                        | 42.9      |            | 0.5             | 99.5       |            |             |
| PHF  | .970            | .429      | .976       | .636                        | .750      | .817       | .500            | .892       | .896       | .930        |

# Peggy Malone & Associates, Inc.

## (888) 247-8602

File Name : TM 10-Hazel Hollow and US 11 MID  
 Site Code :  
 Start Date : 2/4/2015  
 Page No : 1

Groups Printed- Car

| Start Time         | US 11 Westbound |           |          |             | Hazel Hollow Rd. Northbound |           |          |            | US 11 Eastbound |             |          |             | Int. Total  |
|--------------------|-----------------|-----------|----------|-------------|-----------------------------|-----------|----------|------------|-----------------|-------------|----------|-------------|-------------|
|                    | Thru            | Left      | Peds     | App. Total  | Right                       | Left      | Peds     | App. Total | Right           | Thru        | Peds     | App. Total  |             |
| 11:00 AM           | 206             | 4         | 0        | 210         | 3                           | 4         | 0        | 7          | 4               | 161         | 0        | 165         | 382         |
| 11:15 AM           | 207             | 3         | 0        | 210         | 1                           | 6         | 0        | 7          | 5               | 180         | 0        | 185         | 402         |
| 11:30 AM           | 214             | 4         | 0        | 218         | 0                           | 1         | 0        | 1          | 3               | 191         | 0        | 194         | 413         |
| 11:45 AM           | 234             | 3         | 0        | 237         | 3                           | 3         | 0        | 6          | 3               | 181         | 0        | 184         | 427         |
| <b>Total</b>       | <b>861</b>      | <b>14</b> | <b>0</b> | <b>875</b>  | <b>7</b>                    | <b>14</b> | <b>0</b> | <b>21</b>  | <b>15</b>       | <b>713</b>  | <b>0</b> | <b>728</b>  | <b>1624</b> |
| 12:00 PM           | 284             | 5         | 0        | 289         | 2                           | 4         | 0        | 6          | 5               | 214         | 0        | 219         | 514         |
| 12:15 PM           | 252             | 7         | 0        | 259         | 10                          | 2         | 0        | 12         | 4               | 249         | 0        | 253         | 524         |
| 12:30 PM           | 190             | 3         | 0        | 193         | 9                           | 7         | 0        | 16         | 6               | 252         | 0        | 258         | 467         |
| 12:45 PM           | 229             | 4         | 0        | 233         | 6                           | 2         | 0        | 8          | 4               | 245         | 0        | 249         | 490         |
| <b>Total</b>       | <b>955</b>      | <b>19</b> | <b>0</b> | <b>974</b>  | <b>27</b>                   | <b>15</b> | <b>0</b> | <b>42</b>  | <b>19</b>       | <b>960</b>  | <b>0</b> | <b>979</b>  | <b>1995</b> |
| <b>Grand Total</b> | <b>1816</b>     | <b>33</b> | <b>0</b> | <b>1849</b> | <b>34</b>                   | <b>29</b> | <b>0</b> | <b>63</b>  | <b>34</b>       | <b>1673</b> | <b>0</b> | <b>1707</b> | <b>3619</b> |
| Apprch %           | 98.2            | 1.8       | 0        |             | 54                          | 46        | 0        |            | 2               | 98          | 0        |             |             |
| Total %            | 50.2            | 0.9       | 0        | 51.1        | 0.9                         | 0.8       | 0        | 1.7        | 0.9             | 46.2        | 0        | 47.2        |             |

| Start Time   | US 11 Westbound |           |            | Hazel Hollow Rd. Northbound |           |            | US 11 Eastbound |            |            | Int. Total  |
|--|-----------------|-----------|------------|-----------------------------|-----------|------------|-----------------|------------|------------|-------------|
|  | Thru            | Left      | App. Total | Right                       | Left      | App. Total | Right           | Thru       | App. Total |             |
| Peak Hour Analysis From 11:00 AM to 12:45 PM - Peak 1 of 1 |                 |           |            |                             |           |            |                 |            |            |             |
| Peak Hour for Entire Intersection Begins at 12:00 PM       |                 |           |            |                             |           |            |                 |            |            |             |
| 12:00 PM   | <b>284</b>      | 5         | <b>289</b> | 2                           | 4         | 6          | 5               | 214        | 219        | 514         |
| 12:15 PM   | 252             | 7         | 259        | 10                          | 2         | 12         | 4               | 249        | 253        | 524         |
| 12:30 PM   | 190             | 3         | 193        | 9                           | 7         | 16         | 6               | 252        | 258        | 467         |
| 12:45 PM   | 229             | 4         | 233        | 6                           | 2         | 8          | 4               | 245        | 249        | 490         |
| <b>Total Volume</b>  | <b>955</b>      | <b>19</b> | <b>974</b> | <b>27</b>                   | <b>15</b> | <b>42</b>  | <b>19</b>       | <b>960</b> | <b>979</b> | <b>1995</b> |
| % App. Total   | 98              | 2         |            | 64.3                        | 35.7      |            | 1.9             | 98.1       |            |             |
| PHF  | .841            | .679      | .843       | .675                        | .536      | .656       | .792            | .952       | .949       | .952        |

# Peggy Malone & Associates, Inc.

## (888) 247-8602

File Name : TM 10-Hazel Hollow and US 11 MID  
 Site Code :  
 Start Date : 2/4/2015  
 Page No : 1

Groups Printed- Truck

| Start Time         | US 11 Westbound |      |      |            | Hazel Hollow Rd. Northbound |      |      |            | US 11 Eastbound |      |      |            | Int. Total |
|--------------------|-----------------|------|------|------------|-----------------------------|------|------|------------|-----------------|------|------|------------|------------|
|                    | Thru            | Left | Peds | App. Total | Right                       | Left | Peds | App. Total | Right           | Thru | Peds | App. Total |            |
| 11:00 AM           | 4               | 0    | 0    | 4          | 0                           | 0    | 0    | 0          | 0               | 6    | 0    | 6          | 10         |
| 11:15 AM           | 4               | 0    | 0    | 4          | 1                           | 0    | 0    | 1          | 0               | 2    | 0    | 2          | 7          |
| 11:30 AM           | 6               | 0    | 0    | 6          | 1                           | 2    | 0    | 3          | 0               | 4    | 0    | 4          | 13         |
| 11:45 AM           | 3               | 0    | 0    | 3          | 0                           | 0    | 0    | 0          | 1               | 7    | 0    | 8          | 11         |
| <b>Total</b>       | 17              | 0    | 0    | 17         | 2                           | 2    | 0    | 4          | 1               | 19   | 0    | 20         | 41         |
| 12:00 PM           | 5               | 0    | 0    | 5          | 0                           | 0    | 0    | 0          | 0               | 4    | 0    | 4          | 9          |
| 12:15 PM           | 2               | 0    | 0    | 2          | 0                           | 0    | 0    | 0          | 0               | 7    | 0    | 7          | 9          |
| 12:30 PM           | 4               | 2    | 0    | 6          | 0                           | 1    | 0    | 1          | 0               | 4    | 0    | 4          | 11         |
| 12:45 PM           | 4               | 0    | 0    | 4          | 0                           | 0    | 0    | 0          | 1               | 6    | 0    | 7          | 11         |
| <b>Total</b>       | 15              | 2    | 0    | 17         | 0                           | 1    | 0    | 1          | 1               | 21   | 0    | 22         | 40         |
| <b>Grand Total</b> | 32              | 2    | 0    | 34         | 2                           | 3    | 0    | 5          | 2               | 40   | 0    | 42         | 81         |
| Apprch %           | 94.1            | 5.9  | 0    |            | 40                          | 60   | 0    |            | 4.8             | 95.2 | 0    |            |            |
| Total %            | 39.5            | 2.5  | 0    | 42         | 2.5                         | 3.7  | 0    | 6.2        | 2.5             | 49.4 | 0    | 51.9       |            |

| Start Time   | US 11 Westbound |      |            | Hazel Hollow Rd. Northbound |      |            | US 11 Eastbound |      |            | Int. Total |
|--|-----------------|------|------------|-----------------------------|------|------------|-----------------|------|------------|------------|
|  | Thru            | Left | App. Total | Right                       | Left | App. Total | Right           | Thru | App. Total |            |
| Peak Hour Analysis From 11:00 AM to 12:45 PM - Peak 1 of 1 |                 |      |            |                             |      |            |                 |      |            |            |
| Peak Hour for Entire Intersection Begins at 11:30 AM       |                 |      |            |                             |      |            |                 |      |            |            |
| 11:30 AM   | 6               | 0    | 6          | 1                           | 2    | 3          | 0               | 4    | 4          | 13         |
| 11:45 AM   | 3               | 0    | 3          | 0                           | 0    | 0          | 1               | 7    | 8          | 11         |
| 12:00 PM   | 5               | 0    | 5          | 0                           | 0    | 0          | 0               | 4    | 4          | 9          |
| 12:15 PM   | 2               | 0    | 2          | 0                           | 0    | 0          | 0               | 7    | 7          | 9          |
| <b>Total Volume</b>  | 16              | 0    | 16         | 1                           | 2    | 3          | 1               | 22   | 23         | 42         |
| <b>% App. Total</b>  | 100             | 0    |            | 33.3                        | 66.7 |            | 4.3             | 95.7 |            |            |
| <b>PHF</b>   | .667            | .000 | .667       | .250                        | .250 | .250       | .250            | .786 | .719       | .808       |

# Peggy Malone & Associates, Inc.

## (888) 247-8602

File Name : TM 10-Hazel Hollow and US 11 MID  
 Site Code :  
 Start Date : 2/4/2015  
 Page No : 1

Groups Printed- Combined

| Start Time         | US 11 Westbound |           |          |             | Hazel Hollow Rd. Northbound |           |          |            | US 11 Eastbound |             |          |             | Int. Total  |
|--------------------|-----------------|-----------|----------|-------------|-----------------------------|-----------|----------|------------|-----------------|-------------|----------|-------------|-------------|
|                    | Thru            | Left      | Peds     | App. Total  | Right                       | Left      | Peds     | App. Total | Right           | Thru        | Peds     | App. Total  |             |
| 11:00 AM           | 210             | 4         | 0        | 214         | 3                           | 4         | 0        | 7          | 4               | 167         | 0        | 171         | 392         |
| 11:15 AM           | 211             | 3         | 0        | 214         | 2                           | 6         | 0        | 8          | 5               | 182         | 0        | 187         | 409         |
| 11:30 AM           | 220             | 4         | 0        | 224         | 1                           | 3         | 0        | 4          | 3               | 195         | 0        | 198         | 426         |
| 11:45 AM           | 237             | 3         | 0        | 240         | 3                           | 3         | 0        | 6          | 4               | 188         | 0        | 192         | 438         |
| <b>Total</b>       | <b>878</b>      | <b>14</b> | <b>0</b> | <b>892</b>  | <b>9</b>                    | <b>16</b> | <b>0</b> | <b>25</b>  | <b>16</b>       | <b>732</b>  | <b>0</b> | <b>748</b>  | <b>1665</b> |
| 12:00 PM           | 289             | 5         | 0        | 294         | 2                           | 4         | 0        | 6          | 5               | 218         | 0        | 223         | 523         |
| 12:15 PM           | 254             | 7         | 0        | 261         | 10                          | 2         | 0        | 12         | 4               | 256         | 0        | 260         | 533         |
| 12:30 PM           | 194             | 5         | 0        | 199         | 9                           | 8         | 0        | 17         | 6               | 256         | 0        | 262         | 478         |
| 12:45 PM           | 233             | 4         | 0        | 237         | 6                           | 2         | 0        | 8          | 5               | 251         | 0        | 256         | 501         |
| <b>Total</b>       | <b>970</b>      | <b>21</b> | <b>0</b> | <b>991</b>  | <b>27</b>                   | <b>16</b> | <b>0</b> | <b>43</b>  | <b>20</b>       | <b>981</b>  | <b>0</b> | <b>1001</b> | <b>2035</b> |
| <b>Grand Total</b> | <b>1848</b>     | <b>35</b> | <b>0</b> | <b>1883</b> | <b>36</b>                   | <b>32</b> | <b>0</b> | <b>68</b>  | <b>36</b>       | <b>1713</b> | <b>0</b> | <b>1749</b> | <b>3700</b> |
| Apprch %           | 98.1            | 1.9       | 0        |             | 52.9                        | 47.1      | 0        |            | 2.1             | 97.9        | 0        |             |             |
| Total %            | 49.9            | 0.9       | 0        | 50.9        | 1                           | 0.9       | 0        | 1.8        | 1               | 46.3        | 0        | 47.3        |             |

| Start Time   | US 11 Westbound |           |            | Hazel Hollow Rd. Northbound |           |            | US 11 Eastbound |            |             | Int. Total  |
|--|-----------------|-----------|------------|-----------------------------|-----------|------------|-----------------|------------|-------------|-------------|
|  | Thru            | Left      | App. Total | Right                       | Left      | App. Total | Right           | Thru       | App. Total  |             |
| Peak Hour Analysis From 11:00 AM to 12:45 PM - Peak 1 of 1 |                 |           |            |                             |           |            |                 |            |             |             |
| Peak Hour for Entire Intersection Begins at 12:00 PM       |                 |           |            |                             |           |            |                 |            |             |             |
| 12:00 PM   | <b>289</b>      | 5         | <b>294</b> | 2                           | 4         | 6          | 5               | 218        | 223         | 523         |
| 12:15 PM   | 254             | <b>7</b>  | 261        | <b>10</b>                   | 2         | 12         | 4               | <b>256</b> | 260         | <b>533</b>  |
| 12:30 PM   | 194             | 5         | 199        | 9                           | <b>8</b>  | <b>17</b>  | <b>6</b>        | 256        | <b>262</b>  | 478         |
| 12:45 PM   | 233             | 4         | 237        | 6                           | 2         | 8          | 5               | 251        | 256         | 501         |
| <b>Total Volume</b>  | <b>970</b>      | <b>21</b> | <b>991</b> | <b>27</b>                   | <b>16</b> | <b>43</b>  | <b>20</b>       | <b>981</b> | <b>1001</b> | <b>2035</b> |
| % App. Total   | 97.9            | 2.1       |            | 62.8                        | 37.2      |            | 2               | 98         |             |             |
| PHF  | .839            | .750      | .843       | .675                        | .500      | .632       | .833            | .958       | .955        | .955        |



# Peggy Malone & Associates, Inc.

## (888) 247-8602

File Name : TM 10-Hazel Hollow and US 11 PM  
 Site Code :  
 Start Date : 2/4/2015  
 Page No : 1

Groups Printed- Car

| Start Time         | US 11 Westbound |           |          |             | Hazel Hollow Rd. Northbound |           |          |            | US 11 Eastbound |             |          |             | Int. Total  |
|--------------------|-----------------|-----------|----------|-------------|-----------------------------|-----------|----------|------------|-----------------|-------------|----------|-------------|-------------|
|                    | Thru            | Left      | Peds     | App. Total  | Right                       | Left      | Peds     | App. Total | Right           | Thru        | Peds     | App. Total  |             |
| 03:00 PM           | 276             | 6         | 0        | 282         | 3                           | 4         | 0        | 7          | 6               | 264         | 0        | 270         | 559         |
| 03:15 PM           | 255             | 5         | 0        | 260         | 5                           | 3         | 0        | 8          | 8               | 254         | 0        | 262         | 530         |
| 03:30 PM           | 322             | 6         | 0        | 328         | 4                           | 3         | 0        | 7          | 6               | 253         | 0        | 259         | 594         |
| 03:45 PM           | 294             | 9         | 0        | 303         | 8                           | 5         | 1        | 14         | 10              | 260         | 0        | 270         | 587         |
| <b>Total</b>       | <b>1147</b>     | <b>26</b> | <b>0</b> | <b>1173</b> | <b>20</b>                   | <b>15</b> | <b>1</b> | <b>36</b>  | <b>30</b>       | <b>1031</b> | <b>0</b> | <b>1061</b> | <b>2270</b> |
| 04:00 PM           | 301             | 10        | 0        | 311         | 9                           | 2         | 0        | 11         | 9               | 241         | 0        | 250         | 572         |
| 04:15 PM           | 239             | 6         | 0        | 245         | 7                           | 4         | 0        | 11         | 8               | 266         | 0        | 274         | 530         |
| 04:30 PM           | 326             | 14        | 0        | 340         | 8                           | 5         | 0        | 13         | 7               | 260         | 0        | 267         | 620         |
| 04:45 PM           | 303             | 7         | 0        | 310         | 4                           | 3         | 0        | 7          | 5               | 278         | 0        | 283         | 600         |
| <b>Total</b>       | <b>1169</b>     | <b>37</b> | <b>0</b> | <b>1206</b> | <b>28</b>                   | <b>14</b> | <b>0</b> | <b>42</b>  | <b>29</b>       | <b>1045</b> | <b>0</b> | <b>1074</b> | <b>2322</b> |
| 05:00 PM           | 375             | 12        | 0        | 387         | 9                           | 5         | 1        | 15         | 14              | 308         | 0        | 322         | 724         |
| 05:15 PM           | 382             | 10        | 0        | 392         | 9                           | 3         | 0        | 12         | 6               | 258         | 0        | 264         | 668         |
| 05:30 PM           | 280             | 10        | 0        | 290         | 5                           | 2         | 0        | 7          | 9               | 289         | 0        | 298         | 595         |
| 05:45 PM           | 252             | 4         | 0        | 256         | 4                           | 6         | 0        | 10         | 10              | 254         | 0        | 264         | 530         |
| <b>Total</b>       | <b>1289</b>     | <b>36</b> | <b>0</b> | <b>1325</b> | <b>27</b>                   | <b>16</b> | <b>1</b> | <b>44</b>  | <b>39</b>       | <b>1109</b> | <b>0</b> | <b>1148</b> | <b>2517</b> |
| <b>Grand Total</b> | <b>3605</b>     | <b>99</b> | <b>0</b> | <b>3704</b> | <b>75</b>                   | <b>45</b> | <b>2</b> | <b>122</b> | <b>98</b>       | <b>3185</b> | <b>0</b> | <b>3283</b> | <b>7109</b> |
| Apprch %           | 97.3            | 2.7       | 0        |             | 61.5                        | 36.9      | 1.6      |            | 3               | 97          | 0        |             |             |
| Total %            | 50.7            | 1.4       | 0        | 52.1        | 1.1                         | 0.6       | 0        | 1.7        | 1.4             | 44.8        | 0        | 46.2        |             |

| Start Time   | US 11 Westbound |           |            | Hazel Hollow Rd. Northbound |          |            | US 11 Eastbound |            |            | Int. Total |
|--|-----------------|-----------|------------|-----------------------------|----------|------------|-----------------|------------|------------|------------|
|  | Thru            | Left      | App. Total | Right                       | Left     | App. Total | Right           | Thru       | App. Total |            |
| Peak Hour Analysis From 03:00 PM to 05:45 PM - Peak 1 of 1 |                 |           |            |                             |          |            |                 |            |            |            |
| Peak Hour for Entire Intersection Begins at 04:30 PM       |                 |           |            |                             |          |            |                 |            |            |            |
| 04:30 PM   | 326             | <b>14</b> | 340        | 8                           | <b>5</b> | 13         | 7               | 260        | 267        | 620        |
| 04:45 PM   | 303             | 7         | 310        | 4                           | 3        | 7          | 5               | 278        | 283        | 600        |
| 05:00 PM   | 375             | 12        | 387        | <b>9</b>                    | 5        | <b>14</b>  | <b>14</b>       | <b>308</b> | <b>322</b> | <b>723</b> |
| 05:15 PM   | <b>382</b>      | 10        | <b>392</b> | 9                           | 3        | 12         | 6               | 258        | 264        | 668        |
| Total Volume   | 1386            | 43        | 1429       | 30                          | 16       | 46         | 32              | 1104       | 1136       | 2611       |
| % App. Total   | 97              | 3         |            | 65.2                        | 34.8     |            | 2.8             | 97.2       |            |            |
| PHF  | .907            | .768      | .911       | .833                        | .800     | .821       | .571            | .896       | .882       | .903       |

# Peggy Malone & Associates, Inc.

## (888) 247-8602

File Name : TM 10-Hazel Hollow and US 11 PM  
 Site Code :  
 Start Date : 2/4/2015  
 Page No : 1

Groups Printed- Truck

| Start Time         | US 11 Westbound |      |      |            | Hazel Hollow Rd. Northbound |      |      |            | US 11 Eastbound |      |      |            | Int. Total |
|--------------------|-----------------|------|------|------------|-----------------------------|------|------|------------|-----------------|------|------|------------|------------|
|                    | Thru            | Left | Peds | App. Total | Right                       | Left | Peds | App. Total | Right           | Thru | Peds | App. Total |            |
| 03:00 PM           | 8               | 0    | 0    | 8          | 0                           | 0    | 0    | 0          | 1               | 6    | 0    | 7          | 15         |
| 03:15 PM           | 4               | 0    | 0    | 4          | 0                           | 0    | 0    | 0          | 0               | 4    | 0    | 4          | 8          |
| 03:30 PM           | 4               | 0    | 0    | 4          | 0                           | 0    | 0    | 0          | 0               | 9    | 0    | 9          | 13         |
| 03:45 PM           | 3               | 0    | 0    | 3          | 0                           | 0    | 0    | 0          | 0               | 1    | 0    | 1          | 4          |
| <b>Total</b>       | 19              | 0    | 0    | 19         | 0                           | 0    | 0    | 0          | 1               | 20   | 0    | 21         | 40         |
| 04:00 PM           | 3               | 0    | 0    | 3          | 0                           | 0    | 0    | 0          | 0               | 5    | 0    | 5          | 8          |
| 04:15 PM           | 4               | 0    | 0    | 4          | 0                           | 0    | 0    | 0          | 0               | 5    | 0    | 5          | 9          |
| 04:30 PM           | 3               | 0    | 0    | 3          | 0                           | 0    | 0    | 0          | 1               | 4    | 0    | 5          | 8          |
| 04:45 PM           | 5               | 0    | 0    | 5          | 0                           | 0    | 0    | 0          | 0               | 0    | 0    | 0          | 5          |
| <b>Total</b>       | 15              | 0    | 0    | 15         | 0                           | 0    | 0    | 0          | 1               | 14   | 0    | 15         | 30         |
| 05:00 PM           | 3               | 0    | 0    | 3          | 1                           | 0    | 0    | 1          | 0               | 6    | 0    | 6          | 10         |
| 05:15 PM           | 1               | 0    | 0    | 1          | 0                           | 0    | 0    | 0          | 0               | 2    | 0    | 2          | 3          |
| 05:30 PM           | 1               | 0    | 0    | 1          | 0                           | 0    | 0    | 0          | 0               | 0    | 0    | 0          | 1          |
| 05:45 PM           | 4               | 0    | 0    | 4          | 0                           | 0    | 0    | 0          | 0               | 0    | 0    | 0          | 4          |
| <b>Total</b>       | 9               | 0    | 0    | 9          | 1                           | 0    | 0    | 1          | 0               | 8    | 0    | 8          | 18         |
| <b>Grand Total</b> | 43              | 0    | 0    | 43         | 1                           | 0    | 0    | 1          | 2               | 42   | 0    | 44         | 88         |
| Apprch %           | 100             | 0    | 0    |            | 100                         | 0    | 0    |            | 4.5             | 95.5 | 0    |            |            |
| Total %            | 48.9            | 0    | 0    | 48.9       | 1.1                         | 0    | 0    | 1.1        | 2.3             | 47.7 | 0    | 50         |            |

| Start Time   | US 11 Westbound |      |            | Hazel Hollow Rd. Northbound |      |            | US 11 Eastbound |      |            | Int. Total |
|--|-----------------|------|------------|-----------------------------|------|------------|-----------------|------|------------|------------|
|  | Thru            | Left | App. Total | Right                       | Left | App. Total | Right           | Thru | App. Total |            |
| Peak Hour Analysis From 03:00 PM to 05:45 PM - Peak 1 of 1 |                 |      |            |                             |      |            |                 |      |            |            |
| Peak Hour for Entire Intersection Begins at 03:00 PM       |                 |      |            |                             |      |            |                 |      |            |            |
| 03:00 PM   | 8               | 0    | 8          | 0                           | 0    | 0          | 1               | 6    | 7          | 15         |
| 03:15 PM   | 4               | 0    | 4          | 0                           | 0    | 0          | 0               | 4    | 4          | 8          |
| 03:30 PM   | 4               | 0    | 4          | 0                           | 0    | 0          | 0               | 9    | 9          | 13         |
| 03:45 PM   | 3               | 0    | 3          | 0                           | 0    | 0          | 0               | 1    | 1          | 4          |
| <b>Total Volume</b>  | 19              | 0    | 19         | 0                           | 0    | 0          | 1               | 20   | 21         | 40         |
| <b>% App. Total</b>  | 100             | 0    |            | 0                           | 0    |            | 4.8             | 95.2 |            |            |
| PHF  | .594            | .000 | .594       | .000                        | .000 | .000       | .250            | .556 | .583       | .667       |

# Peggy Malone & Associates, Inc.

## (888) 247-8602

File Name : TM 10-Hazel Hollow and US 11 PM  
 Site Code :  
 Start Date : 2/4/2015  
 Page No : 1

### Groups Printed- Combined

| Start Time         | US 11 Westbound |           |          |             | Hazel Hollow Rd. Northbound |           |          |            | US 11 Eastbound |             |          |             | Int. Total  |
|--------------------|-----------------|-----------|----------|-------------|-----------------------------|-----------|----------|------------|-----------------|-------------|----------|-------------|-------------|
|                    | Thru            | Left      | Peds     | App. Total  | Right                       | Left      | Peds     | App. Total | Right           | Thru        | Peds     | App. Total  |             |
| 03:00 PM           | 284             | 6         | 0        | 290         | 3                           | 4         | 0        | 7          | 7               | 270         | 0        | 277         | 574         |
| 03:15 PM           | 259             | 5         | 0        | 264         | 5                           | 3         | 0        | 8          | 8               | 258         | 0        | 266         | 538         |
| 03:30 PM           | 326             | 6         | 0        | 332         | 4                           | 3         | 0        | 7          | 6               | 262         | 0        | 268         | 607         |
| 03:45 PM           | 297             | 9         | 0        | 306         | 8                           | 5         | 1        | 14         | 10              | 261         | 0        | 271         | 591         |
| <b>Total</b>       | <b>1166</b>     | <b>26</b> | <b>0</b> | <b>1192</b> | <b>20</b>                   | <b>15</b> | <b>1</b> | <b>36</b>  | <b>31</b>       | <b>1051</b> | <b>0</b> | <b>1082</b> | <b>2310</b> |
| 04:00 PM           | 304             | 10        | 0        | 314         | 9                           | 2         | 0        | 11         | 9               | 246         | 0        | 255         | 580         |
| 04:15 PM           | 243             | 6         | 0        | 249         | 7                           | 4         | 0        | 11         | 8               | 271         | 0        | 279         | 539         |
| 04:30 PM           | 329             | 14        | 0        | 343         | 8                           | 5         | 0        | 13         | 8               | 264         | 0        | 272         | 628         |
| 04:45 PM           | 308             | 7         | 0        | 315         | 4                           | 3         | 0        | 7          | 5               | 278         | 0        | 283         | 605         |
| <b>Total</b>       | <b>1184</b>     | <b>37</b> | <b>0</b> | <b>1221</b> | <b>28</b>                   | <b>14</b> | <b>0</b> | <b>42</b>  | <b>30</b>       | <b>1059</b> | <b>0</b> | <b>1089</b> | <b>2352</b> |
| 05:00 PM           | 378             | 12        | 0        | 390         | 10                          | 5         | 1        | 16         | 14              | 314         | 0        | 328         | 734         |
| 05:15 PM           | 383             | 10        | 0        | 393         | 9                           | 3         | 0        | 12         | 6               | 260         | 0        | 266         | 671         |
| 05:30 PM           | 281             | 10        | 0        | 291         | 5                           | 2         | 0        | 7          | 9               | 289         | 0        | 298         | 596         |
| 05:45 PM           | 256             | 4         | 0        | 260         | 4                           | 6         | 0        | 10         | 10              | 254         | 0        | 264         | 534         |
| <b>Total</b>       | <b>1298</b>     | <b>36</b> | <b>0</b> | <b>1334</b> | <b>28</b>                   | <b>16</b> | <b>1</b> | <b>45</b>  | <b>39</b>       | <b>1117</b> | <b>0</b> | <b>1156</b> | <b>2535</b> |
| <b>Grand Total</b> | <b>3648</b>     | <b>99</b> | <b>0</b> | <b>3747</b> | <b>76</b>                   | <b>45</b> | <b>2</b> | <b>123</b> | <b>100</b>      | <b>3227</b> | <b>0</b> | <b>3327</b> | <b>7197</b> |
| Apprch %           | 97.4            | 2.6       | 0        |             | 61.8                        | 36.6      | 1.6      |            | 3               | 97          | 0        |             |             |
| Total %            | 50.7            | 1.4       | 0        | 52.1        | 1.1                         | 0.6       | 0        | 1.7        | 1.4             | 44.8        | 0        | 46.2        |             |

| Start Time   | US 11 Westbound |           |            | Hazel Hollow Rd. Northbound |          |            | US 11 Eastbound |            |            | Int. Total |
|--|-----------------|-----------|------------|-----------------------------|----------|------------|-----------------|------------|------------|------------|
|  | Thru            | Left      | App. Total | Right                       | Left     | App. Total | Right           | Thru       | App. Total |            |
| Peak Hour Analysis From 03:00 PM to 05:45 PM - Peak 1 of 1 |                 |           |            |                             |          |            |                 |            |            |            |
| Peak Hour for Entire Intersection Begins at 04:30 PM       |                 |           |            |                             |          |            |                 |            |            |            |
| 04:30 PM   | 329             | <b>14</b> | 343        | 8                           | <b>5</b> | 13         | 8               | 264        | 272        | 628        |
| 04:45 PM   | 308             | 7         | 315        | 4                           | 3        | 7          | 5               | 278        | 283        | 605        |
| 05:00 PM   | 378             | 12        | 390        | <b>10</b>                   | 5        | <b>15</b>  | <b>14</b>       | <b>314</b> | <b>328</b> | <b>733</b> |
| 05:15 PM   | <b>383</b>      | 10        | <b>393</b> | 9                           | 3        | 12         | 6               | 260        | 266        | 671        |
| Total Volume   | 1398            | 43        | 1441       | 31                          | 16       | 47         | 33              | 1116       | 1149       | 2637       |
| % App. Total   | 97              | 3         |            | 66                          | 34       |            | 2.9             | 97.1       |            |            |
| PHF  | .913            | .768      | .917       | .775                        | .800     | .783       | .589            | .889       | .876       | .899       |

# Peggy Malone & Associates, Inc.

## (888) 247-8602

File Name : TM2-Cherry Ct. and US 11 TURNS ONLY with Uturns AM  
 Site Code :  
 Start Date : 2/4/2015  
 Page No : 1

Groups Printed- Car

| Start Time         | Business Driveway Southbound |          |          |          |            | US 11 Westbound |          |          |          |            | Cherry Ct. Northbound |          |          |          |            | US 11 Eastbound |          |          |          |            | Int. Total |
|--------------------|------------------------------|----------|----------|----------|------------|-----------------|----------|----------|----------|------------|-----------------------|----------|----------|----------|------------|-----------------|----------|----------|----------|------------|------------|
|                    | Right                        | Thru     | Left     | U-Turn   | App. Total | Right           | Thru     | Left     | U-Turn   | App. Total | Right                 | Thru     | Left     | U-Turn   | App. Total | Right           | Thru     | Left     | U-Turn   | App. Total |            |
| 07:00 AM           | 1                            | 0        | 0        | 0        | 1          | 0               | 0        | 0        | 0        | 0          | 0                     | 0        | 1        | 0        | 1          | 0               | 0        | 0        | 0        | 0          | 2          |
| 07:15 AM           | 2                            | 0        | 0        | 0        | 2          | 0               | 0        | 0        | 0        | 0          | 0                     | 0        | 0        | 0        | 0          | 0               | 0        | 0        | 1        | 1          | 3          |
| 07:30 AM           | 0                            | 0        | 0        | 0        | 0          | 0               | 0        | 0        | 0        | 0          | 1                     | 0        | 0        | 0        | 1          | 0               | 0        | 0        | 1        | 1          | 2          |
| 07:45 AM           | 0                            | 0        | 0        | 0        | 0          | 0               | 0        | 0        | 0        | 0          | 0                     | 0        | 0        | 1        | 1          | 1               | 0        | 0        | 1        | 2          | 3          |
| <b>Total</b>       | <b>3</b>                     | <b>0</b> | <b>0</b> | <b>0</b> | <b>3</b>   | <b>0</b>        | <b>0</b> | <b>0</b> | <b>0</b> | <b>0</b>   | <b>1</b>              | <b>0</b> | <b>1</b> | <b>1</b> | <b>3</b>   | <b>1</b>        | <b>0</b> | <b>0</b> | <b>3</b> | <b>4</b>   | <b>10</b>  |
| 08:00 AM           | 1                            | 0        | 0        | 0        | 1          | 0               | 0        | 0        | 0        | 0          | 2                     | 0        | 3        | 0        | 5          | 0               | 0        | 0        | 0        | 0          | 6          |
| 08:15 AM           | 0                            | 0        | 0        | 0        | 0          | 0               | 0        | 0        | 0        | 0          | 0                     | 0        | 0        | 0        | 0          | 0               | 0        | 0        | 0        | 0          | 0          |
| 08:30 AM           | 0                            | 0        | 0        | 0        | 0          | 0               | 0        | 0        | 0        | 0          | 0                     | 0        | 0        | 0        | 0          | 0               | 0        | 0        | 0        | 0          | 0          |
| 08:45 AM           | 0                            | 0        | 0        | 0        | 0          | 1               | 0        | 0        | 0        | 1          | 0                     | 0        | 0        | 0        | 0          | 0               | 0        | 2        | 0        | 2          | 3          |
| <b>Total</b>       | <b>1</b>                     | <b>0</b> | <b>0</b> | <b>0</b> | <b>1</b>   | <b>1</b>        | <b>0</b> | <b>0</b> | <b>0</b> | <b>1</b>   | <b>2</b>              | <b>0</b> | <b>3</b> | <b>0</b> | <b>5</b>   | <b>0</b>        | <b>0</b> | <b>2</b> | <b>0</b> | <b>2</b>   | <b>9</b>   |
| <b>Grand Total</b> | <b>4</b>                     | <b>0</b> | <b>0</b> | <b>0</b> | <b>4</b>   | <b>1</b>        | <b>0</b> | <b>0</b> | <b>0</b> | <b>1</b>   | <b>3</b>              | <b>0</b> | <b>4</b> | <b>1</b> | <b>8</b>   | <b>1</b>        | <b>0</b> | <b>2</b> | <b>3</b> | <b>6</b>   | <b>19</b>  |
| Apprch %           | 100                          | 0        | 0        | 0        |            | 100             | 0        | 0        | 0        |            | 37.5                  | 0        | 50       | 12.5     |            | 16.7            | 0        | 33.3     | 50       |            |            |
| Total %            | 21.1                         | 0        | 0        | 0        | 21.1       | 5.3             | 0        | 0        | 0        | 5.3        | 15.8                  | 0        | 21.1     | 5.3      | 42.1       | 5.3             | 0        | 10.5     | 15.8     | 31.6       |            |

| Start Time   | Business Driveway Southbound |          |          |            | US 11 Westbound |          |          |            | Cherry Ct. Northbound |          |          |            | US 11 Eastbound |          |          |            | Int. Total |
|--|------------------------------|----------|----------|------------|-----------------|----------|----------|------------|-----------------------|----------|----------|------------|-----------------|----------|----------|------------|------------|
|  | Right                        | Thru     | Left     | App. Total | Right           | Thru     | Left     | App. Total | Right                 | Thru     | Left     | App. Total | Right           | Thru     | Left     | App. Total |            |
| Peak Hour Analysis From 07:00 AM to 08:45 AM - Peak 1 of 1 |                              |          |          |            |                 |          |          |            |                       |          |          |            |                 |          |          |            |            |
| Peak Hour for Entire Intersection Begins at 07:15 AM       |                              |          |          |            |                 |          |          |            |                       |          |          |            |                 |          |          |            |            |
| 07:15 AM   | 2                            | 0        | 0        | 2          | 0               | 0        | 0        | 0          | 0                     | 0        | 0        | 0          | 0               | 0        | 0        | 0          | 2          |
| 07:30 AM   | 0                            | 0        | 0        | 0          | 0               | 0        | 0        | 0          | 1                     | 0        | 0        | 1          | 0               | 0        | 0        | 0          | 1          |
| 07:45 AM   | 0                            | 0        | 0        | 0          | 0               | 0        | 0        | 0          | 0                     | 0        | 0        | 0          | 1               | 0        | 0        | 1          | 1          |
| 08:00 AM   | 1                            | 0        | 0        | 1          | 0               | 0        | 0        | 0          | 2                     | 0        | 3        | 5          | 0               | 0        | 0        | 0          | 6          |
| <b>Total Volume</b>  | <b>3</b>                     | <b>0</b> | <b>0</b> | <b>3</b>   | <b>0</b>        | <b>0</b> | <b>0</b> | <b>0</b>   | <b>3</b>              | <b>0</b> | <b>3</b> | <b>6</b>   | <b>1</b>        | <b>0</b> | <b>0</b> | <b>1</b>   | <b>10</b>  |
| % App. Total   | 100                          | 0        | 0        |            | 0               | 0        | 0        |            | 50                    | 0        | 50       |            | 100             | 0        | 0        |            |            |
| PHF  | .375                         | .000     | .000     | .375       | .000            | .000     | .000     | .000       | .375                  | .000     | .250     | .300       | .250            | .000     | .000     | .250       | .417       |

# Peggy Malone & Associates, Inc.

## (888) 247-8602

File Name : TM2-Cherry Ct. and US 11 TURNS ONLY with Uturns AM  
 Site Code :  
 Start Date : 2/4/2015  
 Page No : 1

Groups Printed- Truck

| Start Time  | Business Driveway Southbound |      |      |        |            | US 11 Westbound |      |      |        |            | Cherry Ct. Northbound |      |      |        |            | US 11 Eastbound |      |      |        |            | Int. Total |
|-------------|------------------------------|------|------|--------|------------|-----------------|------|------|--------|------------|-----------------------|------|------|--------|------------|-----------------|------|------|--------|------------|------------|
|             | Right                        | Thru | Left | U-Turn | App. Total | Right           | Thru | Left | U-Turn | App. Total | Right                 | Thru | Left | U-Turn | App. Total | Right           | Thru | Left | U-Turn | App. Total |            |
| 07:00 AM    | 0                            | 0    | 0    | 0      | 0          | 0               | 0    | 0    | 0      | 0          | 0                     | 0    | 0    | 0      | 0          | 0               | 0    | 0    | 0      | 0          | 0          |
| 07:15 AM    | 0                            | 0    | 0    | 0      | 0          | 0               | 0    | 0    | 0      | 0          | 0                     | 0    | 0    | 0      | 0          | 0               | 0    | 0    | 0      | 0          | 0          |
| 07:30 AM    | 0                            | 0    | 0    | 0      | 0          | 0               | 0    | 0    | 0      | 0          | 0                     | 0    | 0    | 0      | 0          | 0               | 0    | 0    | 0      | 0          | 0          |
| 07:45 AM    | 0                            | 0    | 0    | 0      | 0          | 0               | 0    | 0    | 0      | 0          | 0                     | 0    | 0    | 0      | 0          | 0               | 0    | 0    | 0      | 0          | 0          |
| Total       | 0                            | 0    | 0    | 0      | 0          | 0               | 0    | 0    | 0      | 0          | 0                     | 0    | 0    | 0      | 0          | 0               | 0    | 0    | 0      | 0          | 0          |
| 08:00 AM    | 0                            | 0    | 0    | 0      | 0          | 0               | 0    | 0    | 0      | 0          | 0                     | 0    | 0    | 0      | 0          | 0               | 0    | 0    | 0      | 0          | 0          |
| 08:15 AM    | 0                            | 0    | 0    | 0      | 0          | 0               | 0    | 0    | 0      | 0          | 0                     | 0    | 0    | 0      | 0          | 0               | 0    | 0    | 0      | 0          | 0          |
| 08:30 AM    | 0                            | 0    | 0    | 0      | 0          | 0               | 0    | 0    | 0      | 0          | 0                     | 0    | 0    | 0      | 0          | 0               | 0    | 0    | 0      | 0          | 0          |
| 08:45 AM    | 0                            | 0    | 0    | 0      | 0          | 0               | 0    | 0    | 0      | 0          | 0                     | 0    | 0    | 0      | 0          | 0               | 0    | 0    | 0      | 0          | 0          |
| Total       | 0                            | 0    | 0    | 0      | 0          | 0               | 0    | 0    | 0      | 0          | 0                     | 0    | 0    | 0      | 0          | 0               | 0    | 0    | 0      | 0          | 0          |
| Grand Total | 0                            | 0    | 0    | 0      | 0          | 0               | 0    | 0    | 0      | 0          | 0                     | 0    | 0    | 0      | 0          | 0               | 0    | 0    | 0      | 0          | 0          |
| Apprch %    | 0                            | 0    | 0    | 0      |            | 0               | 0    | 0    | 0      |            | 0                     | 0    | 0    | 0      |            | 0               | 0    | 0    | 0      |            |            |
| Total %     |                              |      |      |        |            |                 |      |      |        |            |                       |      |      |        |            |                 |      |      |        |            |            |

| Start Time   | Business Driveway Southbound |      |      |            | US 11 Westbound |      |      |            | Cherry Ct. Northbound |      |      |            | US 11 Eastbound |      |      |            | Int. Total |
|--|------------------------------|------|------|------------|-----------------|------|------|------------|-----------------------|------|------|------------|-----------------|------|------|------------|------------|
|  | Right                        | Thru | Left | App. Total | Right           | Thru | Left | App. Total | Right                 | Thru | Left | App. Total | Right           | Thru | Left | App. Total |            |
| Peak Hour Analysis From 07:00 AM to 08:45 AM - Peak 1 of 1 |                              |      |      |            |                 |      |      |            |                       |      |      |            |                 |      |      |            |            |
| Peak Hour for Entire Intersection Begins at 07:00 AM       |                              |      |      |            |                 |      |      |            |                       |      |      |            |                 |      |      |            |            |
| 07:00 AM   | 0                            | 0    | 0    | 0          | 0               | 0    | 0    | 0          | 0                     | 0    | 0    | 0          | 0               | 0    | 0    | 0          |            |
| 07:15 AM   | 0                            | 0    | 0    | 0          | 0               | 0    | 0    | 0          | 0                     | 0    | 0    | 0          | 0               | 0    | 0    | 0          |            |
| 07:30 AM   | 0                            | 0    | 0    | 0          | 0               | 0    | 0    | 0          | 0                     | 0    | 0    | 0          | 0               | 0    | 0    | 0          |            |
| 07:45 AM   | 0                            | 0    | 0    | 0          | 0               | 0    | 0    | 0          | 0                     | 0    | 0    | 0          | 0               | 0    | 0    | 0          |            |
| Total Volume   | 0                            | 0    | 0    | 0          | 0               | 0    | 0    | 0          | 0                     | 0    | 0    | 0          | 0               | 0    | 0    | 0          |            |
| % App. Total   | 0                            | 0    | 0    |            | 0               | 0    | 0    |            | 0                     | 0    | 0    |            | 0               | 0    | 0    |            |            |
| PHF  | .000                         | .000 | .000 | .000       | .000            | .000 | .000 | .000       | .000                  | .000 | .000 | .000       | .000            | .000 | .000 | .000       |            |

# Peggy Malone & Associates, Inc.

## (888) 247-8602

File Name : TM2-Cherry Ct. and US 11 TURNS ONLY with Uturns AM  
 Site Code :  
 Start Date : 2/4/2015  
 Page No : 1

Groups Printed- Combined

| Start Time         | Business Driveway Southbound |          |          |          |            | US 11 Westbound |          |          |          |            | Cherry Ct. Northbound |          |          |          |            | US 11 Eastbound |          |          |          |            | Int. Total |
|--------------------|------------------------------|----------|----------|----------|------------|-----------------|----------|----------|----------|------------|-----------------------|----------|----------|----------|------------|-----------------|----------|----------|----------|------------|------------|
|                    | Right                        | Thru     | Left     | U-Turn   | App. Total | Right           | Thru     | Left     | U-Turn   | App. Total | Right                 | Thru     | Left     | U-Turn   | App. Total | Right           | Thru     | Left     | U-Turn   | App. Total |            |
| 07:00 AM           | 1                            | 0        | 0        | 0        | 1          | 0               | 0        | 0        | 0        | 0          | 0                     | 0        | 1        | 0        | 1          | 0               | 0        | 0        | 0        | 0          | 2          |
| 07:15 AM           | 2                            | 0        | 0        | 0        | 2          | 0               | 0        | 0        | 0        | 0          | 0                     | 0        | 0        | 0        | 0          | 0               | 0        | 0        | 1        | 1          | 3          |
| 07:30 AM           | 0                            | 0        | 0        | 0        | 0          | 0               | 0        | 0        | 0        | 0          | 1                     | 0        | 0        | 0        | 1          | 0               | 0        | 0        | 1        | 1          | 2          |
| 07:45 AM           | 0                            | 0        | 0        | 0        | 0          | 0               | 0        | 0        | 0        | 0          | 0                     | 0        | 0        | 1        | 1          | 1               | 0        | 0        | 1        | 2          | 3          |
| <b>Total</b>       | <b>3</b>                     | <b>0</b> | <b>0</b> | <b>0</b> | <b>3</b>   | <b>0</b>        | <b>0</b> | <b>0</b> | <b>0</b> | <b>0</b>   | <b>1</b>              | <b>0</b> | <b>1</b> | <b>1</b> | <b>3</b>   | <b>1</b>        | <b>0</b> | <b>0</b> | <b>3</b> | <b>4</b>   | <b>10</b>  |
| 08:00 AM           | 1                            | 0        | 0        | 0        | 1          | 0               | 0        | 0        | 0        | 0          | 2                     | 0        | 3        | 0        | 5          | 0               | 0        | 0        | 0        | 0          | 6          |
| 08:15 AM           | 0                            | 0        | 0        | 0        | 0          | 0               | 0        | 0        | 0        | 0          | 0                     | 0        | 0        | 0        | 0          | 0               | 0        | 0        | 0        | 0          | 0          |
| 08:30 AM           | 0                            | 0        | 0        | 0        | 0          | 0               | 0        | 0        | 0        | 0          | 0                     | 0        | 0        | 0        | 0          | 0               | 0        | 0        | 0        | 0          | 0          |
| 08:45 AM           | 0                            | 0        | 0        | 0        | 0          | 1               | 0        | 0        | 0        | 1          | 0                     | 0        | 0        | 0        | 0          | 0               | 0        | 2        | 0        | 2          | 3          |
| <b>Total</b>       | <b>1</b>                     | <b>0</b> | <b>0</b> | <b>0</b> | <b>1</b>   | <b>1</b>        | <b>0</b> | <b>0</b> | <b>0</b> | <b>1</b>   | <b>2</b>              | <b>0</b> | <b>3</b> | <b>0</b> | <b>5</b>   | <b>0</b>        | <b>0</b> | <b>2</b> | <b>0</b> | <b>2</b>   | <b>9</b>   |
| <b>Grand Total</b> | <b>4</b>                     | <b>0</b> | <b>0</b> | <b>0</b> | <b>4</b>   | <b>1</b>        | <b>0</b> | <b>0</b> | <b>0</b> | <b>1</b>   | <b>3</b>              | <b>0</b> | <b>4</b> | <b>1</b> | <b>8</b>   | <b>1</b>        | <b>0</b> | <b>2</b> | <b>3</b> | <b>6</b>   | <b>19</b>  |
| Apprch %           | 100                          | 0        | 0        | 0        |            | 100             | 0        | 0        | 0        |            | 37.5                  | 0        | 50       | 12.5     |            | 16.7            | 0        | 33.3     | 50       |            |            |
| Total %            | 21.1                         | 0        | 0        | 0        | 21.1       | 5.3             | 0        | 0        | 0        | 5.3        | 15.8                  | 0        | 21.1     | 5.3      | 42.1       | 5.3             | 0        | 10.5     | 15.8     | 31.6       |            |

| Start Time   | Business Driveway Southbound |          |          |            | US 11 Westbound |          |          |            | Cherry Ct. Northbound |          |          |            | US 11 Eastbound |          |          |            | Int. Total |
|--|------------------------------|----------|----------|------------|-----------------|----------|----------|------------|-----------------------|----------|----------|------------|-----------------|----------|----------|------------|------------|
|  | Right                        | Thru     | Left     | App. Total | Right           | Thru     | Left     | App. Total | Right                 | Thru     | Left     | App. Total | Right           | Thru     | Left     | App. Total |            |
| Peak Hour Analysis From 07:00 AM to 08:45 AM - Peak 1 of 1 |                              |          |          |            |                 |          |          |            |                       |          |          |            |                 |          |          |            |            |
| Peak Hour for Entire Intersection Begins at 07:15 AM       |                              |          |          |            |                 |          |          |            |                       |          |          |            |                 |          |          |            |            |
| 07:15 AM   | 2                            | 0        | 0        | 2          | 0               | 0        | 0        | 0          | 0                     | 0        | 0        | 0          | 0               | 0        | 0        | 0          | 2          |
| 07:30 AM   | 0                            | 0        | 0        | 0          | 0               | 0        | 0        | 0          | 1                     | 0        | 0        | 1          | 0               | 0        | 0        | 0          | 1          |
| 07:45 AM   | 0                            | 0        | 0        | 0          | 0               | 0        | 0        | 0          | 0                     | 0        | 0        | 0          | 1               | 0        | 0        | 1          | 1          |
| 08:00 AM   | 1                            | 0        | 0        | 1          | 0               | 0        | 0        | 0          | 2                     | 0        | 3        | 5          | 0               | 0        | 0        | 0          | 6          |
| <b>Total Volume</b>  | <b>3</b>                     | <b>0</b> | <b>0</b> | <b>3</b>   | <b>0</b>        | <b>0</b> | <b>0</b> | <b>0</b>   | <b>3</b>              | <b>0</b> | <b>3</b> | <b>6</b>   | <b>1</b>        | <b>0</b> | <b>0</b> | <b>1</b>   | <b>10</b>  |
| % App. Total   | 100                          | 0        | 0        |            | 0               | 0        | 0        |            | 50                    | 0        | 50       |            | 100             | 0        | 0        |            |            |
| PHF  | .375                         | .000     | .000     | .375       | .000            | .000     | .000     | .000       | .375                  | .000     | .250     | .300       | .250            | .000     | .000     | .250       | .417       |

# Peggy Malone & Associates, Inc.

## (888) 247-8602

File Name : TM2-Cherry Ct. and US 11 TURNS ONLY with Uturns MID  
 Site Code :  
 Start Date : 2/4/2015  
 Page No : 1

Groups Printed- Car

| Start Time         | Business Driveway Southbound |          |          |          |            | US 11 Westbound |          |          |          |            | Cherry Ct. Northbound |          |          |          |            | US 11 Eastbound |          |          |          |            | Int. Total |
|--------------------|------------------------------|----------|----------|----------|------------|-----------------|----------|----------|----------|------------|-----------------------|----------|----------|----------|------------|-----------------|----------|----------|----------|------------|------------|
|                    | Right                        | Thru     | Left     | U-Turn   | App. Total | Right           | Thru     | Left     | U-Turn   | App. Total | Right                 | Thru     | Left     | U-Turn   | App. Total | Right           | Thru     | Left     | U-Turn   | App. Total |            |
| 11:00 AM           | 3                            | 0        | 1        | 0        | 4          | 1               | 0        | 0        | 0        | 1          | 1                     | 0        | 0        | 0        | 1          | 1               | 0        | 3        | 0        | 4          | 10         |
| 11:15 AM           | 8                            | 0        | 0        | 0        | 8          | 0               | 0        | 0        | 0        | 0          | 0                     | 0        | 0        | 0        | 0          | 0               | 0        | 1        | 0        | 1          | 9          |
| 11:30 AM           | 1                            | 0        | 0        | 0        | 1          | 0               | 0        | 0        | 0        | 0          | 0                     | 0        | 1        | 0        | 1          | 1               | 0        | 1        | 0        | 2          | 4          |
| 11:45 AM           | 4                            | 0        | 1        | 0        | 5          | 0               | 0        | 1        | 0        | 1          | 0                     | 0        | 2        | 0        | 2          | 1               | 0        | 0        | 0        | 1          | 9          |
| <b>Total</b>       | <b>16</b>                    | <b>0</b> | <b>2</b> | <b>0</b> | <b>18</b>  | <b>1</b>        | <b>0</b> | <b>1</b> | <b>0</b> | <b>2</b>   | <b>1</b>              | <b>0</b> | <b>3</b> | <b>0</b> | <b>4</b>   | <b>3</b>        | <b>0</b> | <b>5</b> | <b>0</b> | <b>8</b>   | <b>32</b>  |
| 12:00 PM           | 3                            | 0        | 2        | 0        | 5          | 1               | 0        | 1        | 0        | 2          | 1                     | 0        | 0        | 0        | 1          | 0               | 0        | 2        | 0        | 2          | 10         |
| 12:15 PM           | 3                            | 0        | 0        | 0        | 3          | 0               | 0        | 1        | 0        | 1          | 1                     | 0        | 0        | 0        | 1          | 0               | 0        | 0        | 1        | 1          | 6          |
| 12:30 PM           | 3                            | 0        | 1        | 0        | 4          | 0               | 0        | 0        | 0        | 0          | 0                     | 0        | 0        | 0        | 0          | 0               | 0        | 1        | 0        | 1          | 5          |
| 12:45 PM           | 2                            | 0        | 1        | 0        | 3          | 0               | 0        | 0        | 0        | 0          | 0                     | 0        | 2        | 0        | 2          | 0               | 0        | 1        | 0        | 1          | 6          |
| <b>Total</b>       | <b>11</b>                    | <b>0</b> | <b>4</b> | <b>0</b> | <b>15</b>  | <b>1</b>        | <b>0</b> | <b>2</b> | <b>0</b> | <b>3</b>   | <b>2</b>              | <b>0</b> | <b>2</b> | <b>0</b> | <b>4</b>   | <b>0</b>        | <b>0</b> | <b>4</b> | <b>1</b> | <b>5</b>   | <b>27</b>  |
| <b>Grand Total</b> | <b>27</b>                    | <b>0</b> | <b>6</b> | <b>0</b> | <b>33</b>  | <b>2</b>        | <b>0</b> | <b>3</b> | <b>0</b> | <b>5</b>   | <b>3</b>              | <b>0</b> | <b>5</b> | <b>0</b> | <b>8</b>   | <b>3</b>        | <b>0</b> | <b>9</b> | <b>1</b> | <b>13</b>  | <b>59</b>  |
| Apprch %           | 81.8                         | 0        | 18.2     | 0        |            | 40              | 0        | 60       | 0        |            | 37.5                  | 0        | 62.5     | 0        |            | 23.1            | 0        | 69.2     | 7.7      |            |            |
| Total %            | 45.8                         | 0        | 10.2     | 0        | 55.9       | 3.4             | 0        | 5.1      | 0        | 8.5        | 5.1                   | 0        | 8.5      | 0        | 13.6       | 5.1             | 0        | 15.3     | 1.7      | 22         |            |

| Start Time   | Business Driveway Southbound |          |          |            | US 11 Westbound |          |          |            | Cherry Ct. Northbound |          |          |            | US 11 Eastbound |          |          |            | Int. Total |
|--|------------------------------|----------|----------|------------|-----------------|----------|----------|------------|-----------------------|----------|----------|------------|-----------------|----------|----------|------------|------------|
|  | Right                        | Thru     | Left     | App. Total | Right           | Thru     | Left     | App. Total | Right                 | Thru     | Left     | App. Total | Right           | Thru     | Left     | App. Total |            |
| Peak Hour Analysis From 11:00 AM to 12:45 PM - Peak 1 of 1 |                              |          |          |            |                 |          |          |            |                       |          |          |            |                 |          |          |            |            |
| Peak Hour for Entire Intersection Begins at 11:00 AM       |                              |          |          |            |                 |          |          |            |                       |          |          |            |                 |          |          |            |            |
| 11:00 AM   | 3                            | 0        | 1        | 4          | 1               | 0        | 0        | 1          | 1                     | 0        | 0        | 1          | 1               | 0        | 3        | 4          | 10         |
| 11:15 AM   | 8                            | 0        | 0        | 8          | 0               | 0        | 0        | 0          | 0                     | 0        | 0        | 0          | 0               | 0        | 1        | 1          | 9          |
| 11:30 AM   | 1                            | 0        | 0        | 1          | 0               | 0        | 0        | 0          | 0                     | 0        | 1        | 1          | 1               | 0        | 1        | 2          | 4          |
| 11:45 AM   | 4                            | 0        | 1        | 5          | 0               | 0        | 1        | 1          | 0                     | 0        | 2        | 2          | 1               | 0        | 0        | 1          | 9          |
| <b>Total Volume</b>  | <b>16</b>                    | <b>0</b> | <b>2</b> | <b>18</b>  | <b>1</b>        | <b>0</b> | <b>1</b> | <b>2</b>   | <b>1</b>              | <b>0</b> | <b>3</b> | <b>4</b>   | <b>3</b>        | <b>0</b> | <b>5</b> | <b>8</b>   | <b>32</b>  |
| % App. Total   | 88.9                         | 0        | 11.1     |            | 50              | 0        | 50       |            | 25                    | 0        | 75       |            | 37.5            | 0        | 62.5     |            |            |
| PHF  | .500                         | .000     | .500     | .563       | .250            | .000     | .250     | .500       | .250                  | .000     | .375     | .500       | .750            | .000     | .417     | .500       | .800       |

# Peggy Malone & Associates, Inc.

## (888) 247-8602

File Name : TM2-Cherry Ct. and US 11 TURNS ONLY with Uturns MID  
 Site Code :  
 Start Date : 2/4/2015  
 Page No : 1

Groups Printed- Truck

| Start Time         | Business Driveway Southbound |          |          |          |            | US 11 Westbound |          |          |          |            | Cherry Ct. Northbound |          |          |          |            | US 11 Eastbound |          |          |          |            | Int. Total |
|--------------------|------------------------------|----------|----------|----------|------------|-----------------|----------|----------|----------|------------|-----------------------|----------|----------|----------|------------|-----------------|----------|----------|----------|------------|------------|
|                    | Right                        | Thru     | Left     | U-Turn   | App. Total | Right           | Thru     | Left     | U-Turn   | App. Total | Right                 | Thru     | Left     | U-Turn   | App. Total | Right           | Thru     | Left     | U-Turn   | App. Total |            |
| 11:00 AM           | 0                            | 0        | 0        | 0        | 0          | 0               | 0        | 0        | 0        | 0          | 0                     | 0        | 0        | 0        | 0          | 0               | 0        | 0        | 0        | 0          | 0          |
| 11:15 AM           | 0                            | 0        | 0        | 0        | 0          | 0               | 0        | 0        | 0        | 0          | 0                     | 0        | 0        | 0        | 0          | 0               | 0        | 0        | 0        | 0          | 0          |
| 11:30 AM           | 1                            | 0        | 0        | 0        | 1          | 0               | 0        | 0        | 0        | 0          | 0                     | 0        | 0        | 0        | 0          | 0               | 0        | 0        | 0        | 0          | 1          |
| 11:45 AM           | 0                            | 0        | 0        | 0        | 0          | 0               | 0        | 0        | 0        | 0          | 0                     | 0        | 0        | 0        | 0          | 0               | 0        | 0        | 0        | 0          | 0          |
| <b>Total</b>       | <b>1</b>                     | <b>0</b> | <b>0</b> | <b>0</b> | <b>1</b>   | <b>0</b>        | <b>0</b> | <b>0</b> | <b>0</b> | <b>0</b>   | <b>0</b>              | <b>0</b> | <b>0</b> | <b>0</b> | <b>0</b>   | <b>0</b>        | <b>0</b> | <b>0</b> | <b>0</b> | <b>0</b>   | <b>1</b>   |
| 12:00 PM           | 0                            | 0        | 0        | 0        | 0          | 0               | 0        | 0        | 0        | 0          | 0                     | 0        | 0        | 0        | 0          | 0               | 0        | 0        | 0        | 0          | 0          |
| 12:15 PM           | 0                            | 0        | 0        | 0        | 0          | 0               | 0        | 0        | 0        | 0          | 0                     | 0        | 0        | 0        | 0          | 0               | 0        | 0        | 0        | 0          | 0          |
| 12:30 PM           | 0                            | 0        | 0        | 0        | 0          | 0               | 0        | 0        | 0        | 0          | 0                     | 0        | 0        | 0        | 0          | 0               | 0        | 0        | 0        | 0          | 0          |
| 12:45 PM           | 1                            | 0        | 0        | 0        | 1          | 0               | 0        | 0        | 0        | 0          | 0                     | 0        | 0        | 0        | 0          | 0               | 0        | 0        | 0        | 0          | 1          |
| <b>Total</b>       | <b>1</b>                     | <b>0</b> | <b>0</b> | <b>0</b> | <b>1</b>   | <b>0</b>        | <b>0</b> | <b>0</b> | <b>0</b> | <b>0</b>   | <b>0</b>              | <b>0</b> | <b>0</b> | <b>0</b> | <b>0</b>   | <b>0</b>        | <b>0</b> | <b>0</b> | <b>0</b> | <b>0</b>   | <b>1</b>   |
| <b>Grand Total</b> | <b>2</b>                     | <b>0</b> | <b>0</b> | <b>0</b> | <b>2</b>   | <b>0</b>        | <b>0</b> | <b>0</b> | <b>0</b> | <b>0</b>   | <b>0</b>              | <b>0</b> | <b>0</b> | <b>0</b> | <b>0</b>   | <b>0</b>        | <b>0</b> | <b>0</b> | <b>0</b> | <b>0</b>   | <b>2</b>   |
| Apprch %           | 100                          | 0        | 0        | 0        |            | 0               | 0        | 0        | 0        |            | 0                     | 0        | 0        | 0        |            | 0               | 0        | 0        | 0        |            |            |
| Total %            | 100                          | 0        | 0        | 0        | 100        | 0               | 0        | 0        | 0        | 0          | 0                     | 0        | 0        | 0        | 0          | 0               | 0        | 0        | 0        | 0          | 0          |

| Start Time   | Business Driveway Southbound |          |          |            | US 11 Westbound |          |          |            | Cherry Ct. Northbound |          |          |            | US 11 Eastbound |          |          |            | Int. Total |
|--|------------------------------|----------|----------|------------|-----------------|----------|----------|------------|-----------------------|----------|----------|------------|-----------------|----------|----------|------------|------------|
|  | Right                        | Thru     | Left     | App. Total | Right           | Thru     | Left     | App. Total | Right                 | Thru     | Left     | App. Total | Right           | Thru     | Left     | App. Total |            |
| Peak Hour Analysis From 11:00 AM to 12:45 PM - Peak 1 of 1 |                              |          |          |            |                 |          |          |            |                       |          |          |            |                 |          |          |            |            |
| Peak Hour for Entire Intersection Begins at 11:00 AM       |                              |          |          |            |                 |          |          |            |                       |          |          |            |                 |          |          |            |            |
| 11:00 AM   | 0                            | 0        | 0        | 0          | 0               | 0        | 0        | 0          | 0                     | 0        | 0        | 0          | 0               | 0        | 0        | 0          |            |
| 11:15 AM   | 0                            | 0        | 0        | 0          | 0               | 0        | 0        | 0          | 0                     | 0        | 0        | 0          | 0               | 0        | 0        | 0          |            |
| 11:30 AM   | 1                            | 0        | 0        | 1          | 0               | 0        | 0        | 0          | 0                     | 0        | 0        | 0          | 0               | 0        | 0        | 0          |            |
| 11:45 AM   | 0                            | 0        | 0        | 0          | 0               | 0        | 0        | 0          | 0                     | 0        | 0        | 0          | 0               | 0        | 0        | 0          |            |
| <b>Total Volume</b>  | <b>1</b>                     | <b>0</b> | <b>0</b> | <b>1</b>   | <b>0</b>        | <b>0</b> | <b>0</b> | <b>0</b>   | <b>0</b>              | <b>0</b> | <b>0</b> | <b>0</b>   | <b>0</b>        | <b>0</b> | <b>0</b> | <b>0</b>   |            |
| % App. Total   | 100                          | 0        | 0        |            | 0               | 0        | 0        |            | 0                     | 0        | 0        |            | 0               | 0        | 0        |            |            |
| PHF  | .250                         | .000     | .000     | .250       | .000            | .000     | .000     | .000       | .000                  | .000     | .000     | .000       | .000            | .000     | .000     | .000       |            |



# Peggy Malone & Associates, Inc.

## (888) 247-8602

File Name : TM2-Cherry Ct. and US 11 TURNS ONLY with Uturns MID  
 Site Code :  
 Start Date : 2/4/2015  
 Page No : 1

Groups Printed- Combined

| Start Time         | Business Driveway Southbound |      |      |        |            | US 11 Westbound |      |      |        |            | Cherry Ct. Northbound |      |      |        |            | US 11 Eastbound |      |      |        |            | Int. Total |
|--------------------|------------------------------|------|------|--------|------------|-----------------|------|------|--------|------------|-----------------------|------|------|--------|------------|-----------------|------|------|--------|------------|------------|
|                    | Right                        | Thru | Left | U-Turn | App. Total | Right           | Thru | Left | U-Turn | App. Total | Right                 | Thru | Left | U-Turn | App. Total | Right           | Thru | Left | U-Turn | App. Total |            |
| 11:00 AM           | 3                            | 0    | 1    | 0      | 4          | 1               | 0    | 0    | 0      | 1          | 1                     | 0    | 0    | 0      | 1          | 1               | 0    | 3    | 0      | 4          | 10         |
| 11:15 AM           | 8                            | 0    | 0    | 0      | 8          | 0               | 0    | 0    | 0      | 0          | 0                     | 0    | 0    | 0      | 0          | 0               | 0    | 1    | 0      | 1          | 9          |
| 11:30 AM           | 2                            | 0    | 0    | 0      | 2          | 0               | 0    | 0    | 0      | 0          | 0                     | 0    | 1    | 0      | 1          | 1               | 0    | 1    | 0      | 2          | 5          |
| 11:45 AM           | 4                            | 0    | 1    | 0      | 5          | 0               | 0    | 1    | 0      | 1          | 0                     | 0    | 2    | 0      | 2          | 1               | 0    | 0    | 0      | 1          | 9          |
| <b>Total</b>       | 17                           | 0    | 2    | 0      | 19         | 1               | 0    | 1    | 0      | 2          | 1                     | 0    | 3    | 0      | 4          | 3               | 0    | 5    | 0      | 8          | 33         |
| 12:00 PM           | 3                            | 0    | 2    | 0      | 5          | 1               | 0    | 1    | 0      | 2          | 1                     | 0    | 0    | 0      | 1          | 0               | 0    | 2    | 0      | 2          | 10         |
| 12:15 PM           | 3                            | 0    | 0    | 0      | 3          | 0               | 0    | 1    | 0      | 1          | 1                     | 0    | 0    | 0      | 1          | 0               | 0    | 0    | 1      | 1          | 6          |
| 12:30 PM           | 3                            | 0    | 1    | 0      | 4          | 0               | 0    | 0    | 0      | 0          | 0                     | 0    | 0    | 0      | 0          | 0               | 0    | 1    | 0      | 1          | 5          |
| 12:45 PM           | 3                            | 0    | 1    | 0      | 4          | 0               | 0    | 0    | 0      | 0          | 0                     | 0    | 2    | 0      | 2          | 0               | 0    | 1    | 0      | 1          | 7          |
| <b>Total</b>       | 12                           | 0    | 4    | 0      | 16         | 1               | 0    | 2    | 0      | 3          | 2                     | 0    | 2    | 0      | 4          | 0               | 0    | 4    | 1      | 5          | 28         |
| <b>Grand Total</b> | 29                           | 0    | 6    | 0      | 35         | 2               | 0    | 3    | 0      | 5          | 3                     | 0    | 5    | 0      | 8          | 3               | 0    | 9    | 1      | 13         | 61         |
| Apprch %           | 82.9                         | 0    | 17.1 | 0      |            | 40              | 0    | 60   | 0      |            | 37.5                  | 0    | 62.5 | 0      |            | 23.1            | 0    | 69.2 | 7.7    |            |            |
| Total %            | 47.5                         | 0    | 9.8  | 0      | 57.4       | 3.3             | 0    | 4.9  | 0      | 8.2        | 4.9                   | 0    | 8.2  | 0      | 13.1       | 4.9             | 0    | 14.8 | 1.6    | 21.3       |            |

| Start Time   | Business Driveway Southbound |      |      |            | US 11 Westbound |      |      |            | Cherry Ct. Northbound |      |      |            | US 11 Eastbound |      |      |            | Int. Total |
|--|------------------------------|------|------|------------|-----------------|------|------|------------|-----------------------|------|------|------------|-----------------|------|------|------------|------------|
|  | Right                        | Thru | Left | App. Total | Right           | Thru | Left | App. Total | Right                 | Thru | Left | App. Total | Right           | Thru | Left | App. Total |            |
| Peak Hour Analysis From 11:00 AM to 12:45 PM - Peak 1 of 1 |                              |      |      |            |                 |      |      |            |                       |      |      |            |                 |      |      |            |            |
| Peak Hour for Entire Intersection Begins at 11:00 AM       |                              |      |      |            |                 |      |      |            |                       |      |      |            |                 |      |      |            |            |
| 11:00 AM   | 3                            | 0    | 1    | 4          | 1               | 0    | 0    | 1          | 1                     | 0    | 0    | 1          | 1               | 0    | 3    | 4          | 10         |
| 11:15 AM   | 8                            | 0    | 0    | 8          | 0               | 0    | 0    | 0          | 0                     | 0    | 0    | 0          | 0               | 0    | 1    | 1          | 9          |
| 11:30 AM   | 2                            | 0    | 0    | 2          | 0               | 0    | 0    | 0          | 0                     | 0    | 1    | 1          | 1               | 0    | 1    | 2          | 5          |
| 11:45 AM   | 4                            | 0    | 1    | 5          | 0               | 0    | 1    | 1          | 0                     | 0    | 2    | 2          | 1               | 0    | 0    | 1          | 9          |
| <b>Total Volume</b>  | 17                           | 0    | 2    | 19         | 1               | 0    | 1    | 2          | 1                     | 0    | 3    | 4          | 3               | 0    | 5    | 8          | 33         |
| % App. Total   | 89.5                         | 0    | 10.5 |            | 50              | 0    | 50   |            | 25                    | 0    | 75   |            | 37.5            | 0    | 62.5 |            |            |
| PHF  | .531                         | .000 | .500 | .594       | .250            | .000 | .250 | .500       | .250                  | .000 | .375 | .500       | .750            | .000 | .417 | .500       | .825       |

# Peggy Malone & Associates, Inc.

## (888) 247-8602

File Name : TM2-Cherry Ct. and US 11 TURNS ONLY with Uturns PM  
 Site Code :  
 Start Date : 2/4/2015  
 Page No : 1

Groups Printed- Car

| Start Time         | Business Driveway Southbound |      |      |        |            | US 11 Westbound |      |      |        |            | Cherry Ct. Northbound |      |      |        |            | US 11 Eastbound |      |      |        |            | Int. Total |
|--------------------|------------------------------|------|------|--------|------------|-----------------|------|------|--------|------------|-----------------------|------|------|--------|------------|-----------------|------|------|--------|------------|------------|
|                    | Right                        | Thru | Left | U-Turn | App. Total | Right           | Thru | Left | U-Turn | App. Total | Right                 | Thru | Left | U-Turn | App. Total | Right           | Thru | Left | U-Turn | App. Total |            |
| 03:00 PM           | 6                            | 0    | 2    | 0      | 8          | 0               | 0    | 1    | 0      | 1          | 0                     | 0    | 0    | 0      | 0          | 1               | 0    | 2    | 0      | 3          | 12         |
| 03:15 PM           | 4                            | 0    | 1    | 0      | 5          | 0               | 0    | 0    | 0      | 0          | 0                     | 0    | 0    | 0      | 0          | 0               | 0    | 2    | 0      | 2          | 7          |
| 03:30 PM           | 3                            | 0    | 1    | 0      | 4          | 0               | 0    | 0    | 0      | 0          | 1                     | 0    | 0    | 0      | 1          | 1               | 0    | 1    | 0      | 2          | 7          |
| 03:45 PM           | 3                            | 0    | 1    | 0      | 4          | 0               | 0    | 0    | 0      | 0          | 3                     | 0    | 0    | 0      | 3          | 0               | 0    | 4    | 1      | 5          | 12         |
| <b>Total</b>       | 16                           | 0    | 5    | 0      | 21         | 0               | 0    | 1    | 0      | 1          | 4                     | 0    | 0    | 0      | 4          | 2               | 0    | 9    | 1      | 12         | 38         |
| 04:00 PM           | 4                            | 0    | 2    | 0      | 6          | 0               | 0    | 0    | 0      | 0          | 0                     | 0    | 0    | 0      | 0          | 2               | 0    | 4    | 1      | 7          | 13         |
| 04:15 PM           | 5                            | 0    | 0    | 0      | 5          | 0               | 0    | 0    | 0      | 0          | 0                     | 0    | 0    | 0      | 0          | 0               | 0    | 1    | 1      | 2          | 7          |
| 04:30 PM           | 9                            | 0    | 1    | 0      | 10         | 0               | 0    | 0    | 0      | 0          | 0                     | 0    | 3    | 0      | 3          | 0               | 0    | 4    | 0      | 4          | 17         |
| 04:45 PM           | 7                            | 0    | 2    | 0      | 9          | 0               | 0    | 1    | 0      | 1          | 0                     | 0    | 0    | 0      | 0          | 2               | 0    | 1    | 0      | 3          | 13         |
| <b>Total</b>       | 25                           | 0    | 5    | 0      | 30         | 0               | 0    | 1    | 0      | 1          | 0                     | 0    | 3    | 0      | 3          | 4               | 0    | 10   | 2      | 16         | 50         |
| 05:00 PM           | 3                            | 0    | 0    | 0      | 3          | 0               | 0    | 0    | 0      | 0          | 0                     | 0    | 1    | 0      | 1          | 1               | 0    | 1    | 0      | 2          | 6          |
| 05:15 PM           | 4                            | 0    | 2    | 0      | 6          | 0               | 0    | 1    | 0      | 1          | 0                     | 0    | 0    | 0      | 0          | 1               | 0    | 0    | 0      | 1          | 8          |
| 05:30 PM           | 3                            | 0    | 2    | 0      | 5          | 0               | 0    | 0    | 0      | 0          | 0                     | 0    | 0    | 0      | 0          | 0               | 0    | 1    | 0      | 1          | 6          |
| 05:45 PM           | 2                            | 0    | 1    | 0      | 3          | 2               | 0    | 1    | 0      | 3          | 0                     | 0    | 0    | 0      | 0          | 0               | 0    | 2    | 0      | 2          | 8          |
| <b>Total</b>       | 12                           | 0    | 5    | 0      | 17         | 2               | 0    | 2    | 0      | 4          | 0                     | 0    | 1    | 0      | 1          | 2               | 0    | 4    | 0      | 6          | 28         |
| <b>Grand Total</b> | 53                           | 0    | 15   | 0      | 68         | 2               | 0    | 4    | 0      | 6          | 4                     | 0    | 4    | 0      | 8          | 8               | 0    | 23   | 3      | 34         | 116        |
| Apprch %           | 77.9                         | 0    | 22.1 | 0      |            | 33.3            | 0    | 66.7 | 0      |            | 50                    | 0    | 50   | 0      |            | 23.5            | 0    | 67.6 | 8.8    |            |            |
| Total %            | 45.7                         | 0    | 12.9 | 0      | 58.6       | 1.7             | 0    | 3.4  | 0      | 5.2        | 3.4                   | 0    | 3.4  | 0      | 6.9        | 6.9             | 0    | 19.8 | 2.6    | 29.3       |            |

| Start Time   | Business Driveway Southbound |      |      |            | US 11 Westbound |      |      |            | Cherry Ct. Northbound |      |      |            | US 11 Eastbound |      |      |            | Int. Total |
|--|------------------------------|------|------|------------|-----------------|------|------|------------|-----------------------|------|------|------------|-----------------|------|------|------------|------------|
|  | Right                        | Thru | Left | App. Total | Right           | Thru | Left | App. Total | Right                 | Thru | Left | App. Total | Right           | Thru | Left | App. Total |            |
| Peak Hour Analysis From 03:00 PM to 05:45 PM - Peak 1 of 1 |                              |      |      |            |                 |      |      |            |                       |      |      |            |                 |      |      |            |            |
| Peak Hour for Entire Intersection Begins at 04:00 PM       |                              |      |      |            |                 |      |      |            |                       |      |      |            |                 |      |      |            |            |
| 04:00 PM   | 4                            | 0    | 2    | 6          | 0               | 0    | 0    | 0          | 0                     | 0    | 0    | 0          | 2               | 0    | 4    | 6          | 12         |
| 04:15 PM   | 5                            | 0    | 0    | 5          | 0               | 0    | 0    | 0          | 0                     | 0    | 0    | 0          | 0               | 0    | 1    | 1          | 6          |
| 04:30 PM   | 9                            | 0    | 1    | 10         | 0               | 0    | 0    | 0          | 0                     | 0    | 3    | 3          | 0               | 0    | 4    | 4          | 17         |
| 04:45 PM   | 7                            | 0    | 2    | 9          | 0               | 0    | 1    | 1          | 0                     | 0    | 0    | 0          | 2               | 0    | 1    | 3          | 13         |
| <b>Total Volume</b>  | 25                           | 0    | 5    | 30         | 0               | 0    | 1    | 1          | 0                     | 0    | 3    | 3          | 4               | 0    | 10   | 14         | 48         |
| % App. Total   | 83.3                         | 0    | 16.7 |            | 0               | 0    | 100  |            | 0                     | 0    | 100  |            | 28.6            | 0    | 71.4 |            |            |
| PHF  | .694                         | .000 | .625 | .750       | .000            | .000 | .250 | .250       | .000                  | .000 | .250 | .250       | .500            | .000 | .625 | .583       | .706       |

# Peggy Malone & Associates, Inc.

## (888) 247-8602

File Name : TM2-Cherry Ct. and US 11 TURNS ONLY with Uturns PM  
 Site Code :  
 Start Date : 2/4/2015  
 Page No : 1

Groups Printed- Truck

| Start Time  | Business Driveway Southbound |      |      |        |            | US 11 Westbound |      |      |        |            | Cherry Ct. Northbound |      |      |        |            | US 11 Eastbound |      |      |        |            | Int. Total |
|-------------|------------------------------|------|------|--------|------------|-----------------|------|------|--------|------------|-----------------------|------|------|--------|------------|-----------------|------|------|--------|------------|------------|
|             | Right                        | Thru | Left | U-Turn | App. Total | Right           | Thru | Left | U-Turn | App. Total | Right                 | Thru | Left | U-Turn | App. Total | Right           | Thru | Left | U-Turn | App. Total |            |
| 03:00 PM    | 0                            | 0    | 0    | 0      | 0          | 0               | 0    | 0    | 0      | 0          | 0                     | 0    | 0    | 0      | 0          | 0               | 0    | 0    | 0      | 0          | 0          |
| 03:15 PM    | 0                            | 0    | 0    | 0      | 0          | 0               | 0    | 0    | 0      | 0          | 0                     | 0    | 0    | 0      | 0          | 0               | 0    | 0    | 0      | 0          | 0          |
| 03:30 PM    | 0                            | 0    | 0    | 0      | 0          | 0               | 0    | 0    | 0      | 0          | 0                     | 0    | 0    | 0      | 0          | 0               | 0    | 0    | 0      | 0          | 0          |
| 03:45 PM    | 0                            | 0    | 0    | 0      | 0          | 0               | 0    | 0    | 0      | 0          | 0                     | 0    | 0    | 0      | 0          | 0               | 0    | 0    | 0      | 0          | 0          |
| Total       | 0                            | 0    | 0    | 0      | 0          | 0               | 0    | 0    | 0      | 0          | 0                     | 0    | 0    | 0      | 0          | 0               | 0    | 0    | 0      | 0          | 0          |
| 04:00 PM    | 0                            | 0    | 0    | 0      | 0          | 0               | 0    | 0    | 0      | 0          | 0                     | 0    | 0    | 0      | 0          | 0               | 0    | 0    | 0      | 0          | 0          |
| 04:15 PM    | 0                            | 0    | 0    | 0      | 0          | 0               | 0    | 0    | 0      | 0          | 0                     | 0    | 0    | 0      | 0          | 0               | 0    | 0    | 0      | 0          | 0          |
| 04:30 PM    | 0                            | 0    | 0    | 0      | 0          | 0               | 0    | 0    | 0      | 0          | 0                     | 0    | 0    | 0      | 0          | 0               | 0    | 0    | 0      | 0          | 0          |
| 04:45 PM    | 0                            | 0    | 0    | 0      | 0          | 0               | 0    | 0    | 0      | 0          | 0                     | 0    | 0    | 0      | 0          | 0               | 0    | 0    | 0      | 0          | 0          |
| Total       | 0                            | 0    | 0    | 0      | 0          | 0               | 0    | 0    | 0      | 0          | 0                     | 0    | 0    | 0      | 0          | 0               | 0    | 0    | 0      | 0          | 0          |
| 05:00 PM    | 0                            | 0    | 0    | 0      | 0          | 0               | 0    | 0    | 0      | 0          | 0                     | 0    | 0    | 0      | 0          | 0               | 0    | 0    | 0      | 0          | 0          |
| 05:15 PM    | 0                            | 0    | 0    | 0      | 0          | 0               | 0    | 0    | 0      | 0          | 0                     | 0    | 0    | 0      | 0          | 0               | 0    | 0    | 0      | 0          | 0          |
| 05:30 PM    | 0                            | 0    | 0    | 0      | 0          | 0               | 0    | 0    | 0      | 0          | 0                     | 0    | 0    | 0      | 0          | 0               | 0    | 0    | 0      | 0          | 0          |
| 05:45 PM    | 0                            | 0    | 0    | 0      | 0          | 0               | 0    | 0    | 0      | 0          | 0                     | 0    | 0    | 0      | 0          | 0               | 0    | 0    | 0      | 0          | 0          |
| Total       | 0                            | 0    | 0    | 0      | 0          | 0               | 0    | 0    | 0      | 0          | 0                     | 0    | 0    | 0      | 0          | 0               | 0    | 0    | 0      | 0          | 0          |
| Grand Total | 0                            | 0    | 0    | 0      | 0          | 0               | 0    | 0    | 0      | 0          | 0                     | 0    | 0    | 0      | 0          | 0               | 0    | 0    | 0      | 0          | 0          |
| Apprch %    | 0                            | 0    | 0    | 0      | 0          | 0               | 0    | 0    | 0      | 0          | 0                     | 0    | 0    | 0      | 0          | 0               | 0    | 0    | 0      | 0          | 0          |
| Total %     |                              |      |      |        |            |                 |      |      |        |            |                       |      |      |        |            |                 |      |      |        |            |            |

| Start Time   | Business Driveway Southbound |      |      |            | US 11 Westbound |      |      |            | Cherry Ct. Northbound |      |      |            | US 11 Eastbound |      |      |            | Int. Total |
|--|------------------------------|------|------|------------|-----------------|------|------|------------|-----------------------|------|------|------------|-----------------|------|------|------------|------------|
|  | Right                        | Thru | Left | App. Total | Right           | Thru | Left | App. Total | Right                 | Thru | Left | App. Total | Right           | Thru | Left | App. Total |            |
| Peak Hour Analysis From 03:00 PM to 05:45 PM - Peak 1 of 1 |                              |      |      |            |                 |      |      |            |                       |      |      |            |                 |      |      |            |            |
| Peak Hour for Entire Intersection Begins at 03:00 PM       |                              |      |      |            |                 |      |      |            |                       |      |      |            |                 |      |      |            |            |
| 03:00 PM   | 0                            | 0    | 0    | 0          | 0               | 0    | 0    | 0          | 0                     | 0    | 0    | 0          | 0               | 0    | 0    | 0          | 0          |
| 03:15 PM   | 0                            | 0    | 0    | 0          | 0               | 0    | 0    | 0          | 0                     | 0    | 0    | 0          | 0               | 0    | 0    | 0          | 0          |
| 03:30 PM   | 0                            | 0    | 0    | 0          | 0               | 0    | 0    | 0          | 0                     | 0    | 0    | 0          | 0               | 0    | 0    | 0          | 0          |
| 03:45 PM   | 0                            | 0    | 0    | 0          | 0               | 0    | 0    | 0          | 0                     | 0    | 0    | 0          | 0               | 0    | 0    | 0          | 0          |
| Total Volume   | 0                            | 0    | 0    | 0          | 0               | 0    | 0    | 0          | 0                     | 0    | 0    | 0          | 0               | 0    | 0    | 0          | 0          |
| % App. Total   | 0                            | 0    | 0    | 0          | 0               | 0    | 0    | 0          | 0                     | 0    | 0    | 0          | 0               | 0    | 0    | 0          | 0          |
| PHF  | .000                         | .000 | .000 | .000       | .000            | .000 | .000 | .000       | .000                  | .000 | .000 | .000       | .000            | .000 | .000 | .000       | .000       |

# Peggy Malone & Associates, Inc.

## (888) 247-8602

File Name : TM2-Cherry Ct. and US 11 TURNS ONLY with Uturns PM  
 Site Code :  
 Start Date : 2/4/2015  
 Page No : 1

Groups Printed- Combined

| Start Time         | Business Driveway Southbound |      |      |        |            | US 11 Westbound |      |      |        |            | Cherry Ct. Northbound |      |      |        |            | US 11 Eastbound |      |      |        |            | Int. Total |
|--------------------|------------------------------|------|------|--------|------------|-----------------|------|------|--------|------------|-----------------------|------|------|--------|------------|-----------------|------|------|--------|------------|------------|
|                    | Right                        | Thru | Left | U-Turn | App. Total | Right           | Thru | Left | U-Turn | App. Total | Right                 | Thru | Left | U-Turn | App. Total | Right           | Thru | Left | U-Turn | App. Total |            |
| 03:00 PM           | 6                            | 0    | 2    | 0      | 8          | 0               | 0    | 1    | 0      | 1          | 0                     | 0    | 0    | 0      | 0          | 1               | 0    | 2    | 0      | 3          | 12         |
| 03:15 PM           | 4                            | 0    | 1    | 0      | 5          | 0               | 0    | 0    | 0      | 0          | 0                     | 0    | 0    | 0      | 0          | 0               | 0    | 2    | 0      | 2          | 7          |
| 03:30 PM           | 3                            | 0    | 1    | 0      | 4          | 0               | 0    | 0    | 0      | 0          | 1                     | 0    | 0    | 0      | 1          | 1               | 0    | 1    | 0      | 2          | 7          |
| 03:45 PM           | 3                            | 0    | 1    | 0      | 4          | 0               | 0    | 0    | 0      | 0          | 3                     | 0    | 0    | 0      | 3          | 0               | 0    | 4    | 1      | 5          | 12         |
| <b>Total</b>       | 16                           | 0    | 5    | 0      | 21         | 0               | 0    | 1    | 0      | 1          | 4                     | 0    | 0    | 0      | 4          | 2               | 0    | 9    | 1      | 12         | 38         |
| 04:00 PM           | 4                            | 0    | 2    | 0      | 6          | 0               | 0    | 0    | 0      | 0          | 0                     | 0    | 0    | 0      | 0          | 2               | 0    | 4    | 1      | 7          | 13         |
| 04:15 PM           | 5                            | 0    | 0    | 0      | 5          | 0               | 0    | 0    | 0      | 0          | 0                     | 0    | 0    | 0      | 0          | 0               | 0    | 1    | 1      | 2          | 7          |
| 04:30 PM           | 9                            | 0    | 1    | 0      | 10         | 0               | 0    | 0    | 0      | 0          | 0                     | 0    | 3    | 3      | 0          | 0               | 0    | 4    | 0      | 4          | 17         |
| 04:45 PM           | 7                            | 0    | 2    | 0      | 9          | 0               | 0    | 1    | 0      | 1          | 0                     | 0    | 0    | 0      | 0          | 2               | 0    | 1    | 0      | 3          | 13         |
| <b>Total</b>       | 25                           | 0    | 5    | 0      | 30         | 0               | 0    | 1    | 0      | 1          | 0                     | 0    | 3    | 3      | 3          | 4               | 0    | 10   | 2      | 16         | 50         |
| 05:00 PM           | 3                            | 0    | 0    | 0      | 3          | 0               | 0    | 0    | 0      | 0          | 0                     | 0    | 1    | 1      | 1          | 1               | 0    | 1    | 0      | 2          | 6          |
| 05:15 PM           | 4                            | 0    | 2    | 0      | 6          | 0               | 0    | 1    | 0      | 1          | 0                     | 0    | 0    | 0      | 0          | 1               | 0    | 0    | 0      | 1          | 8          |
| 05:30 PM           | 3                            | 0    | 2    | 0      | 5          | 0               | 0    | 0    | 0      | 0          | 0                     | 0    | 0    | 0      | 0          | 0               | 0    | 1    | 0      | 1          | 6          |
| 05:45 PM           | 2                            | 0    | 1    | 0      | 3          | 2               | 0    | 1    | 0      | 3          | 0                     | 0    | 0    | 0      | 0          | 0               | 0    | 2    | 0      | 2          | 8          |
| <b>Total</b>       | 12                           | 0    | 5    | 0      | 17         | 2               | 0    | 2    | 0      | 4          | 0                     | 0    | 1    | 1      | 1          | 2               | 0    | 4    | 0      | 6          | 28         |
| <b>Grand Total</b> | 53                           | 0    | 15   | 0      | 68         | 2               | 0    | 4    | 0      | 6          | 4                     | 0    | 4    | 8      | 8          | 8               | 0    | 23   | 3      | 34         | 116        |
| Apprch %           | 77.9                         | 0    | 22.1 | 0      |            | 33.3            | 0    | 66.7 | 0      |            | 50                    | 0    | 50   | 0      |            | 23.5            | 0    | 67.6 | 8.8    |            |            |
| Total %            | 45.7                         | 0    | 12.9 | 0      | 58.6       | 1.7             | 0    | 3.4  | 0      | 5.2        | 3.4                   | 0    | 3.4  | 6.9    | 6.9        | 0               | 19.8 | 2.6  | 29.3   |            |            |

| Start Time   | Business Driveway Southbound |      |      |            | US 11 Westbound |      |      |            | Cherry Ct. Northbound |      |      |            | US 11 Eastbound |      |      |            | Int. Total |    |
|--|------------------------------|------|------|------------|-----------------|------|------|------------|-----------------------|------|------|------------|-----------------|------|------|------------|------------|----|
|  | Right                        | Thru | Left | App. Total | Right           | Thru | Left | App. Total | Right                 | Thru | Left | App. Total | Right           | Thru | Left | App. Total |            |    |
| Peak Hour Analysis From 03:00 PM to 05:45 PM - Peak 1 of 1 |                              |      |      |            |                 |      |      |            |                       |      |      |            |                 |      |      |            |            |    |
| Peak Hour for Entire Intersection Begins at 04:00 PM       |                              |      |      |            |                 |      |      |            |                       |      |      |            |                 |      |      |            |            |    |
| 04:00 PM   | 4                            | 0    | 2    | 6          | 0               | 0    | 0    | 0          | 0                     | 0    | 0    | 0          | 0               | 2    | 0    | 4          | 6          | 12 |
| 04:15 PM   | 5                            | 0    | 0    | 5          | 0               | 0    | 0    | 0          | 0                     | 0    | 0    | 0          | 0               | 0    | 0    | 1          | 1          | 6  |
| 04:30 PM   | 9                            | 0    | 1    | 10         | 0               | 0    | 0    | 0          | 0                     | 0    | 0    | 3          | 3               | 0    | 0    | 4          | 4          | 17 |
| 04:45 PM   | 7                            | 0    | 2    | 9          | 0               | 0    | 1    | 1          | 0                     | 0    | 0    | 0          | 0               | 2    | 0    | 1          | 3          | 13 |
| <b>Total Volume</b>  | 25                           | 0    | 5    | 30         | 0               | 0    | 1    | 1          | 0                     | 0    | 3    | 3          | 4               | 0    | 10   | 14         | 48         |    |
| % App. Total   | 83.3                         | 0    | 16.7 |            | 0               | 0    | 100  |            | 0                     | 0    | 100  |            | 28.6            | 0    | 71.4 |            |            |    |
| PHF  | .694                         | .000 | .625 | .750       | .000            | .000 | .250 | .250       | .000                  | .000 | .250 | .250       | .500            | .000 | .625 | .583       | .706       |    |

Peggy Malone & Associates  
WEEKLY SUMMARY  
Starting:2/4/2015

Station #: Site A-EB  
Site ID: 000000010649  
Location: US 11, W of Cherry Ct.  
Direction: EAST

File: D0204001.prn  
City: 15-016 KL/TO Min  
County: 37.14581, -80.58437

| TIME     | MON |    | TUE |    | WED<br>04 |     | THU<br>5 |     | FRI |    | SAT |    | SUN |    | WK TOT |     | WK AVG |     |
|----------|-----|----|-----|----|-----------|-----|----------|-----|-----|----|-----|----|-----|----|--------|-----|--------|-----|
|          | am  | pm | am  | pm | am        | pm  | am       | pm  | am  | pm | am  | pm | am  | pm | am     | pm  | am     | pm  |
| Lane 1   |     |    |     |    |           |     |          |     |     |    |     |    |     |    |        |     |        |     |
| 00:15    |     |    |     |    | 17        | 213 | 25       | 214 |     |    |     |    |     |    | 42     | 427 | 21     | 213 |
| 00:30    |     |    |     |    | 18        | 236 | 14       | 214 |     |    |     |    |     |    | 32     | 450 | 16     | 225 |
| 00:45    |     |    |     |    | 11        | 242 | 16       | 191 |     |    |     |    |     |    | 27     | 433 | 13     | 216 |
| 01:00    |     |    |     |    | 17        | 217 | 15       | 194 |     |    |     |    |     |    | 32     | 411 | 16     | 205 |
| 01:15    |     |    |     |    | 7         | 214 | 16       | 201 |     |    |     |    |     |    | 23     | 415 | 11     | 207 |
| 01:30    |     |    |     |    | 10        | 200 | 12       | 206 |     |    |     |    |     |    | 22     | 406 | 11     | 203 |
| 01:45    |     |    |     |    | 9         | 233 | 11       | 214 |     |    |     |    |     |    | 20     | 447 | 10     | 223 |
| 02:00    |     |    |     |    | 11        | 207 | 12       | 191 |     |    |     |    |     |    | 23     | 398 | 11     | 199 |
| 02:15    |     |    |     |    | 15        | 214 | 12       | 218 |     |    |     |    |     |    | 27     | 432 | 13     | 216 |
| 02:30    |     |    |     |    | 8         | 229 | 18       | 228 |     |    |     |    |     |    | 26     | 457 | 13     | 228 |
| 02:45    |     |    |     |    | 12        | 204 | 7        | 217 |     |    |     |    |     |    | 19     | 421 | 9      | 210 |
| 03:00    |     |    |     |    | 8         | 230 | 7        | 190 |     |    |     |    |     |    | 15     | 420 | 7      | 210 |
| 03:15    |     |    |     |    | 4         | 220 | 4        | 231 |     |    |     |    |     |    | 8      | 451 | 4      | 225 |
| 03:30    |     |    |     |    | 6         | 267 | 3        | 222 |     |    |     |    |     |    | 9      | 489 | 4      | 244 |
| 03:45    |     |    |     |    | 2         | 260 | 4        | 253 |     |    |     |    |     |    | 6      | 513 | 3      | 256 |
| 04:00    |     |    |     |    | 1         | 236 | 7        | 245 |     |    |     |    |     |    | 8      | 481 | 4      | 240 |
| 04:15    |     |    |     |    | 12        | 257 | 8        | 245 |     |    |     |    |     |    | 20     | 502 | 10     | 251 |
| 04:30    |     |    |     |    | 5         | 266 | 8        | 257 |     |    |     |    |     |    | 13     | 523 | 6      | 261 |
| 04:45    |     |    |     |    | 14        | 263 | 19       | 244 |     |    |     |    |     |    | 33     | 507 | 16     | 253 |
| 05:00    |     |    |     |    | 28        | 261 | 27       | 265 |     |    |     |    |     |    | 55     | 526 | 27     | 263 |
| 05:15    |     |    |     |    | 25        | 276 | 16       | 221 |     |    |     |    |     |    | 41     | 497 | 20     | 248 |
| 05:30    |     |    |     |    | 36        | 247 | 33       | 250 |     |    |     |    |     |    | 69     | 497 | 34     | 248 |
| 05:45    |     |    |     |    | 39        | 281 | 40       | 234 |     |    |     |    |     |    | 79     | 515 | 39     | 257 |
| 06:00    |     |    |     |    | 41        | 245 | 41       | 242 |     |    |     |    |     |    | 82     | 487 | 41     | 243 |
| 06:15    |     |    |     |    | 39        | 237 | 49       | 202 |     |    |     |    |     |    | 88     | 439 | 44     | 219 |
| 06:30    |     |    |     |    | 76        | 224 | 69       | 214 |     |    |     |    |     |    | 145    | 438 | 72     | 219 |
| 06:45    |     |    |     |    | 96        | 200 | 78       | 202 |     |    |     |    |     |    | 174    | 402 | 87     | 201 |
| 07:00    |     |    |     |    | 120       | 197 | 135      | 159 |     |    |     |    |     |    | 255    | 356 | 127    | 178 |
| 07:15    |     |    |     |    | 128       | 173 | 135      | 166 |     |    |     |    |     |    | 263    | 339 | 131    | 169 |
| 07:30    |     |    |     |    | 193       | 150 | 174      | 138 |     |    |     |    |     |    | 367    | 288 | 183    | 144 |
| 07:45    |     |    |     |    | 236       | 101 | 227      | 162 |     |    |     |    |     |    | 463    | 263 | 231    | 131 |
| 08:00    |     |    |     |    | 239       | 123 | 283      | 124 |     |    |     |    |     |    | 522    | 247 | 261    | 123 |
| 08:15    |     |    |     |    | 196       | 125 | 168      | 115 |     |    |     |    |     |    | 364    | 240 | 182    | 120 |
| 08:30    |     |    |     |    | 198       | 126 | 203      | 106 |     |    |     |    |     |    | 401    | 232 | 200    | 116 |
| 08:45    |     |    |     |    | 170       | 90  | 163      | 90  |     |    |     |    |     |    | 333    | 180 | 166    | 90  |
| 09:00    |     |    |     |    | 184       | 108 | 176      | 85  |     |    |     |    |     |    | 360    | 193 | 180    | 96  |
| 09:15    |     |    |     |    | 122       | 108 | 150      | 109 |     |    |     |    |     |    | 272    | 217 | 136    | 108 |
| 09:30    |     |    |     |    | 123       | 101 | 137      | 104 |     |    |     |    |     |    | 260    | 205 | 130    | 102 |
| 09:45    |     |    |     |    | 168       | 71  | 135      | 69  |     |    |     |    |     |    | 303    | 140 | 151    | 70  |
| 10:00    |     |    |     |    | 129       | 77  | 156      | 60  |     |    |     |    |     |    | 285    | 137 | 142    | 68  |
| 10:15    |     |    |     |    | 151       | 63  | 142      | 63  |     |    |     |    |     |    | 293    | 126 | 146    | 63  |
| 10:30    |     |    |     |    | 177       | 51  | 163      | 58  |     |    |     |    |     |    | 340    | 109 | 170    | 54  |
| 10:45    |     |    |     |    | 168       | 45  | 169      | 40  |     |    |     |    |     |    | 337    | 85  | 168    | 42  |
| 11:00    |     |    |     |    | 143       | 39  | 165      | 41  |     |    |     |    |     |    | 308    | 80  | 154    | 40  |
| 11:15    |     |    |     |    | 164       | 32  | 175      | 36  |     |    |     |    |     |    | 339    | 68  | 169    | 34  |
| 11:30    |     |    |     |    | 194       | 43  | 179      | 33  |     |    |     |    |     |    | 373    | 76  | 186    | 38  |
| 11:45    |     |    |     |    | 198       | 29  | 167      | 16  |     |    |     |    |     |    | 365    | 45  | 182    | 22  |
| 12:00    |     |    |     |    | 187       | 18  | 191      | 21  |     |    |     |    |     |    | 378    | 39  | 189    | 19  |
| -----    |     |    |     |    |           |     |          |     |     |    |     |    |     |    |        |     |        |     |
| TOTALS   | 0   | 0  |     |    | 12404     |     | 11964    |     | 0   | 0  | 0   |    |     | 0  | 24368  |     | 12156  |     |
| AM Times |     |    |     |    | 7:45      |     | 7:45     |     |     |    |     |    |     |    | 7:45   |     | 7:45   |     |
| AM Peaks |     |    |     |    | 869       |     | 881      |     |     |    |     |    |     |    | 1750   |     | 874    |     |
| AM PHF   |     |    |     |    | 0.91      |     | 0.78     |     |     |    |     |    |     |    | 0.84   |     | 0.84   |     |
| PM Times |     |    |     |    | 16:30     |     | 16:15    |     |     |    |     |    |     |    | 16:15  |     | 16:15  |     |
| PM Peaks |     |    |     |    | 1066      |     | 1011     |     |     |    |     |    |     |    | 2058   |     | 1028   |     |
| PM PHF   |     |    |     |    | 0.97      |     | 0.95     |     |     |    |     |    |     |    | 0.98   |     | 0.98   |     |

Peggy Malone & Associates  
 WEEKLY SUMMARY  
 Starting:2/4/2015

Station #: Site A-WB  
 Site ID: 000000010742  
 Location: US 11, W of Cherry Ct.  
 Direction: WEST

File: D0204002.prn  
 City: 15-016 KL/TO Min  
 County: 37.14581, -80.58437

| TIME     | MON |    | TUE |    | WED<br>04 |     | THU<br>5 |     | FRI |    | SAT |    | SUN |    | WK TOT |     | WK AVG |     |
|----------|-----|----|-----|----|-----------|-----|----------|-----|-----|----|-----|----|-----|----|--------|-----|--------|-----|
|          | am  | pm | am  | pm | am        | pm  | am       | pm  | am  | pm | am  | pm | am  | pm | am     | pm  | am     | pm  |
| 00:15    |     |    |     |    | 16        | 195 | 22       | 178 |     |    |     |    |     |    | 38     | 373 | 19     | 186 |
| 00:30    |     |    |     |    | 18        | 193 | 17       | 176 |     |    |     |    |     |    | 35     | 369 | 17     | 184 |
| 00:45    |     |    |     |    | 9         | 169 | 18       | 209 |     |    |     |    |     |    | 27     | 378 | 13     | 189 |
| 01:00    |     |    |     |    | 11        | 198 | 8        | 166 |     |    |     |    |     |    | 19     | 364 | 9      | 182 |
| 01:15    |     |    |     |    | 13        | 180 | 11       | 164 |     |    |     |    |     |    | 24     | 344 | 12     | 172 |
| 01:30    |     |    |     |    | 5         | 182 | 11       | 151 |     |    |     |    |     |    | 16     | 333 | 8      | 166 |
| 01:45    |     |    |     |    | 5         | 156 | 7        | 159 |     |    |     |    |     |    | 12     | 315 | 6      | 157 |
| 02:00    |     |    |     |    | 8         | 183 | 11       | 182 |     |    |     |    |     |    | 19     | 365 | 9      | 182 |
| 02:15    |     |    |     |    | 10        | 192 | 6        | 185 |     |    |     |    |     |    | 16     | 377 | 8      | 188 |
| 02:30    |     |    |     |    | 3         | 199 | 6        | 198 |     |    |     |    |     |    | 9      | 397 | 4      | 198 |
| 02:45    |     |    |     |    | 5         | 218 | 7        | 189 |     |    |     |    |     |    | 12     | 407 | 6      | 203 |
| 03:00    |     |    |     |    | 3         | 191 | 1        | 196 |     |    |     |    |     |    | 4      | 387 | 2      | 193 |
| 03:15    |     |    |     |    | 6         | 208 | 8        | 192 |     |    |     |    |     |    | 14     | 400 | 7      | 200 |
| 03:30    |     |    |     |    | 3         | 194 | 4        | 193 |     |    |     |    |     |    | 7      | 387 | 3      | 193 |
| 03:45    |     |    |     |    | 3         | 254 | 5        | 215 |     |    |     |    |     |    | 8      | 469 | 4      | 234 |
| 04:00    |     |    |     |    | 4         | 227 | 7        | 220 |     |    |     |    |     |    | 11     | 447 | 5      | 223 |
| 04:15    |     |    |     |    | 8         | 237 | 8        | 228 |     |    |     |    |     |    | 16     | 465 | 8      | 232 |
| 04:30    |     |    |     |    | 5         | 202 | 7        | 208 |     |    |     |    |     |    | 12     | 410 | 6      | 205 |
| 04:45    |     |    |     |    | 8         | 248 | 5        | 207 |     |    |     |    |     |    | 13     | 455 | 6      | 227 |
| 05:00    |     |    |     |    | 12        | 243 | 14       | 242 |     |    |     |    |     |    | 26     | 485 | 13     | 242 |
| 05:15    |     |    |     |    | 22        | 285 | 13       | 278 |     |    |     |    |     |    | 35     | 563 | 17     | 281 |
| 05:30    |     |    |     |    | 23        | 284 | 29       | 294 |     |    |     |    |     |    | 52     | 578 | 26     | 289 |
| 05:45    |     |    |     |    | 35        | 216 | 36       | 209 |     |    |     |    |     |    | 71     | 425 | 35     | 212 |
| 06:00    |     |    |     |    | 21        | 184 | 32       | 203 |     |    |     |    |     |    | 53     | 387 | 26     | 193 |
| 06:15    |     |    |     |    | 38        | 199 | 41       | 181 |     |    |     |    |     |    | 79     | 380 | 39     | 190 |
| 06:30    |     |    |     |    | 68        | 186 | 62       | 162 |     |    |     |    |     |    | 130    | 348 | 65     | 174 |
| 06:45    |     |    |     |    | 70        | 152 | 71       | 157 |     |    |     |    |     |    | 141    | 309 | 70     | 154 |
| 07:00    |     |    |     |    | 97        | 142 | 85       | 146 |     |    |     |    |     |    | 182    | 288 | 91     | 144 |
| 07:15    |     |    |     |    | 93        | 115 | 107      | 117 |     |    |     |    |     |    | 200    | 232 | 100    | 116 |
| 07:30    |     |    |     |    | 119       | 106 | 108      | 124 |     |    |     |    |     |    | 227    | 230 | 113    | 115 |
| 07:45    |     |    |     |    | 119       | 93  | 132      | 116 |     |    |     |    |     |    | 251    | 209 | 125    | 104 |
| 08:00    |     |    |     |    | 139       | 98  | 132      | 94  |     |    |     |    |     |    | 271    | 192 | 135    | 96  |
| 08:15    |     |    |     |    | 118       | 85  | 121      | 85  |     |    |     |    |     |    | 239    | 170 | 119    | 85  |
| 08:30    |     |    |     |    | 112       | 95  | 117      | 88  |     |    |     |    |     |    | 229    | 183 | 114    | 91  |
| 08:45    |     |    |     |    | 116       | 104 | 123      | 82  |     |    |     |    |     |    | 239    | 186 | 119    | 93  |
| 09:00    |     |    |     |    | 123       | 94  | 118      | 52  |     |    |     |    |     |    | 241    | 146 | 120    | 73  |
| 09:15    |     |    |     |    | 120       | 69  | 128      | 73  |     |    |     |    |     |    | 248    | 142 | 124    | 71  |
| 09:30    |     |    |     |    | 101       | 75  | 123      | 67  |     |    |     |    |     |    | 224    | 142 | 112    | 71  |
| 09:45    |     |    |     |    | 106       | 72  | 111      | 60  |     |    |     |    |     |    | 217    | 132 | 108    | 66  |
| 10:00    |     |    |     |    | 153       | 58  | 109      | 59  |     |    |     |    |     |    | 262    | 117 | 131    | 58  |
| 10:15    |     |    |     |    | 145       | 42  | 126      | 51  |     |    |     |    |     |    | 271    | 93  | 135    | 46  |
| 10:30    |     |    |     |    | 114       | 34  | 131      | 53  |     |    |     |    |     |    | 245    | 87  | 122    | 43  |
| 10:45    |     |    |     |    | 142       | 44  | 142      | 24  |     |    |     |    |     |    | 284    | 68  | 142    | 34  |
| 11:00    |     |    |     |    | 152       | 24  | 140      | 32  |     |    |     |    |     |    | 292    | 56  | 146    | 28  |
| 11:15    |     |    |     |    | 155       | 38  | 175      | 31  |     |    |     |    |     |    | 330    | 69  | 165    | 34  |
| 11:30    |     |    |     |    | 163       | 28  | 162      | 31  |     |    |     |    |     |    | 325    | 59  | 162    | 29  |
| 11:45    |     |    |     |    | 174       | 27  | 155      | 17  |     |    |     |    |     |    | 329    | 44  | 164    | 22  |
| 12:00    |     |    |     |    | 191       | 17  | 192      | 17  |     |    |     |    |     |    | 383    | 34  | 191    | 17  |
| TOTALS   | 0   | 0  |     |    | 10219     |     | 9965     |     | 0   | 0  | 0   | 0  | 0   | 0  | 20184  |     | 10066  |     |
| AM Times |     |    |     |    | 11:15     |     | 11:15    |     |     |    |     |    |     |    | 11:15  |     | 11:15  |     |
| AM Peaks |     |    |     |    | 683       |     | 684      |     |     |    |     |    |     |    | 1367   |     | 682    |     |
| AM PHF   |     |    |     |    | 0.89      |     | 0.89     |     |     |    |     |    |     |    | 0.89   |     | 0.89   |     |
| PM Times |     |    |     |    | 16:45     |     | 17:00    |     |     |    |     |    |     |    | 16:45  |     | 16:45  |     |
| PM Peaks |     |    |     |    | 1060      |     | 1023     |     |     |    |     |    |     |    | 2081   |     | 1039   |     |
| PM PHF   |     |    |     |    | 0.93      |     | 0.87     |     |     |    |     |    |     |    | 0.90   |     | 0.90   |     |

## Coordination Mode Data

Date 1/22/2014 Time 9:53:54

Intersection Name 11 @ 114

Source Database

Operation Mode 0-Free

Mode (Normal) 1-Yield

Maximum 0-Inhibit

Correction 2-Short Way

Offset Mode 1-End Green

Force Mode 1-Cycle

Max Dwell Time 0

Yield Period 0

Manual Controls: Dial 1

Split 1

Offset 1

## Intersection Configuration

Date 1/22/2014

Time 9:54

|                     |                 |                  |         |
|---------------------|-----------------|------------------|---------|
| Intersection Name   | 11 @ 114        | Alias            | 0       |
| Source Intersection | Default         | Connection Type  | Dial Up |
| Group Identifier    | Fairlawn System | Control Method   | Marc    |
| Phone Number        |                 | Baud Rate        | Marc    |
| Protocol            | ECOM            | Controller Type  | EPAC    |
| Owning Agency       | Root            | Auto Reports     | No      |
|                     |                 | Version          | 2.32    |
|                     |                 | System Control   | No      |
| Page Number         | 1               |                  |         |
| Port Number         | 1               | Lock Dialing Out | No      |
| Address Number      | 1               |                  |         |
| Port Server Name    | LocalHost       |                  |         |





## Local TBC Special Function Data

Date 1/22/20                      Time 9:54:28

| Intersection Name            | 11 @ 114          |   |   |   |   |   |   |   |
|------------------------------|-------------------|---|---|---|---|---|---|---|
| Source                       | Database          |   |   |   |   |   |   |   |
| Function                     | Special<br>Func 1 | 2 | 3 | 4 | 5 | 6 | 7 | 8 |
| Special Function 1           | 1                 | 0 | 0 | 0 | 0 | 0 | 0 | 0 |
| Special Function 2           | 0                 | 1 | 0 | 0 | 0 | 0 | 0 | 0 |
| Special Function 3           | 0                 | 0 | 1 | 0 | 0 | 0 | 0 | 0 |
| Special Function 4           | 0                 | 0 | 0 | 1 | 0 | 0 | 0 | 0 |
| Special Function 5           | 0                 | 0 | 0 | 0 | 1 | 0 | 0 | 0 |
| Special Function 6           | 0                 | 0 | 0 | 0 | 0 | 1 | 0 | 0 |
| Special Function 7           | 0                 | 0 | 0 | 0 | 0 | 0 | 1 | 0 |
| Special Function 8           | 0                 | 0 | 0 | 0 | 0 | 0 | 0 | 1 |
| Passage 3 & Max 3            | 0                 | 0 | 0 | 0 | 0 | 0 | 0 | 0 |
| Passage 4 & Max 4            | 0                 | 0 | 0 | 0 | 0 | 0 | 0 | 0 |
| Passage 5 & Max 5            | 0                 | 0 | 0 | 0 | 0 | 0 | 0 | 0 |
| Dynamic Max 3                | 0                 | 0 | 0 | 0 | 0 | 0 | 0 | 0 |
| Dynamic Max 4                | 0                 | 0 | 0 | 0 | 0 | 0 | 0 | 0 |
| Dynamic Max 5                | 0                 | 0 | 0 | 0 | 0 | 0 | 0 | 0 |
| Disable Protected/Perm Omits | 0                 | 0 | 0 | 0 | 0 | 0 | 0 | 0 |
| Phase 2 Sign Control         | 0                 | 0 | 0 | 0 | 0 | 0 | 0 | 0 |

### Local TBC Special Function Data

Date  
1/22/20

Time  
9:54:28

Intersection Name                      11 @ 114

Source                                      Database

| Function                            | Special<br>Function<br>1 | 2 | 3 | 4 | 5 | 6 | 7 | 8 |
|-------------------------------------|--------------------------|---|---|---|---|---|---|---|
| Phase 4 Sign Control                | 0                        | 0 | 0 | 0 | 0 | 0 | 0 | 0 |
| Phase 6 Sign Control                | 0                        | 0 | 0 | 0 | 0 | 0 | 0 | 0 |
| Phase 8 Sign Control                | 0                        | 0 | 0 | 0 | 0 | 0 | 0 | 0 |
| Texas Diamond 4 Phase               | 0                        | 0 | 0 | 0 | 0 | 0 | 0 | 0 |
| Texas Diamond 3 Phase               | 0                        | 0 | 0 | 0 | 0 | 0 | 0 | 0 |
| Texas Diamond Separate              | 0                        | 0 | 0 | 0 | 0 | 0 | 0 | 0 |
| Queue 1/Level 1 Controls            | 0                        | 0 | 0 | 0 | 0 | 0 | 0 | 0 |
| Queue 1/Level 2 Controls            | 0                        | 0 | 0 | 0 | 0 | 0 | 0 | 0 |
| Queue 2/Level 1 Controls            | 0                        | 0 | 0 | 0 | 0 | 0 | 0 | 0 |
| Queue 2/Level 2 Controls            | 0                        | 0 | 0 | 0 | 0 | 0 | 0 | 0 |
| Alt Sequence 8-15 Flash Green Phase | 0                        | 0 | 0 | 0 | 0 | 0 | 0 | 0 |
| Alt Sequence 8-15 Flash Red Phase   | 0                        | 0 | 0 | 0 | 0 | 0 | 0 | 0 |
| Reserved 01                         | 0                        | 0 | 0 | 0 | 0 | 0 | 0 | 0 |
| Coord Adaptive Split                | 0                        | 0 | 0 | 0 | 0 | 0 | 0 | 0 |
| SPC 1-8 AS PHS FUNC 1-8             | 0                        | 0 | 0 | 0 | 0 | 0 | 0 | 0 |
| SPC 1-8 AS PHS FUNC 9-16            | 0                        | 0 | 0 | 0 | 0 | 0 | 0 | 0 |



## Phase General Control Data

Date 1/22/2014

Time 9:50:22

Intersection Name 11 @ 114  
Source Database

| Phase                 | 1          | 2       | 3      | 4          | 5      | 6       |
|-----------------------|------------|---------|--------|------------|--------|---------|
| Initial               | 1-Inactive | 4-Green | 0-None | 1-Inactive | 0-None | 4-Green |
| Non-Actuated Response | 0-None     | 3-Both  | 0-None | 0-None     | 0-None | 3-Both  |
| Vehicle Recalls       | 0-None     | 2-Min   | 0-None | 0-None     | 0-None | 2-Min   |
| Ped Recalls           | 0-None     | 0-None  | 0-None | 0-None     | 0-None | 0-None  |
| Recall Delay          | 0          | 0       | 0      | 0          | 0      | 0       |
| Flash Entry           | 0          |         |        |            |        |         |
| Flash Exit            | ^          |         |        |            |        |         |

| Phase                 | 7      | 8          | 9      | 10     | 11     | 12     |
|-----------------------|--------|------------|--------|--------|--------|--------|
| Initial               | 0-None | 1-Inactive | 0-None | 0-None | 0-None | 0-None |
| Non-Actuated Response | 0-None | 0-None     | 0-None | 0-None | 0-None | 0-None |
| Vehicle Recalls       | 0-None | 0-None     | 0-None | 0-None | 0-None | 0-None |
| Ped Recalls           | 0-None | 0-None     | 0-None | 0-None | 0-None | 0-None |
| Recall Delay          | 0      | 0          | 0      | 0      | 0      | 0      |
| Flash Entry           |        |            |        |        |        |        |
| Flash Exit            |        |            |        |        |        |        |

| Phase                 | 13     | 14     | 15     | 16     |
|-----------------------|--------|--------|--------|--------|
| Initial               | 0-None | 0-None | 0-None | 0-None |
| Non-Actuated Response | 0-None | 0-None | 0-None | 0-None |
| Vehicle Recalls       | 0-None | 0-None | 0-None | 0-None |
| Ped Recalls           | 0-None | 0-None | 0-None | 0-None |
| Recall Delay          | 0      | 0      | 0      | 0      |
| Flash Entry           |        |        |        |        |
| Flash Exit            |        |        |        |        |

## Phase Miscellanenous Data

Date 1/22/2014 Time 9:50:36

Intersection Name 11 @ 114

Source Database

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| <b>Phase</b>            | <b>1</b> | <b>2</b> | <b>3</b> | <b>4</b> | <b>5</b> | <b>6</b> | <b>7</b> | <b>8</b> |
|-------------------------|----------|----------|----------|----------|----------|----------|----------|----------|
| Non-Locking Memory      | 0        | 1        | 1        | 1        | 1        | 1        | 1        | 1        |
| Dual Entry              | 0        | 0        | 0        | 0        | 0        | 0        | 0        | 0        |
| Last Car Passage        | 0        | 0        | 0        | 0        | 0        | 0        | 0        | 0        |
| Conditional Service     | 0        | 0        | 0        | 0        | 0        | 0        | 0        | 0        |
| No Simultaneous Gap Out | 0        | 0        | 0        | 0        | 0        | 0        | 0        | 0        |

---

| <b>Phase</b>            | <b>9</b> | <b>10</b> | <b>11</b> | <b>12</b> | <b>13</b> | <b>14</b> | <b>15</b> | <b>16</b> |
|-------------------------|----------|-----------|-----------|-----------|-----------|-----------|-----------|-----------|
| Non-Locking Memory      | 0        | 0         | 0         | 0         | 0         | 0         | 0         | 0         |
| Dual Entry              | 0        | 0         | 0         | 0         | 0         | 0         | 0         | 0         |
| Last Car Passage        | 0        | 0         | 0         | 0         | 0         | 0         | 0         | 0         |
| Conditional Service     | 0        | 0         | 0         | 0         | 0         | 0         | 0         | 0         |
| No Simultaneous Gap Out | 0        | 0         | 0         | 0         | 0         | 0         | 0         | 0         |

# Phase Vehicle Basic Timing Data

Date 1/22/2014

Time 9:49:58

Intersection Name 11 @ 114

Source Database

| Phase         | 1   | 2   | 3   | 4   | 5   | 6   | 7   | 8   |
|---------------|-----|-----|-----|-----|-----|-----|-----|-----|
| Minimum Green | 6   | 14  | 0   | 8   | 0   | 14  | 0   | 8   |
| Passage       | 4.0 | 4.5 | 0.0 | 4.5 | 0.0 | 4.5 | 0.0 | 4.5 |
| Maximum 1     | 40  | 45  | 0   | 65  | 0   | 45  | 0   | 65  |
| Maximum 2     | 40  | 45  | 0   | 65  | 0   | 45  | 0   | 65  |
| Yellow Change | 4.0 | 4.5 | 3.0 | 4.0 | 3.0 | 4.5 | 3.0 | 4.0 |
| Red Clearance | 1.5 | 1.5 | 0.0 | 1.5 | 0.0 | 1.5 | 0.0 | 1.5 |

| Phase         | 9   | 10  | 11  | 12  | 13  | 14  | 15  | 16  |
|---------------|-----|-----|-----|-----|-----|-----|-----|-----|
| Minimum Green | 0   | 0   | 0   | 0   | 0   | 0   | 0   | 0   |
| Passage       | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 |
| Maximum 1     | 0   | 0   | 0   | 0   | 0   | 0   | 0   | 0   |
| Maximum 2     | 0   | 0   | 0   | 0   | 0   | 0   | 0   | 0   |
| Yellow Change | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 |
| Red Clearance | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 |

## Unit Channel Output Data

Date 1/22/2014 Time 9:51:21

Intersection Name 11 @ 114

Source Database

| Channel             | Control        | Hardware Pin   |
|---------------------|----------------|----------------|
| 1-Phase 1 Vehicle   | 1-Veh Phase 1  | 1-Phase 1 RYG  |
| 2-Phase 2 Vehicle   | 2-Veh Phase 2  | 2-Phase 2 RYG  |
| 3-Phase 3 Vehicle   | 3-Veh Phase 3  | 3-Phase 3 RYG  |
| 4-Phase 4 Vehicle   | 4-Veh Phase 4  | 4-Phase4 RYG   |
| 5-Phase 5 Vehicle   | 5-Veh Phase 5  | 5-Phase 5 RYG  |
| 6-Phase 6 Vehicle   | 6-Veh Phase 6  | 6-Phase 6 RYG  |
| 7-Phase 7 Vehicle   | 7-Veh Phase 7  | 7-Phase 7 RYG  |
| 8-Phase 8 Vehicle   | 8-Veh Phase 8  | 8-Phase 8 RYG  |
| 9-Phase 9 Vehicle   | 18-Ped Phase 2 | 10-Phase 2 DPW |
| 10-Phase 10 Vehicle | 20-Ped Phase 4 | 12-Phase 4 DPW |
| 11-Phase 11 Vehicle | 22-Ped Phase 6 | 14-Phase 6 DPW |
| 12-Phase 12 Vehicle | 24-Ped Phase 8 | 16-Phase 8 DPW |



### Unit Channel Output Data

Date 1/22/20 Time 9:51:21

Intersection Name 11 @ 114

Source Database

| Channel              | Control        | Hardware Pin     |
|----------------------|----------------|------------------|
| 13-Overlap A Vehicle | 33-Overlap A   | 17-Overlap A RYG |
| 14-Overlap B Vehicle | 34-Overlap B   | 18-Overlap B RYG |
| 15-Overlap C Vehicle | 35-Overlap C   | 19-Overlap C RYG |
| 16-Overlap D Vehicle | 36-Overlap D   | 20-Overlap D RYG |
| 17-Phase 1 Ped       | 17-Ped Phase 1 | 9-Phase 1 DPW    |
| 18-Phase 3 Ped       | 19-Ped Phase 3 | 11-Phase 3 DPW   |
| 19-Phase 5 Ped       | 21-Ped Phase 5 | 13-Phase 5 DPW   |
| 20-Phase 7 Ped       | 23-Ped Phase 7 | 15-Phase 7 DPW   |
| 21-Overlap E Vehicle | 37-Overlap E   | 21-Phase 1 ONC   |
| 22-Overlap F Vehicle | 38-Overlap F   | 22-Phase 2 ONC   |
| 23-Overlap G Vehicle | 39-Overlap G   | 23-Phase 3 ONC   |
| 24-Overlap H Vehicle | 40-Overlap H   | 24-Phase 4 ONC   |

# Unit General Control Data

Date 1/22/2014 Time 9:50:54

Intersection Name 11 @ 114  
Source Database

|                    |         |                |          |          |        |          |
|--------------------|---------|----------------|----------|----------|--------|----------|
| Startup Time       | 0       | Ring           | 1        | 2        | 3      | 4        |
| Startup State      | 0-Flash | Input Response | 1-Ring 1 | 2-Ring 2 | 0-None | 0-None   |
| Red Revert         | 2.0     | Output Select  | 1-Ring 1 | 2-Ring 2 | 0-None | 2-Ring 2 |
| Auto Ped Clear     | 0       | I/O Modes      | Input    | Output   |        |          |
| Stop Time reset    | 0       | ABC Connector  | 0        | 0        |        |          |
| Alternate Sequence | 0       | D Connector    | 0        | 0        |        |          |

## Unit Ring Data

Date 1/22/2014                      Time 9:51:09

Intersection Name                      11 @ 114

Source                                      Database

| Phase | Ring | Next | 1 | 2 | 3 | 4 | 5 | 6 | 7 | 8 | 9 | 10 | 11 | 12 | 13 | 14 | 15 | 16 |
|-------|------|------|---|---|---|---|---|---|---|---|---|----|----|----|----|----|----|----|
| 1     | 1    | 2    | 1 | 0 | 0 | 0 | 1 | 1 | 0 | 0 | 0 | 0  | 0  | 0  | 0  | 0  | 0  | 0  |
| 2     | 1    | 3    | 0 | 1 | 0 | 0 | 1 | 1 | 0 | 0 | 0 | 0  | 0  | 0  | 0  | 0  | 0  | 0  |
| 3     | 1    | 4    | 0 | 0 | 1 | 0 | 0 | 0 | 1 | 1 | 0 | 0  | 0  | 0  | 0  | 0  | 0  | 0  |
| 4     | 1    | 1    | 0 | 0 | 0 | 1 | 0 | 0 | 1 | 1 | 0 | 0  | 0  | 0  | 0  | 0  | 0  | 0  |
| 5     | 2    | 6    | 1 | 1 | 0 | 0 | 1 | 0 | 0 | 0 | 0 | 0  | 0  | 0  | 0  | 0  | 0  | 0  |
| 6     | 2    | 7    | 1 | 1 | 0 | 0 | 0 | 1 | 0 | 0 | 0 | 0  | 0  | 0  | 0  | 0  | 0  | 0  |
| 7     | 2    | 8    | 0 | 0 | 1 | 1 | 0 | 0 | 1 | 0 | 0 | 0  | 0  | 0  | 0  | 0  | 0  | 0  |
| 8     | 2    | 5    | 0 | 0 | 1 | 1 | 0 | 0 | 0 | 1 | 0 | 0  | 0  | 0  | 0  | 0  | 0  | 0  |
| 9     | 0    | 0    | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 1 | 0  | 0  | 0  | 0  | 0  | 0  | 0  |
| 10    | 0    | 0    | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 1  | 0  | 0  | 0  | 0  | 0  | 0  |
| 11    | 0    | 0    | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0  | 1  | 0  | 0  | 0  | 0  | 0  |
| 12    | 0    | 0    | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0  | 0  | 1  | 0  | 0  | 0  | 0  |
| 13    | 0    | 0    | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0  | 0  | 0  | 1  | 0  | 0  | 0  |
| 14    | 0    | 0    | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0  | 0  | 0  | 0  | 1  | 0  | 0  |
| 15    | 0    | 0    | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0  | 0  | 0  | 0  | 0  | 1  | 0  |
| 16    | 0    | 0    | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0  | 0  | 0  | 0  | 0  | 0  | 1  |

## Coordination Mode Data

Date 1/22/2014 Time 10:03:37

Intersection Name 11 @ Radford Plaza

Source Database

Operation Mode 0-Free

Mode (Normal) 0-Perm

Maximum 2-Max II

Correction 2-Short Way

Offset Mode 0-Beg Green

Force Mode 0-Plan

Max Dwell Time 0

Yield Period 0

Manual Controls: Dial 0

Split 0

Offset 0

## Intersection Configuration

Date 1/22/2014

Time 10:04

|                     |                   |                  |         |
|---------------------|-------------------|------------------|---------|
| Intersection Name   | 11@ Radford Plaza | Alias            | 0       |
| Source Intersection | Default           | Connection Type  | Dial Up |
| Group Identifier    | Fairlawn System   | Control Method   | Marc    |
| Phone Number        |                   | Baud Rate        | Marc    |
| Protocol            | ECOM              | Controller Type  | EPAC    |
| Owning Agency       | Root              | Auto Reports     | No      |
|                     |                   | Version          | 3.32a   |
|                     |                   | System Control   | No      |
| Page Number         | 1                 |                  |         |
| Port Number         | 1                 | Lock Dialing Out | No      |
| Address Number      | 4                 |                  |         |
| Port Server Name    | LocalHost         |                  |         |

## Phase Miscellanenous Data

Date 1/22/2014 Time 10:01:42

Intersection Name 11@ Radford Plaza

Source Database

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| <b>Phase</b>            | <b>1</b> | <b>2</b> | <b>3</b> | <b>4</b> | <b>5</b> | <b>6</b> | <b>7</b> | <b>8</b> |
|-------------------------|----------|----------|----------|----------|----------|----------|----------|----------|
| Non-Locking Memory      | 1        | 1        | 1        | 1        | 1        | 1        | 1        | 1        |
| Dual Entry              | 0        | 0        | 0        | 0        | 0        | 0        | 0        | 0        |
| Last Car Passage        | 0        | 0        | 0        | 0        | 0        | 0        | 0        | 0        |
| Conditional Service     | 0        | 0        | 0        | 0        | 0        | 0        | 0        | 0        |
| No Simultaneous Gap Out | 0        | 0        | 0        | 0        | 0        | 0        | 0        | 0        |

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| <b>Phase</b>            | <b>9</b> | <b>10</b> | <b>11</b> | <b>12</b> | <b>13</b> | <b>14</b> | <b>15</b> | <b>16</b> |
|-------------------------|----------|-----------|-----------|-----------|-----------|-----------|-----------|-----------|
| Non-Locking Memory      | 0        | 0         | 0         | 0         | 0         | 0         | 0         | 0         |
| Dual Entry              | 0        | 0         | 0         | 0         | 0         | 0         | 0         | 0         |
| Last Car Passage        | 0        | 0         | 0         | 0         | 0         | 0         | 0         | 0         |
| Conditional Service     | 0        | 0         | 0         | 0         | 0         | 0         | 0         | 0         |
| No Simultaneous Gap Out | 0        | 0         | 0         | 0         | 0         | 0         | 0         | 0         |

# Phase Vehicle Basic Timing Data

Date 1/22/2014

Time 10:00:34

Intersection Name 11@ Radford Plaza

Source Database

| Phase         | 1   | 2   | 3   | 4   | 5   | 6   | 7   | 8   |
|---------------|-----|-----|-----|-----|-----|-----|-----|-----|
| Minimum Green | 6   | 16  | 0   | 7   | 6   | 16  | 0   | 7   |
| Passage       | 3.5 | 4.0 | 0.0 | 4.0 | 3.5 | 4.0 | 0.0 | 4.0 |
| Maximum 1     | 16  | 50  | 0   | 20  | 20  | 50  | 0   | 30  |
| Maximum 2     | 16  | 50  | 0   | 20  | 20  | 50  | 0   | 30  |
| Yellow Change | 4.0 | 4.5 | 3.0 | 4.0 | 4.0 | 4.5 | 3.0 | 4.0 |
| Red Clearance | 1.5 | 1.5 | 0.0 | 1.5 | 1.5 | 1.5 | 0.0 | 1.5 |

| Phase         | 9   | 10  | 11  | 12  | 13  | 14  | 15  | 16  |
|---------------|-----|-----|-----|-----|-----|-----|-----|-----|
| Minimum Green | 0   | 0   | 0   | 0   | 0   | 0   | 0   | 0   |
| Passage       | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 |
| Maximum 1     | 0   | 0   | 0   | 0   | 0   | 0   | 0   | 0   |
| Maximum 2     | 0   | 0   | 0   | 0   | 0   | 0   | 0   | 0   |
| Yellow Change | 4.0 | 4.0 | 4.0 | 4.0 | 4.0 | 4.0 | 4.0 | 4.0 |
| Red Clearance | 2.0 | 2.0 | 2.0 | 2.0 | 2.0 | 2.0 | 2.0 | 2.0 |

### Unit Alt Sequence Data

Date 1/22/2014 Time 10:02:57

Intersection Name 11@ Radford Plaza

Source Database

| Sequence/<br>Pairs | 1/1 | 1/2 | 2/1 | 2/2 | 3/1 | 3/2 | 4/1 | 4/2 |
|--------------------|-----|-----|-----|-----|-----|-----|-----|-----|
| 1                  | 1   | 2   | 0   | 0   | 0   | 0   | 0   | 0   |
| 2                  | 3   | 4   | 0   | 0   | 0   | 0   | 0   | 0   |
| 3                  | 1   | 2   | 3   | 4   | 0   | 0   | 0   | 0   |
| 4                  | 5   | 6   | 0   | 0   | 0   | 0   | 0   | 0   |
| 5                  | 1   | 2   | 5   | 6   | 0   | 0   | 0   | 0   |
| 6                  | 3   | 4   | 5   | 6   | 0   | 0   | 0   | 0   |
| 7                  | 1   | 2   | 3   | 4   | 5   | 6   | 0   | 0   |
| 8                  | 7   | 8   | 0   | 0   | 0   | 0   | 0   | 0   |
| 9                  | 1   | 2   | 7   | 8   | 0   | 0   | 0   | 0   |
| 10                 | 3   | 4   | 7   | 8   | 0   | 0   | 0   | 0   |
| 11                 | 1   | 2   | 3   | 4   | 7   | 8   | 0   | 0   |
| 12                 | 5   | 6   | 7   | 8   | 0   | 0   | 0   | 0   |
| 13                 | 1   | 2   | 5   | 6   | 7   | 8   | 0   | 0   |
| 14                 | 3   | 4   | 5   | 6   | 7   | 8   | 0   | 0   |
| 15                 | 1   | 2   | 3   | 4   | 5   | 6   | 7   | 8   |



## Unit Alt Sequence Data

Date 1/22/20 Time 10:02:57

Intersection Name 11@ Radford Plaza

Source Database

| Sequence/<br>Pairs | 5/1 | 5/2 | 6/1 | 6/2 | 7/1 | 7/2 | 8/1 | 8/2 |
|--------------------|-----|-----|-----|-----|-----|-----|-----|-----|
| 1                  | 0   | 0   | 0   | 0   | 0   | 0   | 0   | 0   |
| 2                  | 0   | 0   | 0   | 0   | 0   | 0   | 0   | 0   |
| 3                  | 0   | 0   | 0   | 0   | 0   | 0   | 0   | 0   |
| 4                  | 0   | 0   | 0   | 0   | 0   | 0   | 0   | 0   |
| 5                  | 0   | 0   | 0   | 0   | 0   | 0   | 0   | 0   |
| 6                  | 0   | 0   | 0   | 0   | 0   | 0   | 0   | 0   |
| 7                  | 0   | 0   | 0   | 0   | 0   | 0   | 0   | 0   |
| 8                  | 0   | 0   | 0   | 0   | 0   | 0   | 0   | 0   |
| 9                  | 0   | 0   | 0   | 0   | 0   | 0   | 0   | 0   |
| 10                 | 0   | 0   | 0   | 0   | 0   | 0   | 0   | 0   |
| 11                 | 0   | 0   | 0   | 0   | 0   | 0   | 0   | 0   |
| 12                 | 0   | 0   | 0   | 0   | 0   | 0   | 0   | 0   |
| 13                 | 0   | 0   | 0   | 0   | 0   | 0   | 0   | 0   |
| 14                 | 0   | 0   | 0   | 0   | 0   | 0   | 0   | 0   |
| 15                 | 0   | 0   | 0   | 0   | 0   | 0   | 0   | 0   |



## Unit Channel Output Data

Date 1/22/2014 Time 10:03:11

Intersection Name 11 @ Radford Plaza

Source Database

| Channel             | Control        | Hardware Pin   |
|---------------------|----------------|----------------|
| 1-Phase 1 Vehicle   | 1-Veh Phase 1  | 1-Phase 1 RYG  |
| 2-Phase 2 Vehicle   | 2-Veh Phase 2  | 2-Phase 2 RYG  |
| 3-Phase 3 Vehicle   | 3-Veh Phase 3  | 3-Phase 3 RYG  |
| 4-Phase 4 Vehicle   | 4-Veh Phase 4  | 4-Phase4 RYG   |
| 5-Phase 5 Vehicle   | 5-Veh Phase 5  | 5-Phase 5 RYG  |
| 6-Phase 6 Vehicle   | 6-Veh Phase 6  | 6-Phase 6 RYG  |
| 7-Phase 7 Vehicle   | 7-Veh Phase 7  | 7-Phase 7 RYG  |
| 8-Phase 8 Vehicle   | 8-Veh Phase 8  | 8-Phase 8 RYG  |
| 9-Phase 9 Vehicle   | 18-Ped Phase 2 | 10-Phase 2 DPW |
| 10-Phase 10 Vehicle | 20-Ped Phase 4 | 12-Phase 4 DPW |
| 11-Phase 11 Vehicle | 22-Ped Phase 6 | 14-Phase 6 DPW |
| 12-Phase 12 Vehicle | 24-Ped Phase 8 | 16-Phase 8 DPW |

### Unit Channel Output Data

Date 1/22/20 Time 10:03:11

Intersection Name 11@ Radford Plaza

Source Database

| Channel              | Control        | Hardware Pin        |
|----------------------|----------------|---------------------|
| 13-Overlap A Vehicle | 33-Overlap A   | 17-Overlap A<br>RYG |
| 14-Overlap B Vehicle | 34-Overlap B   | 18-Overlap B<br>RYG |
| 15-Overlap C Vehicle | 35-Overlap C   | 19-Overlap C<br>RYG |
| 16-Overlap D Vehicle | 36-Overlap D   | 20-Overlap D<br>RYG |
| 17-Phase 1 Ped       | 17-Ped Phase 1 | 9-Phase 1 DPW       |
| 18-Phase 3 Ped       | 19-Ped Phase 3 | 11-Phase 3 DPW      |
| 19-Phase 5 Ped       | 21-Ped Phase 5 | 13-Phase 5 DPW      |
| 20-Phase 7 Ped       | 23-Ped Phase 7 | 15-Phase 7 DPW      |
| 21-Overlap E Vehicle | 37-Overlap E   | 0-None              |
| 22-Overlap F Vehicle | 38-Overlap F   | 0-None              |
| 23-Overlap G Vehicle | 39-Overlap G   | 0-None              |
| 24-Overlap H Vehicle | 40-Overlap H   | 0-None              |

# Unit General Control Data

Date 1/22/2014 Time 10:02:32

Intersection Name 11 @ Radford Plaza  
Source Database

|                    |         |                |          |          |        |        |
|--------------------|---------|----------------|----------|----------|--------|--------|
| Startup Time       | 0       | Ring           | 1        | 2        | 3      | 4      |
| Startup State      | 0-Flash | Input Response | 1-Ring 1 | 2-Ring 2 | 0-None | 0-None |
| Red Revert         | 2.0     | Output Select  | 1-Ring 1 | 2-Ring 2 | 0-None | 0-None |
| Auto Ped Clear     | 0       | I/O Modes      | Input    | Output   |        |        |
| Stop Time reset    | 0       | ABC Connector  | 0        | 0        |        |        |
| Alternate Sequence | 0       | D Connector    | 0        | 0        |        |        |

# Unit Ring Data

Date 1/22/2014                      Time 10:02:44

Intersection Name                      11 @ Radford Plaza

Source                                      Database

| Phase | Ring | Next | 1 | 2 | 3 | 4 | 5 | 6 | 7 | 8 | 9 | 10 | 11 | 12 | 13 | 14 | 15 | 16 |
|-------|------|------|---|---|---|---|---|---|---|---|---|----|----|----|----|----|----|----|
| 1     | 1    | 2    | 1 | 0 | 0 | 0 | 1 | 1 | 0 | 0 | 0 | 0  | 0  | 0  | 0  | 0  | 0  | 0  |
| 2     | 1    | 3    | 0 | 1 | 0 | 0 | 1 | 1 | 0 | 0 | 0 | 0  | 0  | 0  | 0  | 0  | 0  | 0  |
| 3     | 1    | 4    | 0 | 0 | 1 | 0 | 0 | 0 | 1 | 1 | 0 | 0  | 0  | 0  | 0  | 0  | 0  | 0  |
| 4     | 1    | 1    | 0 | 0 | 0 | 1 | 0 | 0 | 1 | 1 | 0 | 0  | 0  | 0  | 0  | 0  | 0  | 0  |
| 5     | 2    | 6    | 1 | 1 | 0 | 0 | 1 | 0 | 0 | 0 | 0 | 0  | 0  | 0  | 0  | 0  | 0  | 0  |
| 6     | 2    | 7    | 1 | 1 | 0 | 0 | 0 | 1 | 0 | 0 | 0 | 0  | 0  | 0  | 0  | 0  | 0  | 0  |
| 7     | 2    | 8    | 0 | 0 | 1 | 1 | 0 | 0 | 1 | 0 | 0 | 0  | 0  | 0  | 0  | 0  | 0  | 0  |
| 8     | 2    | 5    | 0 | 0 | 1 | 1 | 0 | 0 | 0 | 1 | 0 | 0  | 0  | 0  | 0  | 0  | 0  | 0  |
| 9     | 0    | 0    | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 1 | 0  | 0  | 0  | 0  | 0  | 0  | 0  |
| 10    | 0    | 0    | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 1  | 0  | 0  | 0  | 0  | 0  | 0  |
| 11    | 0    | 0    | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0  | 1  | 0  | 0  | 0  | 0  | 0  |
| 12    | 0    | 0    | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0  | 0  | 1  | 0  | 0  | 0  | 0  |
| 13    | 0    | 0    | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0  | 0  | 0  | 1  | 0  | 0  | 0  |
| 14    | 0    | 0    | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0  | 0  | 0  | 0  | 1  | 0  | 0  |
| 15    | 0    | 0    | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0  | 0  | 0  | 0  | 0  | 1  | 0  |
| 16    | 0    | 0    | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0  | 0  | 0  | 0  | 0  | 0  | 1  |

## Phase General Control Data

Date 1/22/2014

Time 10:01:02

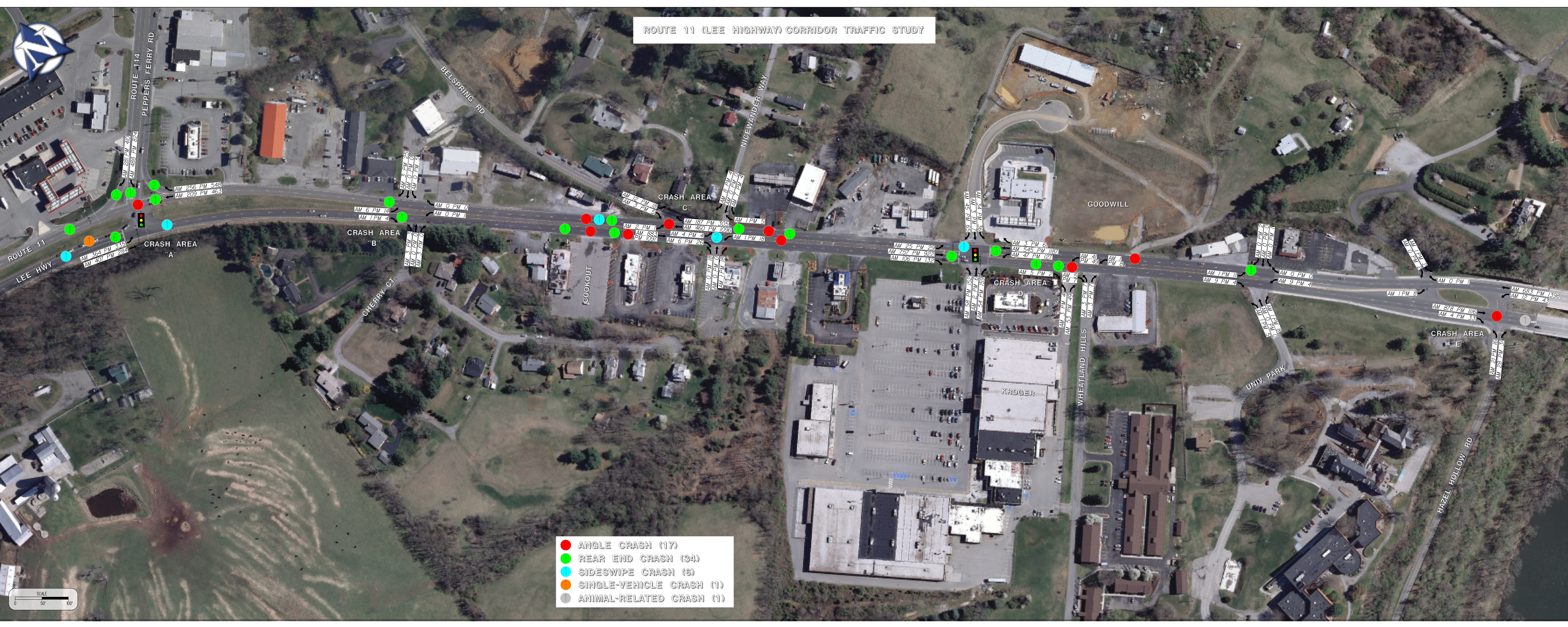
Intersection Name 11@ Radford Plaza  
Source Database

| Phase                 | 1          | 2       | 3      | 4          | 5          | 6       |
|-----------------------|------------|---------|--------|------------|------------|---------|
| Initial               | 1-Inactive | 4-Green | 0-None | 1-Inactive | 1-Inactive | 4-Green |
| Non-Actuated Response | 0-None     | 3-Both  | 0-None | 0-None     | 0-None     | 3-Both  |
| Vehicle Recalls       | 0-None     | 2-Min   | 0-None | 0-None     | 0-None     | 2-Min   |
| Ped Recalls           | 0-None     | 0-None  | 0-None | 0-None     | 0-None     | 0-None  |
| Recall Delay          | 0          | 0       | 0      | 0          | 0          | 0       |
| Flash Entry           | 0          |         |        |            |            |         |
| Flash Exit            | ^          |         |        |            |            |         |

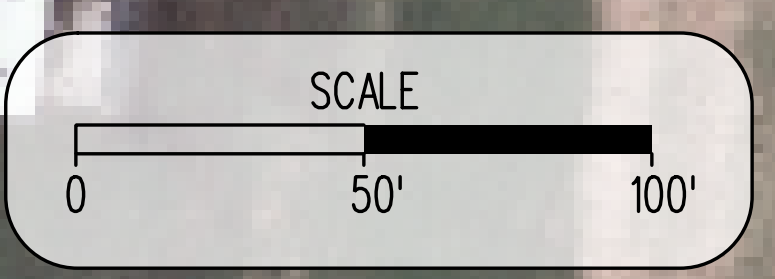
| Phase                 | 7      | 8          | 9      | 10     | 11     | 12     |
|-----------------------|--------|------------|--------|--------|--------|--------|
| Initial               | 0-None | 1-Inactive | 0-None | 0-None | 0-None | 0-None |
| Non-Actuated Response | 0-None | 0-None     | 0-None | 0-None | 0-None | 0-None |
| Vehicle Recalls       | 0-None | 0-None     | 0-None | 0-None | 0-None | 0-None |
| Ped Recalls           | 0-None | 0-None     | 0-None | 0-None | 0-None | 0-None |
| Recall Delay          | 0      | 0          | 0      | 0      | 0      | 0      |
| Flash Entry           |        |            |        |        |        |        |
| Flash Exit            |        |            |        |        |        |        |

| Phase                 | 13     | 14     | 15     | 16     |
|-----------------------|--------|--------|--------|--------|
| Initial               | 0-None | 0-None | 0-None | 0-None |
| Non-Actuated Response | 0-None | 0-None | 0-None | 0-None |
| Vehicle Recalls       | 0-None | 0-None | 0-None | 0-None |
| Ped Recalls           | 0-None | 0-None | 0-None | 0-None |
| Recall Delay          | 0      | 0      | 0      | 0      |
| Flash Entry           |        |        |        |        |
| Flash Exit            |        |        |        |        |

ROUTE 11 (LEE HIGHWAY) CORRIDOR TRAFFIC STUDY



- ANGLE CRASH (17)
- REAR END CRASH (34)
- SIDESWIPE CRASH (6)
- SINGLE-VEHICLE CRASH (1)
- ANIMAL-RELATED CRASH (1)







## APPENDIX B

### Proposed Improvements Decision Matrix



**Route 11 / Lee Highway – Traffic Analysis  
Technical Summary – Comparison of Access / Improvement Options  
June 12, 2015**

| <b>Short-Term – Construct Dual Left-Turn from Route 11 NB to Route 114 EB</b> |   |  |
|---|---|--|
| <b>Measure of Effectiveness</b>   | <b>Leave Existing Conditions</b>  | <b>Construct Improvements</b>  |
| <b>Overall Traffic Operations and Access / Circulation</b>                    | <ul style="list-style-type: none"> <li>✓ No change to existing intersection geometry</li> <li>✓ No change to existing traffic signal</li> <li>✗ Does not address left-turn volumes or queues during peak hours</li> </ul> | <ul style="list-style-type: none"> <li>✗ Existing intersection geometry would need to be changed – WBL would change to yield from free-flow</li> <li>✗ Existing traffic signal would need to be modified or replaced to accommodate change in geometry</li> <li>✓ Would improve delays and queues for EBL turning movement</li> <li>✓ Would improve overall intersection operational efficiency</li> </ul> |
| <b>Vehicle Conflicts and Traffic Safety</b>                                   | <ul style="list-style-type: none"> <li>✗ Does not reduce potential for rear-end crashes due to long queues</li> </ul>   | <ul style="list-style-type: none"> <li>✓ Would reduce potential for rear-end crashes by reducing long queues</li> </ul>  |

| <b>Short-Term – Consolidate Entrances – Powers Fence &amp; Radford Animal Hospital &amp; Construct Route 11 NB Left-Turn Lane</b> |  |  |
|---|--|--|
| <b>Measure of Effectiveness</b>   | <b>Leave Existing Conditions</b>   | <b>Construct Improvements</b>  |
| <b>Overall Traffic Operations and Access / Circulation</b>  | <ul style="list-style-type: none"> <li>✓ Maintains all existing traffic movements</li> <li>✓ Maintains direct and multiple access points to several businesses</li> </ul>                              | <ul style="list-style-type: none"> <li>✗ Property owners may not agree to access changes</li> <li>✓ Full access maintained to parcels</li> </ul>   |
| <b>Vehicle Conflicts and Traffic Safety</b>   | <ul style="list-style-type: none"> <li>✗ Does not reduce potential for rear-end crashes due to slowing/turning vehicles</li> <li>✓ No significant existing safety problems at this location</li> </ul> | <ul style="list-style-type: none"> <li>✓ Provides EB left-turn lane for left and U-turns removed from through lanes</li> <li>✓ 5 total access points consolidated to 2 reduces total potential vehicular conflict points for Route 11</li> </ul> |



| Long-Term – Realign Belspring Road and Consolidate Access – Dalton TV, Empty Parcels |  |  |
|--|--|--|
| Measure of Effectiveness   | Leave Existing Conditions  | Construct Improvements   |
| <b>Overall Traffic Operations and Access / Circulation</b>                           | <ul style="list-style-type: none"> <li>✓ Maintains all existing traffic movements</li> <li>✓ Maintain direct and multiple access points to several businesses</li> </ul> | <ul style="list-style-type: none"> <li>✗ Property owners may not agree to access changes</li> <li>✗ Grade of realigned Belspring Road may be too steep</li> <li>✓ Full access maintained to parcels</li> </ul> |
| <b>Vehicle Conflicts and Traffic Safety</b>  | <ul style="list-style-type: none"> <li>✗ Does not reduce potential vehicle conflicts</li> <li>✓ No significant existing safety problems at these locations</li> </ul>    | <ul style="list-style-type: none"> <li>✓ 5 total access points consolidated to 3 reduces total potential vehicular conflict points for Route 11</li> </ul>   |

| Short-Term – Consolidate Access – Former Deli Mart, TitleMax, Pure |   |   |
|--|---|---|
| Measure of Effectiveness   | Leave Existing Conditions   | Construct Improvements  |
| <b>Overall Traffic Operations and Access / Circulation</b>         | <ul style="list-style-type: none"> <li>✓ Maintains all existing traffic movements</li> <li>✓ Maintains direct and multiple access points to several businesses</li> </ul>   | <ul style="list-style-type: none"> <li>✗ Property owners may not agree to access changes</li> <li>✓ Full access maintained to parcels</li> </ul>  |
| <b>Vehicle Conflicts and Traffic Safety</b>                        | <ul style="list-style-type: none"> <li>✗ Does not reduce potential vehicle conflicts</li> <li>✗ Sight-distance restriction from TitleMax due to retaining wall</li> <li>✓ No significant existing safety problems at these locations</li> </ul> | <ul style="list-style-type: none"> <li>✓ 11 total access points consolidated to 6 reduces total potential conflict points for Route 11</li> <li>✓ Retaining wall removed with entrance consolidation to improve sight distance</li> </ul> |



| Short-Term – Construct Right-Turn Lane on Route 11 NB to Kroger, Improve Entrance Geometry |  |   |
|--|--|---|
| Measure of Effectiveness   | Leave Existing Conditions  | Construct Improvements  |
| <b>Overall Traffic Operations and Access / Circulation</b>                                 | <ul style="list-style-type: none"> <li>✓ Maintains all existing traffic movements</li> <li>✗ Does not increase intersection capacity</li> <li>✗ Does not improve traffic operations</li> </ul> | <ul style="list-style-type: none"> <li>✓ Maintains all existing traffic movements</li> <li>✓ Increases intersection capacity</li> <li>✓ Improves intersection traffic operations</li> </ul> |
| <b>Vehicle Conflicts and Traffic Safety</b>  | <ul style="list-style-type: none"> <li>✗ Does not reduce potential vehicle conflicts</li> <li>✓ No significant existing safety problems at this location</li> </ul>                            | <ul style="list-style-type: none"> <li>✓ Rear-end crash potential would be reduced by separating right-turn vehicles from through traffic</li> </ul>  |

| Short-Term – Construct Median between McDonald’s and Wilco/Hess |   |   |
|---|---|---|
| Measure of Effectiveness  | Leave Existing Conditions   | Construct Improvements  |
| <b>Overall Traffic Operations and Access / Circulation</b>      | <ul style="list-style-type: none"> <li>✗ Maintains a prohibited traffic movement</li> <li>✗ Drivers accessing Wilco/Hess via left-turns are improperly using right-in only entrance</li> </ul>                | <ul style="list-style-type: none"> <li>✓ Right-in entrance would remain in place</li> <li>✓ Volume of left-turns would be easily accommodated at existing traffic signal</li> </ul>   |
| <b>Vehicle Conflicts and Traffic Safety</b>                     | <ul style="list-style-type: none"> <li>✗ Does not reduce potential vehicle conflicts</li> <li>✗ Rear-end crash history with vehicles turning left into Wilco/Hess entrance, which is right-in only</li> </ul> | <ul style="list-style-type: none"> <li>✓ Reduces potential vehicle conflicts</li> <li>✓ Reduces rear-end crash potential by directing drivers to traffic signal for left-turns to Wilco/Hess</li> <li>✗ EB traffic may attempt a U-turn at the end of the median</li> </ul> |



| <b>Long-Term – Consolidate Entrances to Kroger/McDonald’s, Wheatland Hills, Executive Motel</b> |  |  |
|---|--|--|
| <b>Measure of Effectiveness</b>   | <b>Leave Existing Conditions</b>   | <b>Construct Improvements</b>  |
| <b>Overall Traffic Operations and Access / Circulation</b>                                      | <ul style="list-style-type: none"> <li>✓ Maintains all existing traffic movements</li> <li>✓ Maintains direct and multiple access points to existing business parcels</li> </ul> | <ul style="list-style-type: none"> <li>✗ Property owners may not agree to access changes</li> <li>✓ Full access to all businesses provided via a single consolidated entrance</li> </ul>   |
| <b>Vehicle Conflicts and Traffic Safety</b>   | <ul style="list-style-type: none"> <li>✗ Leaves two very closely spaced entrances along Route 11</li> <li>✗ Minor crash history due to close entrance spacing</li> </ul>         | <ul style="list-style-type: none"> <li>✓ Eliminates close entrance spacing between Kroger and Wheatland Hills</li> <li>✓ 4 total access points consolidated to 2 reduces total potential vehicular conflict points for Route 11</li> </ul> |

| <b>Short-Term – Construct Median on Route 11, Consolidate Entrances to Empty Property</b> |   |   |
|---|---|---|
| <b>Measure of Effectiveness</b>   | <b>Leave Existing Conditions</b>  | <b>Construct Improvements</b>   |
| <b>Overall Traffic Operations and Access / Circulation</b>                                | <ul style="list-style-type: none"> <li>✓ Maintains all existing traffic movements</li> <li>✓ Maintains direct and multiple access points to existing business parcels</li> </ul>  | <ul style="list-style-type: none"> <li>✗ Property owners may not agree to access changes</li> <li>✓ Partial and U-turn access maintained to vacant building</li> </ul>  |
| <b>Vehicle Conflicts and Traffic Safety</b>   | <ul style="list-style-type: none"> <li>✗ Leaves two-way left-turn lane in median, which could lead to future vehicle crashes with new developments</li> <li>✓ No significant existing safety problems at this location</li> </ul> | <ul style="list-style-type: none"> <li>✓ Provides dedicated EB and WB left-turn lanes for traffic along Route 11 to access existing businesses</li> <li>✓ 2 total access points consolidated to 1 reduces total potential vehicular conflict points for Route 11</li> <li>✓ Channelized access via left-turn lanes helps future corridor access management for new developments or redevelopment of existing parcels</li> </ul> |



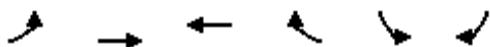
## APPENDIX C

### Traffic Analysis Results

# HCM Signalized Intersection Capacity Analysis

## 2: Route 11 & Route 114

12/8/2015



| Movement               | EBL   | EBT  | WBT   | WBR  | SBL   | SBR  |
|------------------------|-------|------|-------|------|-------|------|
| Lane Configurations    |       |      |       |      |       |      |
| Traffic Volume (vph)   | 364   | 407  | 209   | 256  | 488   | 281  |
| Future Volume (vph)    | 364   | 407  | 209   | 256  | 488   | 281  |
| Ideal Flow (vphpl)     | 1900  | 1900 | 1900  | 1900 | 1900  | 1900 |
| Total Lost time (s)    | 5.5   | 6.0  | 6.0   | 4.0  | 5.5   | 4.0  |
| Lane Util. Factor      | 1.00  | 0.95 | 0.95  | 1.00 | 0.97  | 1.00 |
| Frt                    | 1.00  | 1.00 | 1.00  | 0.85 | 1.00  | 0.85 |
| Flt Protected          | 0.95  | 1.00 | 1.00  | 1.00 | 0.95  | 1.00 |
| Satd. Flow (prot)      | 1719  | 3574 | 3539  | 1553 | 3467  | 1524 |
| Flt Permitted          | 0.95  | 1.00 | 1.00  | 1.00 | 0.95  | 1.00 |
| Satd. Flow (perm)      | 1719  | 3574 | 3539  | 1553 | 3467  | 1524 |
| Peak-hour factor, PHF  | 0.86  | 0.86 | 0.92  | 0.92 | 0.88  | 0.88 |
| Adj. Flow (vph)        | 423   | 473  | 227   | 278  | 555   | 319  |
| RTOR Reduction (vph)   | 0     | 0    | 0     | 0    | 0     | 0    |
| Lane Group Flow (vph)  | 423   | 473  | 227   | 278  | 555   | 319  |
| Heavy Vehicles (%)     | 5%    | 1%   | 2%    | 4%   | 1%    | 6%   |
| Turn Type              | Prot  | NA   | NA    | Free | Prot  | Free |
| Protected Phases       | 1     | 6    | 2     |      | 4     |      |
| Permitted Phases       |       |      |       | Free |       | Free |
| Actuated Green, G (s)  | 37.1  | 57.1 | 14.5  | 90.6 | 22.0  | 90.6 |
| Effective Green, g (s) | 37.1  | 57.1 | 14.5  | 90.6 | 22.0  | 90.6 |
| Actuated g/C Ratio     | 0.41  | 0.63 | 0.16  | 1.00 | 0.24  | 1.00 |
| Clearance Time (s)     | 5.5   | 6.0  | 6.0   |      | 5.5   |      |
| Vehicle Extension (s)  | 4.0   | 4.5  | 4.5   |      | 4.5   |      |
| Lane Grp Cap (vph)     | 703   | 2252 | 566   | 1553 | 841   | 1524 |
| v/s Ratio Prot         | c0.25 | 0.13 | c0.06 |      | c0.16 |      |
| v/s Ratio Perm         |       |      |       | 0.18 |       | 0.21 |
| v/c Ratio              | 0.60  | 0.21 | 0.40  | 0.18 | 0.66  | 0.21 |
| Uniform Delay, d1      | 21.0  | 7.1  | 34.2  | 0.0  | 30.9  | 0.0  |
| Progression Factor     | 1.00  | 1.00 | 1.00  | 1.00 | 1.00  | 1.00 |
| Incremental Delay, d2  | 1.7   | 0.1  | 0.8   | 0.3  | 2.3   | 0.3  |
| Delay (s)              | 22.6  | 7.2  | 35.0  | 0.3  | 33.2  | 0.3  |
| Level of Service       | C     | A    | C     | A    | C     | A    |
| Approach Delay (s)     |       | 14.5 | 15.9  |      | 21.2  |      |
| Approach LOS           |       | B    | B     |      | C     |      |





















### Intersection Summary

|                                   |       |                           |      |
|-----------------------------------|-------|---------------------------|------|
| HCM 2000 Control Delay            | 17.4  | HCM 2000 Level of Service | B    |
| HCM 2000 Volume to Capacity ratio | 0.58  |                           |      |
| Actuated Cycle Length (s)         | 90.6  | Sum of lost time (s)      | 17.0 |
| Intersection Capacity Utilization | 59.9% | ICU Level of Service      | B    |
| Analysis Period (min)             | 15    |                           |      |
| c Critical Lane Group             |       |                           |      |

# HCM Signalized Intersection Capacity Analysis

## 6: Kroger/The Rocks & Route 11

12/8/2015

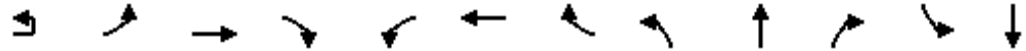
|                                   |  |  |  |  |  |  |  |  |  |  |  |  |
|-----------------------------------|---|---|---|---|---|---|---|---|---|---|---|---|
| Movement                          | EBL   | EBT   | EBR   | WBL   | WBT   | WBR   | NBL   | NBT   | NBR   | SBL   | SBT   | SBR   |
| Lane Configurations               |  |  |   |  |  |   |  |  |   |   |  |  |
| Traffic Volume (vph)              | 25  | 757   | 106   | 47  | 545   | 3   | 58  | 2   | 54  | 34  | 3   | 45  |
| Future Volume (vph)               | 25  | 757   | 106   | 47  | 545   | 3   | 58  | 2   | 54  | 34  | 3   | 45  |
| Ideal Flow (vphpl)                | 1900  | 1900  | 1900  | 1900  | 1900  | 1900  | 1900  | 1900  | 1900  | 1900  | 1900  | 1900  |
| Total Lost time (s)               | 5.5   | 6.0   |   | 5.5   | 6.0   |   | 5.5   | 5.5   |   |   | 5.5   | 5.5   |
| Lane Util. Factor                 | 1.00  | 0.95  |   | 1.00  | 0.95  |   | 1.00  | 1.00  |   |   | 1.00  | 1.00  |
| Frt                               | 1.00  | 0.98  |   | 1.00  | 1.00  |   | 1.00  | 0.85  |   |   | 1.00  | 0.85  |
| Flt Protected                     | 0.95  | 1.00  |   | 0.95  | 1.00  |   | 0.95  | 1.00  |   |   | 0.96  | 1.00  |
| Satd. Flow (prot)                 | 1736  | 3470  |   | 1656  | 3537  |   | 1719  | 1569  |   |   | 1817  | 1583  |
| Flt Permitted                     | 0.95  | 1.00  |   | 0.95  | 1.00  |   | 0.73  | 1.00  |   |   | 0.70  | 1.00  |
| Satd. Flow (perm)                 | 1736  | 3470  |   | 1656  | 3537  |   | 1318  | 1569  |   |   | 1328  | 1583  |
| Peak-hour factor, PHF             | 0.88  | 0.88  | 0.88  | 0.93  | 0.93  | 0.93  | 0.86  | 0.86  | 0.86  | 0.85  | 0.85  | 0.85  |
| Adj. Flow (vph)                   | 28  | 860   | 120   | 51  | 586   | 3   | 67  | 2   | 63  | 40  | 4   | 53  |
| RTOR Reduction (vph)              | 0   | 8   | 0   | 0   | 0   | 0   | 0   | 55  | 0   | 0   | 0   | 46  |
| Lane Group Flow (vph)             | 28  | 972   | 0   | 51  | 589   | 0   | 67  | 10  | 0   | 0   | 44  | 7   |
| Heavy Vehicles (%)                | 4%  | 2%  | 3%  | 9%  | 2%  | 0%  | 5%  | 50%   | 2%  | 0%  | 0%  | 2%  |
| Turn Type                         | Prot  | NA  |   | Prot  | NA  |   | Perm  | NA  |   | Perm  | NA  | Perm  |
| Protected Phases                  | 1   | 6   |   | 5   | 2   |   |   | 8   |   |   | 4   |   |
| Permitted Phases                  |   |   |   |   |   |   | 8   |   |   | 4   |   | 4   |
| Actuated Green, G (s)             | 2.7   | 33.2  |   | 4.8   | 35.3  |   | 8.4   | 8.4   |   |   | 8.4   | 8.4   |
| Effective Green, g (s)            | 2.7   | 33.2  |   | 4.8   | 35.3  |   | 8.4   | 8.4   |   |   | 8.4   | 8.4   |
| Actuated g/C Ratio                | 0.04  | 0.52  |   | 0.08  | 0.56  |   | 0.13  | 0.13  |   |   | 0.13  | 0.13  |
| Clearance Time (s)                | 5.5   | 6.0   |   | 5.5   | 6.0   |   | 5.5   | 5.5   |   |   | 5.5   | 5.5   |
| Vehicle Extension (s)             | 3.5   | 4.0   |   | 3.5   | 4.0   |   | 4.0   | 4.0   |   |   | 4.0   | 4.0   |
| Lane Grp Cap (vph)                | 73  | 1817  |   | 125   | 1969  |   | 174   | 207   |   |   | 175   | 209   |
| v/s Ratio Prot                    | 0.02  | c0.28   |   | c0.03   | 0.17  |   |   | 0.01  |   |   |   |   |
| v/s Ratio Perm                    |   |   |   |   |   |   | c0.05   |   |   |   | 0.03  | 0.00  |
| v/c Ratio                         | 0.38  | 0.54  |   | 0.41  | 0.30  |   | 0.39  | 0.05  |   |   | 0.25  | 0.03  |
| Uniform Delay, d1                 | 29.5  | 10.0  |   | 27.9  | 7.5   |   | 25.1  | 24.0  |   |   | 24.7  | 24.0  |
| Progression Factor                | 1.00  | 1.00  |   | 1.00  | 1.00  |   | 1.00  | 1.00  |   |   | 1.00  | 1.00  |
| Incremental Delay, d2             | 3.9   | 0.4   |   | 2.6   | 0.1   |   | 1.9   | 0.1   |   |   | 1.0   | 0.1   |
| Delay (s)                         | 33.5  | 10.4  |   | 30.5  | 7.6   |   | 27.1  | 24.2  |   |   | 25.7  | 24.1  |
| Level of Service                  | C   | B   |   | C   | A   |   | C   | C   |   |   | C   | C   |
| Approach Delay (s)                |   | 11.0  |   |   | 9.4   |   |   | 25.6  |   |   | 24.8  |   |
| Approach LOS                      |   | B   |   |   | A   |   |   | C   |   |   | C   |   |
| <b>Intersection Summary</b>       |   |   |   |   |   |   |   |   |   |   |   |   |
| HCM 2000 Control Delay            |   |   | 12.2  |   |   |   | HCM 2000 Level of Service   |   |   | B   |   |   |
| HCM 2000 Volume to Capacity ratio |   |   | 0.49  |   |   |   |   |   |   |   |   |   |
| Actuated Cycle Length (s)         |   |   | 63.4  |   |   |   | Sum of lost time (s)  |   | 17.0  |   |   |   |
| Intersection Capacity Utilization |   |   | 53.3%   |   |   |   | ICU Level of Service  |   | A   |   |   |   |
| Analysis Period (min)             |   |   | 15  |   |   |   |   |   |   |   |   |   |
| c Critical Lane Group             |   |   |   |   |   |   |   |   |   |   |   |   |



# HCM Unsignalized Intersection Capacity Analysis

## 3: Cherry Ct/Driveway & Route 11

12/8/2015



| Movement               | EBU  | EBL  | EBT  | EBR  | WBL  | WBT   | WBR  | NBL  | NBT  | NBR  | SBL  | SBT  |
|------------------------|------|------|------|------|------|-------|------|------|------|------|------|------|
| Lane Configurations    |      |      |      |      |      |       |      |      |      |      |      |      |
| Traffic Volume (veh/h) | 3    | 3    | 888  | 1    | 0    | 463   | 0    | 3    | 0    | 3    | 0    | 0    |
| Future Volume (Veh/h)  | 3    | 3    | 888  | 1    | 0    | 463   | 0    | 3    | 0    | 3    | 0    | 0    |
| Sign Control           |      |      | Free |      |      | Free  |      |      | Stop |      |      | Stop |
| Grade                  |      |      | 0%   |      |      | 0%    |      |      | 0%   |      |      | 0%   |
| Peak Hour Factor       | 0.87 | 0.87 | 0.87 | 0.87 | 0.92 | 0.92  | 0.92 | 0.30 | 0.30 | 0.30 | 0.38 | 0.38 |
| Hourly flow rate (vph) | 0    | 3    | 1021 | 1    | 0    | 503   | 0    | 10   | 0    | 10   | 0    | 0    |
| Pedestrians            |      |      |      |      |      |       |      |      |      |      |      |      |
| Lane Width (ft)        |      |      |      |      |      |       |      |      |      |      |      |      |
| Walking Speed (ft/s)   |      |      |      |      |      |       |      |      |      |      |      |      |
| Percent Blockage       |      |      |      |      |      |       |      |      |      |      |      |      |
| Right turn flare (veh) |      |      |      |      |      |       |      |      |      |      |      |      |
| Median type            |      |      | None |      |      | TWLTL |      |      |      |      |      |      |
| Median storage (veh)   |      |      |      |      |      | 2     |      |      |      |      |      |      |
| Upstream signal (ft)   |      |      | 779  |      |      |       |      |      |      |      |      |      |
| pX, platoon unblocked  | 0.00 |      |      |      | 0.96 |       |      | 0.96 | 0.96 | 0.96 | 0.96 | 0.96 |
| vC, conflicting volume | 0    | 503  |      |      | 1022 |       |      | 1287 | 1530 | 511  | 1030 | 1531 |
| vC1, stage 1 conf vol  |      |      |      |      |      |       |      | 1028 | 1028 |      | 503  | 503  |
| vC2, stage 2 conf vol  |      |      |      |      |      |       |      | 260  | 503  |      | 526  | 1028 |
| vCu, unblocked vol     | 0    | 503  |      |      | 950  |       |      | 1224 | 1477 | 420  | 958  | 1477 |
| tC, single (s)         | 0.0  | 4.1  |      |      | 4.1  |       |      | 7.5  | 6.5  | 6.9  | 7.5  | 6.5  |
| tC, 2 stage (s)        |      |      |      |      |      |       |      | 6.5  | 5.5  |      | 6.5  | 5.5  |
| tF (s)                 | 0.0  | 2.2  |      |      | 2.2  |       |      | 3.5  | 4.0  | 3.3  | 3.5  | 4.0  |
| p0 queue free %        | 0    | 100  |      |      | 100  |       |      | 96   | 100  | 98   | 100  | 100  |
| cM capacity (veh/h)    | 0    | 1072 |      |      | 705  |       |      | 258  | 291  | 567  | 413  | 291  |

| Direction, Lane #      | EB 1 | EB 2 | WB 1 | WB 2 | WB 3 | NB 1 | SB 1 |
|------------------------|------|------|------|------|------|------|------|
| Volume Total           | 514  | 512  | 0    | 335  | 168  | 20   | 8    |
| Volume Left            | 3    | 0    | 0    | 0    | 0    | 10   | 0    |
| Volume Right           | 0    | 1    | 0    | 0    | 0    | 10   | 8    |
| cSH                    | 1072 | 1700 | 1700 | 1700 | 1700 | 355  | 754  |
| Volume to Capacity     | 0.00 | 0.30 | 0.00 | 0.20 | 0.10 | 0.06 | 0.01 |
| Queue Length 95th (ft) | 0    | 0    | 0    | 0    | 0    | 4    | 1    |
| Control Delay (s)      | 0.1  | 0.0  | 0.0  | 0.0  | 0.0  | 15.8 | 9.8  |
| Lane LOS               | A    |      |      |      |      | C    | A    |
| Approach Delay (s)     | 0.0  |      | 0.0  |      |      | 15.8 | 9.8  |
| Approach LOS           |      |      |      |      |      | C    | A    |

### Intersection Summary

|                                   |       |
|-----------------------------------|-------|
| Average Delay                     | 0.3   |
| Intersection Capacity Utilization | 38.8% |
| ICU Level of Service              | A     |
| Analysis Period (min)             | 15    |

# HCM Unsignalized Intersection Capacity Analysis

## 3: Cherry Ct/Driveway & Route 11



















12/8/2015

| Movement               | SBR  |
|------------------------|------|
| Lane Configurations    |      |
| Traffic Volume (veh/h) | 3    |
| Future Volume (Veh/h)  | 3    |
| Sign Control           |      |
| Grade                  |      |
| Peak Hour Factor       | 0.38 |
| Hourly flow rate (vph) | 8    |
| Pedestrians            |      |
| Lane Width (ft)        |      |
| Walking Speed (ft/s)   |      |
| Percent Blockage       |      |
| Right turn flare (veh) |      |
| Median type            |      |
| Median storage (veh)   |      |
| Upstream signal (ft)   |      |
| pX, platoon unblocked  |      |
| vC, conflicting volume | 252  |
| vC1, stage 1 conf vol  |      |
| vC2, stage 2 conf vol  |      |
| vCu, unblocked vol     | 252  |
| tC, single (s)         | 6.9  |
| tC, 2 stage (s)        |      |
| tF (s)                 | 3.3  |
| p0 queue free %        | 99   |
| cM capacity (veh/h)    | 754  |
| Direction, Lane #      |      |

# HCM Unsignalized Intersection Capacity Analysis

## 4: Route 11 & Belspring Rd

12/8/2015

|                                   |  |  |  |  |  |  |  |  |  |  |  |  |
|-----------------------------------|---|---|---|---|---|---|--|---|---|---|---|---|
| Movement                          | EBL   | EBT   | EBR   | WBL   | WBT   | WBR   | NBL  | NBT   | NBR   | SBL   | SBT   | SBR   |
| Lane Configurations               |  |  |   |  |  |   |  |  |   |  |   |   |
| Traffic Volume (veh/h)            | 2   | 883   | 0   | 0   | 460   | 187   | 0  | 0   | 0   | 12  | 0   | 3   |
| Future Volume (Veh/h)             | 2   | 883   | 0   | 0   | 460   | 187   | 0  | 0   | 0   | 12  | 0   | 3   |
| Sign Control                      |   | Free  |   |   | Free  |   |  | Stop  |   |   | Stop  |   |
| Grade                             |   | 0%  |   |   | 0%  |   |  | 0%  |   |   | 0%  |   |
| Peak Hour Factor                  | 0.90  | 0.90  | 0.90  | 0.95  | 0.95  | 0.95  | 1.00   | 1.00  | 1.00  | 0.75  | 0.75  | 0.75  |
| Hourly flow rate (vph)            | 2   | 981   | 0   | 0   | 484   | 197   | 0  | 0   | 0   | 16  | 0   | 4   |
| Pedestrians                       |   |   |   |   |   |   |  |   |   |   |   |   |
| Lane Width (ft)                   |   |   |   |   |   |   |  |   |   |   |   |   |
| Walking Speed (ft/s)              |   |   |   |   |   |   |  |   |   |   |   |   |
| Percent Blockage                  |   |   |   |   |   |   |  |   |   |   |   |   |
| Right turn flare (veh)            |   |   |   |   |   |   |  |   |   |   |   |   |
| Median type                       |   | TWLTL   |   |   | TWLTL   |   |  |   |   |   |   |   |
| Median storage (veh)              |   | 2   |   |   | 2   |   |  |   |   |   |   |   |
| Upstream signal (ft)              |   |   |   |   | 842   |   |  |   |   |   |   |   |
| pX, platoon unblocked             | 0.95  |   |   |   |   |   | 0.95   | 0.95  |   | 0.95  | 0.95  | 0.95  |
| vC, conflicting volume            | 681   |   |   | 981   |   |   | 1231   | 1666  | 490   | 1077  | 1568  | 340   |
| vC1, stage 1 conf vol             |   |   |   |   |   |   | 985  | 985   |   | 582   | 582   |   |
| vC2, stage 2 conf vol             |   |   |   |   |   |   | 246  | 681   |   | 494   | 985   |   |
| vCu, unblocked vol                | 570   |   |   | 981   |   |   | 1146   | 1602  | 490   | 985   | 1499  | 213   |
| tC, single (s)                    | 4.1   |   |   | 4.1   |   |   | 7.5  | 6.5   | 6.9   | 7.5   | 6.5   | 6.9   |
| tC, 2 stage (s)                   |   |   |   |   |   |   | 6.5  | 5.5   |   | 6.5   | 5.5   |   |
| tF (s)                            | 2.2   |   |   | 2.2   |   |   | 3.5  | 4.0   | 3.3   | 3.5   | 4.0   | 3.3   |
| p0 queue free %                   | 100   |   |   | 100   |   |   | 100  | 100   | 100   | 96  | 100   | 99  |
| cM capacity (veh/h)               | 966   |   |   | 712   |   |   | 260  | 280   | 529   | 407   | 291   | 761   |
| <b>Direction, Lane #</b>          | <b>EB 1</b>   | <b>EB 2</b>   | <b>EB 3</b>   | <b>WB 1</b>   | <b>WB 2</b>   | <b>WB 3</b>   | <b>NB 1</b>  | <b>SB 1</b>   |   |   |   |   |
| Volume Total                      | 2   | 654   | 327   | 0   | 323   | 358   | 0  | 20  |   |   |   |   |
| Volume Left                       | 2   | 0   | 0   | 0   | 0   | 0   | 0  | 16  |   |   |   |   |
| Volume Right                      | 0   | 0   | 0   | 0   | 0   | 197   | 0  | 4   |   |   |   |   |
| cSH                               | 966   | 1700  | 1700  | 1700  | 1700  | 1700  | 1700   | 449   |   |   |   |   |
| Volume to Capacity                | 0.00  | 0.38  | 0.19  | 0.00  | 0.19  | 0.21  | 0.00   | 0.04  |   |   |   |   |
| Queue Length 95th (ft)            | 0   | 0   | 0   | 0   | 0   | 0   | 0  | 3   |   |   |   |   |
| Control Delay (s)                 | 8.7   | 0.0   | 0.0   | 0.0   | 0.0   | 0.0   | 0.0  | 13.4  |   |   |   |   |
| Lane LOS                          | A   |   |   |   |   |   | A  | B   |   |   |   |   |
| Approach Delay (s)                | 0.0   |   |   | 0.0   |   |   | 0.0  | 13.4  |   |   |   |   |
| Approach LOS                      |   |   |   |   |   |   | A  | B   |   |   |   |   |
| <b>Intersection Summary</b>       |   |   |   |   |   |   |  |   |   |   |   |   |
| Average Delay                     |   |   | 0.2   |   |   |   |  |   |   |   |   |   |
| Intersection Capacity Utilization |   |   | Err%  |   | ICU Level of Service  |   |  | H   |   |   |   |   |
| Analysis Period (min)             |   |   | 15  |   |   |   |  |   |   |   |   |   |

# HCM Unsignalized Intersection Capacity Analysis

## 5: Driveway/Nicewander Way & Route 11

12/8/2015



| Movement               | EBL  | EBT  | EBR  | WBL  | WBT  | WBR  | NBL  | NBT  | NBR  | SBL  | SBT  | SBR  |
|------------------------|------|------|------|------|------|------|------|------|------|------|------|------|
| Lane Configurations    | ↗    | ↕    |      | ↖    | ↕    |      |      | ↕    |      |      | ↕    | ↘    |
| Traffic Volume (veh/h) | 4    | 885  | 6    | 1    | 639  | 1    | 0    | 0    | 1    | 7    | 0    | 8    |
| Future Volume (Veh/h)  | 4    | 885  | 6    | 1    | 639  | 1    | 0    | 0    | 1    | 7    | 0    | 8    |
| Sign Control           | Free |      |      | Free |      |      | Stop |      |      | Stop |      |      |
| Grade                  | 0%   |      |      | 0%   |      |      | 0%   |      |      | 0%   |      |      |
| Peak Hour Factor       | 0.90 | 0.90 | 0.90 | 0.95 | 0.95 | 0.95 | 0.25 | 0.25 | 0.25 | 0.63 | 0.63 | 0.63 |
| Hourly flow rate (vph) | 4    | 983  | 7    | 1    | 673  | 1    | 0    | 0    | 4    | 11   | 0    | 13   |
| Pedestrians            |      |      |      |      |      |      |      |      |      |      |      |      |
| Lane Width (ft)        |      |      |      |      |      |      |      |      |      |      |      |      |
| Walking Speed (ft/s)   |      |      |      |      |      |      |      |      |      |      |      |      |
| Percent Blockage       |      |      |      |      |      |      |      |      |      |      |      |      |
| Right turn flare (veh) |      |      |      |      |      |      |      |      |      |      |      |      |
| Median type            |      |      |      |      |      |      |      |      |      |      |      |      |
| Median storage veh     |      |      |      |      |      |      |      |      |      |      |      |      |
| Upstream signal (ft)   |      |      |      |      |      |      |      |      |      |      |      |      |
| pX, platoon unblocked  | 0.94 |      |      |      |      |      | 0.94 |      |      | 0.94 |      |      |
| vC, conflicting volume | 674  |      |      | 990  |      |      | 1346 |      |      | 1670 |      |      |
| vC1, stage 1 conf vol  |      |      |      |      |      |      | 994  |      |      | 994  |      |      |
| vC2, stage 2 conf vol  |      |      |      |      |      |      | 352  |      |      | 676  |      |      |
| vCu, unblocked vol     | 530  |      |      | 990  |      |      | 1244 |      |      | 1588 |      |      |
| tC, single (s)         | 4.1  |      |      | 4.1  |      |      | 7.5  |      |      | 6.5  |      |      |
| tC, 2 stage (s)        |      |      |      |      |      |      | 6.5  |      |      | 5.5  |      |      |
| tF (s)                 | 2.2  |      |      | 2.2  |      |      | 3.5  |      |      | 4.0  |      |      |
| p0 queue free %        | 100  |      |      | 100  |      |      | 100  |      |      | 99   |      |      |
| cM capacity (veh/h)    | 986  |      |      | 706  |      |      | 253  |      |      | 279  |      |      |

| Direction, Lane #      | EB 1 | EB 2 | EB 3 | WB 1 | WB 2 | WB 3 | NB 1 | SB 1 |
|------------------------|------|------|------|------|------|------|------|------|
| Volume Total           | 4    | 655  | 335  | 1    | 449  | 225  | 4    | 24   |
| Volume Left            | 4    | 0    | 0    | 1    | 0    | 0    | 0    | 11   |
| Volume Right           | 0    | 0    | 7    | 0    | 0    | 1    | 4    | 13   |
| cSH                    | 986  | 1700 | 1700 | 706  | 1700 | 1700 | 326  | 526  |
| Volume to Capacity     | 0.00 | 0.39 | 0.20 | 0.00 | 0.26 | 0.13 | 0.01 | 0.05 |
| Queue Length 95th (ft) | 0    | 0    | 0    | 0    | 0    | 0    | 1    | 4    |
| Control Delay (s)      | 8.7  | 0.0  | 0.0  | 10.1 | 0.0  | 0.0  | 16.2 | 12.2 |
| Lane LOS               | A    |      |      | B    |      |      | C    |      |
| Approach Delay (s)     | 0.0  |      |      | 0.0  |      |      | 16.2 |      |
| Approach LOS           |      |      |      |      |      |      | C    |      |

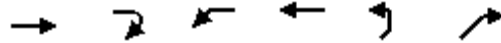
### Intersection Summary

|                                   |       |                      |
|-----------------------------------|-------|----------------------|
| Average Delay                     | 0.2   |                      |
| Intersection Capacity Utilization | 38.6% | ICU Level of Service |
| Analysis Period (min)             | 15    |                      |
|                                   | A     |                      |

# HCM Unsignalized Intersection Capacity Analysis

## 7: Kroger Rear & Route 11

12/8/2015



| Movement                          | EBT   | EBR  | WBL   | WBT  | NEL                  | NER  |
|-----------------------------------|-------|------|-------|------|----------------------|------|
| Lane Configurations               | ↑↑    |      | ↙     | ↑↑   | ↘                    |      |
| Traffic Volume (veh/h)            | 835   | 5    | 41    | 647  | 3                    | 53   |
| Future Volume (Veh/h)             | 835   | 5    | 41    | 647  | 3                    | 53   |
| Sign Control                      | Free  |      |       | Free | Stop                 |      |
| Grade                             | 0%    |      |       | 0%   | 0%                   |      |
| Peak Hour Factor                  | 0.80  | 0.88 | 0.92  | 0.92 | 0.74                 | 0.74 |
| Hourly flow rate (vph)            | 1044  | 6    | 45    | 703  | 4                    | 72   |
| Pedestrians                       |       |      |       |      |                      |      |
| Lane Width (ft)                   |       |      |       |      |                      |      |
| Walking Speed (ft/s)              |       |      |       |      |                      |      |
| Percent Blockage                  |       |      |       |      |                      |      |
| Right turn flare (veh)            |       |      |       |      |                      |      |
| Median type                       | TWLTL |      |       | None |                      |      |
| Median storage veh)               | 2     |      |       |      |                      |      |
| Upstream signal (ft)              | 254   |      |       |      |                      |      |
| pX, platoon unblocked             |       |      | 0.82  |      | 0.82                 | 0.82 |
| vC, conflicting volume            |       |      | 1050  |      | 1488                 | 525  |
| vC1, stage 1 conf vol             |       |      |       |      | 1047                 |      |
| vC2, stage 2 conf vol             |       |      |       |      | 442                  |      |
| vCu, unblocked vol                |       |      | 630   |      | 1163                 | 0    |
| tC, single (s)                    |       |      | 4.1   |      | 7.5                  | 6.9  |
| tC, 2 stage (s)                   |       |      |       |      | 6.5                  |      |
| tF (s)                            |       |      | 2.2   |      | 3.8                  | 3.3  |
| p0 queue free %                   |       |      | 94    |      | 99                   | 92   |
| cM capacity (veh/h)               |       |      | 780   |      | 295                  | 898  |
| Direction, Lane #                 | EB 1  | EB 2 | WB 1  | WB 2 | WB 3                 | NE 1 |
| Volume Total                      | 696   | 354  | 45    | 352  | 352                  | 76   |
| Volume Left                       | 0     | 0    | 45    | 0    | 0                    | 4    |
| Volume Right                      | 0     | 6    | 0     | 0    | 0                    | 72   |
| cSH                               | 1700  | 1700 | 780   | 1700 | 1700                 | 811  |
| Volume to Capacity                | 0.41  | 0.21 | 0.06  | 0.21 | 0.21                 | 0.09 |
| Queue Length 95th (ft)            | 0     | 0    | 5     | 0    | 0                    | 8    |
| Control Delay (s)                 | 0.0   | 0.0  | 9.9   | 0.0  | 0.0                  | 9.9  |
| Lane LOS                          |       |      | A     |      |                      | A    |
| Approach Delay (s)                | 0.0   |      | 0.6   |      |                      | 9.9  |
| Approach LOS                      |       |      |       |      |                      | A    |
| <b>Intersection Summary</b>       |       |      |       |      |                      |      |
| Average Delay                     |       |      | 0.6   |      |                      |      |
| Intersection Capacity Utilization |       |      | 40.0% |      | ICU Level of Service | A    |
| Analysis Period (min)             |       |      | 15    |      |                      |      |

# HCM Unsignalized Intersection Capacity Analysis

## 8: Wheatland & Route 11

12/8/2015



| Movement                          | EBT  | EBR  | WBL   | WBT  | NBL                  | NBR  |
|-----------------------------------|------|------|-------|------|----------------------|------|
| Lane Configurations               | ↑↑   |      | ↙     | ↑↑   | ↘                    |      |
| Traffic Volume (veh/h)            | 883  | 5    | 1     | 685  | 3                    | 3    |
| Future Volume (Veh/h)             | 883  | 5    | 1     | 685  | 3                    | 3    |
| Sign Control                      | Free |      |       | Free | Stop                 |      |
| Grade                             | 0%   |      |       | 0%   | 0%                   |      |
| Peak Hour Factor                  | 0.88 | 0.88 | 0.92  | 0.92 | 0.75                 | 0.75 |
| Hourly flow rate (vph)            | 1003 | 6    | 1     | 745  | 4                    | 4    |
| Pedestrians                       |      |      |       |      |                      |      |
| Lane Width (ft)                   |      |      |       |      |                      |      |
| Walking Speed (ft/s)              |      |      |       |      |                      |      |
| Percent Blockage                  |      |      |       |      |                      |      |
| Right turn flare (veh)            |      |      |       |      |                      |      |
| Median type                       | None |      | TWLTL |      |                      |      |
| Median storage (veh)              | 2    |      |       |      |                      |      |
| Upstream signal (ft)              | 335  |      |       |      |                      |      |
| pX, platoon unblocked             |      |      | 0.84  |      | 0.84                 | 0.84 |
| vC, conflicting volume            |      |      | 1009  |      | 1380                 | 504  |
| vC1, stage 1 conf vol             |      |      |       |      | 1006                 |      |
| vC2, stage 2 conf vol             |      |      |       |      | 374                  |      |
| vCu, unblocked vol                |      |      | 625   |      | 1068                 | 23   |
| tC, single (s)                    |      |      | 4.1   |      | 6.8                  | 7.6  |
| tC, 2 stage (s)                   |      |      |       |      | 5.8                  |      |
| tF (s)                            |      |      | 2.2   |      | 3.5                  | 3.6  |
| p0 queue free %                   |      |      | 100   |      | 99                   | 100  |
| cM capacity (veh/h)               |      |      | 810   |      | 380                  | 801  |
| Direction, Lane #                 | EB 1 | EB 2 | WB 1  | WB 2 | WB 3                 | NB 1 |
| Volume Total                      | 669  | 340  | 1     | 372  | 372                  | 8    |
| Volume Left                       | 0    | 0    | 1     | 0    | 0                    | 4    |
| Volume Right                      | 0    | 6    | 0     | 0    | 0                    | 4    |
| cSH                               | 1700 | 1700 | 810   | 1700 | 1700                 | 515  |
| Volume to Capacity                | 0.39 | 0.20 | 0.00  | 0.22 | 0.22                 | 0.02 |
| Queue Length 95th (ft)            | 0    | 0    | 0     | 0    | 0                    | 1    |
| Control Delay (s)                 | 0.0  | 0.0  | 9.5   | 0.0  | 0.0                  | 12.1 |
| Lane LOS                          | A    |      |       | B    |                      |      |
| Approach Delay (s)                | 0.0  |      | 0.0   |      | 12.1                 |      |
| Approach LOS                      |      |      |       | B    |                      |      |
| Intersection Summary              |      |      |       |      |                      |      |
| Average Delay                     |      |      | 0.1   |      |                      |      |
| Intersection Capacity Utilization |      |      | 34.6% |      | ICU Level of Service |      |
| Analysis Period (min)             |      |      | 15    |      |                      |      |
|                                   |      |      |       |      | A                    |      |

# HCM Unsignalized Intersection Capacity Analysis

## 9: University Park/Roca's & Route 11

12/8/2015



| Movement                          | EBL   | EBT  | EBR  | WBL   | WBT                  | WBR  | NBL  | NBT  | NBR  | SBL  | SBT  | SBR  |
|-----------------------------------|-------|------|------|-------|----------------------|------|------|------|------|------|------|------|
| Lane Configurations               | ↖     | ↕    |      | ↖     | ↕                    |      |      | ↕    |      |      | ↕    |      |
| Traffic Volume (veh/h)            | 1     | 881  | 9    | 9     | 695                  | 0    | 1    | 0    | 1    | 0    | 0    | 0    |
| Future Volume (Veh/h)             | 1     | 881  | 9    | 9     | 695                  | 0    | 1    | 0    | 1    | 0    | 0    | 0    |
| Sign Control                      | Free  |      |      | Free  |                      |      | Stop |      |      | Stop |      |      |
| Grade                             | 0%    |      |      | 0%    |                      |      | 0%   |      |      | 0%   |      |      |
| Peak Hour Factor                  | 0.90  | 0.90 | 0.90 | 0.98  | 0.98                 | 0.98 | 0.25 | 0.25 | 0.25 | 1.00 | 1.00 | 1.00 |
| Hourly flow rate (vph)            | 1     | 979  | 10   | 9     | 709                  | 0    | 4    | 0    | 4    | 0    | 0    | 0    |
| Pedestrians                       |       |      |      |       |                      |      |      |      |      |      |      |      |
| Lane Width (ft)                   |       |      |      |       |                      |      |      |      |      |      |      |      |
| Walking Speed (ft/s)              |       |      |      |       |                      |      |      |      |      |      |      |      |
| Percent Blockage                  |       |      |      |       |                      |      |      |      |      |      |      |      |
| Right turn flare (veh)            |       |      |      |       |                      |      |      |      |      |      |      |      |
| Median type                       | TWLTL |      |      | None  |                      |      |      |      |      |      |      |      |
| Median storage veh                | 2     |      |      |       |                      |      |      |      |      |      |      |      |
| Upstream signal (ft)              | 792   |      |      |       |                      |      |      |      |      |      |      |      |
| pX, platoon unblocked             |       |      |      | 0.88  |                      |      |      | 0.88 | 0.88 | 0.88 | 0.88 | 0.88 |
| vC, conflicting volume            | 709   |      |      | 989   |                      |      | 1358 | 1713 | 494  | 1222 | 1718 | 354  |
| vC1, stage 1 conf vol             |       |      |      |       |                      |      | 986  | 986  |      |      | 727  | 727  |
| vC2, stage 2 conf vol             |       |      |      |       |                      |      | 372  | 727  |      |      | 496  | 991  |
| vCu, unblocked vol                | 709   |      |      | 706   |                      |      | 1127 | 1532 | 142  | 972  | 1538 | 354  |
| tC, single (s)                    | 6.1   |      |      | 4.1   |                      |      | 7.5  | 6.5  | 6.9  | 7.5  | 6.5  | 6.9  |
| tC, 2 stage (s)                   |       |      |      |       |                      |      | 6.5  | 5.5  |      |      | 6.5  | 5.5  |
| tF (s)                            | 3.2   |      |      | 2.2   |                      |      | 3.5  | 4.0  | 3.3  | 3.5  | 4.0  | 3.3  |
| p0 queue free %                   | 100   |      |      | 99    |                      |      | 99   | 100  | 99   | 100  | 100  | 100  |
| cM capacity (veh/h)               | 456   |      |      | 790   |                      |      | 317  | 297  | 777  | 355  | 293  | 648  |
| Direction, Lane #                 | EB 1  | EB 2 | EB 3 | WB 1  | WB 2                 | WB 3 | NB 1 | SB 1 |      |      |      |      |
| Volume Total                      | 1     | 653  | 336  | 9     | 473                  | 236  | 8    | 0    |      |      |      |      |
| Volume Left                       | 1     | 0    | 0    | 9     | 0                    | 0    | 4    | 0    |      |      |      |      |
| Volume Right                      | 0     | 0    | 10   | 0     | 0                    | 0    | 4    | 0    |      |      |      |      |
| cSH                               | 456   | 1700 | 1700 | 790   | 1700                 | 1700 | 450  | 1700 |      |      |      |      |
| Volume to Capacity                | 0.00  | 0.38 | 0.20 | 0.01  | 0.28                 | 0.14 | 0.02 | 0.00 |      |      |      |      |
| Queue Length 95th (ft)            | 0     | 0    | 0    | 1     | 0                    | 0    | 1    | 0    |      |      |      |      |
| Control Delay (s)                 | 12.9  | 0.0  | 0.0  | 9.6   | 0.0                  | 0.0  | 13.1 | 0.0  |      |      |      |      |
| Lane LOS                          | B     |      |      | A     |                      |      | B    | A    |      |      |      |      |
| Approach Delay (s)                | 0.0   |      |      | 0.1   |                      |      | 13.1 | 0.0  |      |      |      |      |
| Approach LOS                      |       |      |      |       |                      |      | B    | A    |      |      |      |      |
| Intersection Summary              |       |      |      |       |                      |      |      |      |      |      |      |      |
| Average Delay                     |       |      |      | 0.1   |                      |      |      |      |      |      |      |      |
| Intersection Capacity Utilization |       |      |      | 34.6% | ICU Level of Service |      |      | A    |      |      |      |      |
| Analysis Period (min)             |       |      |      | 15    |                      |      |      |      |      |      |      |      |

# HCM Unsignalized Intersection Capacity Analysis

## 10: Route 11 & Nesslerod

12/8/2015



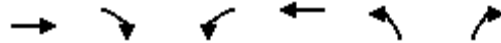
| Movement                          | EBL  | EBT    | WBT    | WBR  | SBL                  | SBR  |
|-----------------------------------|------|--------|--------|------|----------------------|------|
| Lane Configurations               |      |        |        |      |                      |      |
| Traffic Volume (veh/h)            | 1    | 881    | 704    | 0    | 1                    | 0    |
| Future Volume (Veh/h)             | 1    | 881    | 704    | 0    | 1                    | 0    |
| Sign Control                      |      | Free   | Free   |      | Stop                 |      |
| Grade                             |      | 0%     | 0%     |      | 0%                   |      |
| Peak Hour Factor                  | 0.90 | 0.90   | 0.98   | 0.98 | 0.25                 | 0.25 |
| Hourly flow rate (vph)            | 1    | 979    | 718    | 0    | 4                    | 0    |
| Pedestrians                       |      |        |        |      |                      |      |
| Lane Width (ft)                   |      |        |        |      |                      |      |
| Walking Speed (ft/s)              |      |        |        |      |                      |      |
| Percent Blockage                  |      |        |        |      |                      |      |
| Right turn flare (veh)            |      |        |        |      |                      |      |
| Median type                       |      | Raised | Raised |      |                      |      |
| Median storage (veh)              |      | 1      | 1      |      |                      |      |
| Upstream signal (ft)              |      | 1287   |        |      |                      |      |
| pX, platoon unblocked             |      |        |        |      | 0.93                 |      |
| vC, conflicting volume            | 718  |        |        |      | 1210                 | 359  |
| vC1, stage 1 conf vol             |      |        |        |      | 718                  |      |
| vC2, stage 2 conf vol             |      |        |        |      | 492                  |      |
| vCu, unblocked vol                | 718  |        |        |      | 1081                 | 359  |
| tC, single (s)                    | 4.1  |        |        |      | 6.8                  | 6.9  |
| tC, 2 stage (s)                   |      |        |        |      | 5.8                  |      |
| tF (s)                            | 2.2  |        |        |      | 3.5                  | 3.3  |
| p0 queue free %                   | 100  |        |        |      | 99                   | 100  |
| cM capacity (veh/h)               | 892  |        |        |      | 332                  | 643  |
| Direction, Lane #                 | EB 1 | EB 2   | EB 3   | WB 1 | WB 2                 | SB 1 |
| Volume Total                      | 1    | 490    | 490    | 479  | 239                  | 4    |
| Volume Left                       | 1    | 0      | 0      | 0    | 0                    | 4    |
| Volume Right                      | 0    | 0      | 0      | 0    | 0                    | 0    |
| cSH                               | 892  | 1700   | 1700   | 1700 | 1700                 | 332  |
| Volume to Capacity                | 0.00 | 0.29   | 0.29   | 0.28 | 0.14                 | 0.01 |
| Queue Length 95th (ft)            | 0    | 0      | 0      | 0    | 0                    | 1    |
| Control Delay (s)                 | 9.0  | 0.0    | 0.0    | 0.0  | 0.0                  | 16.0 |
| Lane LOS                          | A    |        |        |      |                      | C    |
| Approach Delay (s)                | 0.0  |        |        | 0.0  |                      | 16.0 |
| Approach LOS                      |      |        |        |      |                      | C    |
| Intersection Summary              |      |        |        |      |                      |      |
| Average Delay                     |      |        | 0.0    |      |                      |      |
| Intersection Capacity Utilization |      |        | 34.4%  |      | ICU Level of Service | A    |
| Analysis Period (min)             |      |        | 15     |      |                      |      |



# HCM Unsignalized Intersection Capacity Analysis

## 11: Hazel Hollow Rd & Route 11

12/8/2015



| Movement               | EBT    | EBR  | WBL    | WBT  | NBL  | NBR  |
|------------------------|--------|------|--------|------|------|------|
| Lane Configurations    | ↑↑     | ↑    | ↓      | ↑↑   | ↓    | ↓    |
| Traffic Volume (veh/h) | 878    | 4    | 12     | 683  | 21   | 28   |
| Future Volume (Veh/h)  | 878    | 4    | 12     | 683  | 21   | 28   |
| Sign Control           | Free   |      |        | Free | Stop |      |
| Grade                  | 0%     |      |        | 0%   | 0%   |      |
| Peak Hour Factor       | 0.90   | 0.90 | 0.98   | 0.98 | 0.82 | 0.82 |
| Hourly flow rate (vph) | 976    | 4    | 12     | 697  | 26   | 34   |
| Pedestrians            |        |      |        |      |      |      |
| Lane Width (ft)        |        |      |        |      |      |      |
| Walking Speed (ft/s)   |        |      |        |      |      |      |
| Percent Blockage       |        |      |        |      |      |      |
| Right turn flare (veh) |        |      |        |      |      |      |
| Median type            | Raised |      | Raised |      |      |      |
| Median storage veh     | 1      |      | 1      |      |      |      |
| Upstream signal (ft)   |        |      |        |      |      |      |
| pX, platoon unblocked  |        |      |        |      |      |      |
| vC, conflicting volume |        |      | 980    |      | 1348 | 488  |
| vC1, stage 1 conf vol  |        |      |        |      | 976  |      |
| vC2, stage 2 conf vol  |        |      |        |      | 372  |      |
| vCu, unblocked vol     |        |      | 980    |      | 1348 | 488  |
| tC, single (s)         |        |      | 4.4    |      | 6.8  | 6.9  |
| tC, 2 stage (s)        |        |      |        |      | 5.8  |      |
| tF (s)                 |        |      | 2.4    |      | 3.5  | 3.3  |
| p0 queue free %        |        |      | 98     |      | 90   | 94   |
| cM capacity (veh/h)    |        |      | 615    |      | 256  | 531  |

| Direction, Lane #      | EB 1 | EB 2 | EB 3 | WB 1 | WB 2 | WB 3 | NB 1 |
|------------------------|------|------|------|------|------|------|------|
| Volume Total           | 488  | 488  | 4    | 12   | 348  | 348  | 60   |
| Volume Left            | 0    | 0    | 0    | 12   | 0    | 0    | 26   |
| Volume Right           | 0    | 0    | 4    | 0    | 0    | 0    | 34   |
| cSH                    | 1700 | 1700 | 1700 | 615  | 1700 | 1700 | 362  |
| Volume to Capacity     | 0.29 | 0.29 | 0.00 | 0.02 | 0.20 | 0.20 | 0.17 |
| Queue Length 95th (ft) | 0    | 0    | 0    | 1    | 0    | 0    | 15   |
| Control Delay (s)      | 0.0  | 0.0  | 0.0  | 11.0 | 0.0  | 0.0  | 16.9 |
| Lane LOS               |      |      |      | B    | C    |      |      |
| Approach Delay (s)     | 0.0  |      |      | 0.2  |      |      | 16.9 |
| Approach LOS           |      |      |      |      |      |      | C    |

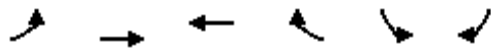
### Intersection Summary

|                                   |       |                        |
|-----------------------------------|-------|------------------------|
| Average Delay                     | 0.7   |                        |
| Intersection Capacity Utilization | 34.3% | ICU Level of Service A |
| Analysis Period (min)             | 15    |                        |

# HCM Signalized Intersection Capacity Analysis

## 2: Route 11 & Route 114

12/8/2015



| Movement               | EBL   | EBT  | WBT  | WBR   | SBL   | SBR  |
|------------------------|-------|------|------|-------|-------|------|
| Lane Configurations    |       |      |      |       |       |      |
| Traffic Volume (vph)   | 214   | 292  | 256  | 488   | 660   | 179  |
| Future Volume (vph)    | 214   | 292  | 256  | 488   | 660   | 179  |
| Ideal Flow (vphpl)     | 1900  | 1900 | 1900 | 1900  | 1900  | 1900 |
| Total Lost time (s)    | 5.5   | 6.0  | 6.0  | 4.0   | 5.5   | 4.0  |
| Lane Util. Factor      | 1.00  | 0.95 | 0.95 | 1.00  | 0.97  | 1.00 |
| Frt                    | 1.00  | 1.00 | 1.00 | 0.85  | 1.00  | 0.85 |
| Flt Protected          | 0.95  | 1.00 | 1.00 | 1.00  | 0.95  | 1.00 |
| Satd. Flow (prot)      | 1719  | 3505 | 3505 | 1583  | 3433  | 1509 |
| Flt Permitted          | 0.95  | 1.00 | 1.00 | 1.00  | 0.95  | 1.00 |
| Satd. Flow (perm)      | 1719  | 3505 | 3505 | 1583  | 3433  | 1509 |
| Peak-hour factor, PHF  | 0.81  | 0.81 | 0.95 | 0.95  | 0.86  | 0.86 |
| Adj. Flow (vph)        | 264   | 360  | 269  | 514   | 767   | 208  |
| RTOR Reduction (vph)   | 0     | 0    | 0    | 0     | 0     | 0    |
| Lane Group Flow (vph)  | 264   | 360  | 269  | 514   | 767   | 208  |
| Heavy Vehicles (%)     | 5%    | 3%   | 3%   | 2%    | 2%    | 7%   |
| Turn Type              | Prot  | NA   | NA   | Free  | Prot  | Free |
| Protected Phases       | 1     | 6    | 2    |       | 4     |      |
| Permitted Phases       |       |      |      | Free  |       | Free |
| Actuated Green, G (s)  | 20.6  | 42.0 | 15.9 | 81.9  | 28.4  | 81.9 |
| Effective Green, g (s) | 20.6  | 42.0 | 15.9 | 81.9  | 28.4  | 81.9 |
| Actuated g/C Ratio     | 0.25  | 0.51 | 0.19 | 1.00  | 0.35  | 1.00 |
| Clearance Time (s)     | 5.5   | 6.0  | 6.0  |       | 5.5   |      |
| Vehicle Extension (s)  | 4.0   | 4.5  | 4.5  |       | 4.5   |      |
| Lane Grp Cap (vph)     | 432   | 1797 | 680  | 1583  | 1190  | 1509 |
| v/s Ratio Prot         | c0.15 | 0.10 | 0.08 |       | c0.22 |      |
| v/s Ratio Perm         |       |      |      | c0.32 |       | 0.14 |
| v/c Ratio              | 0.61  | 0.20 | 0.40 | 0.32  | 0.64  | 0.14 |
| Uniform Delay, d1      | 27.1  | 10.8 | 28.8 | 0.0   | 22.5  | 0.0  |
| Progression Factor     | 1.00  | 1.00 | 1.00 | 1.00  | 1.00  | 1.00 |
| Incremental Delay, d2  | 2.9   | 0.1  | 0.7  | 0.5   | 1.5   | 0.2  |
| Delay (s)              | 30.0  | 10.9 | 29.5 | 0.5   | 24.0  | 0.2  |
| Level of Service       | C     | B    | C    | A     | C     | A    |
| Approach Delay (s)     |       | 19.0 | 10.5 |       | 18.9  |      |
| Approach LOS           |       | B    | B    |       | B     |      |

### Intersection Summary

|                                   |       |                           |      |
|-----------------------------------|-------|---------------------------|------|
| HCM 2000 Control Delay            | 16.2  | HCM 2000 Level of Service | B    |
| HCM 2000 Volume to Capacity ratio | 0.59  |                           |      |
| Actuated Cycle Length (s)         | 81.9  | Sum of lost time (s)      | 17.0 |
| Intersection Capacity Utilization | 56.5% | ICU Level of Service      | B    |
| Analysis Period (min)             | 15    |                           |      |
| c Critical Lane Group             |       |                           |      |

# HCM Signalized Intersection Capacity Analysis

## 6: Kroger/The Rocks & Route 11

12/8/2015



| Movement               | EBL  | EBT   | EBR  | WBL   | WBT   | WBR  | NBL   | NBT  | NBR  | SBL  | SBT  | SBR  |
|------------------------|------|-------|------|-------|-------|------|-------|------|------|------|------|------|
| Lane Configurations    |      |       |      |       |       |      |       |      |      |      |      |      |
| Traffic Volume (vph)   | 32   | 821   | 129  | 84    | 765   | 1    | 101   | 5    | 109  | 36   | 10   | 74   |
| Future Volume (vph)    | 32   | 821   | 129  | 84    | 765   | 1    | 101   | 5    | 109  | 36   | 10   | 74   |
| Ideal Flow (vphpl)     | 1900 | 1900  | 1900 | 1900  | 1900  | 1900 | 1900  | 1900 | 1900 | 1900 | 1900 | 1900 |
| Total Lost time (s)    | 5.5  | 6.0   |      | 5.5   | 6.0   |      | 5.5   | 5.5  |      |      | 5.5  | 5.5  |
| Lane Util. Factor      | 1.00 | 0.95  |      | 1.00  | 0.95  |      | 1.00  | 1.00 |      |      | 1.00 | 1.00 |
| Frt                    | 1.00 | 0.98  |      | 1.00  | 1.00  |      | 1.00  | 0.86 |      |      | 1.00 | 0.85 |
| Flt Protected          | 0.95 | 1.00  |      | 0.95  | 1.00  |      | 0.95  | 1.00 |      |      | 0.96 | 1.00 |
| Satd. Flow (prot)      | 1805 | 3467  |      | 1736  | 3535  |      | 1787  | 1567 |      |      | 1828 | 1599 |
| Flt Permitted          | 0.95 | 1.00  |      | 0.95  | 1.00  |      | 0.71  | 1.00 |      |      | 0.70 | 1.00 |
| Satd. Flow (perm)      | 1805 | 3467  |      | 1736  | 3535  |      | 1344  | 1567 |      |      | 1324 | 1599 |
| Peak-hour factor, PHF  | 0.95 | 0.95  | 0.95 | 0.90  | 0.90  | 0.90 | 0.94  | 0.94 | 0.94 | 0.71 | 0.71 | 0.71 |
| Adj. Flow (vph)        | 34   | 864   | 136  | 93    | 850   | 1    | 107   | 5    | 116  | 51   | 14   | 104  |
| RTOR Reduction (vph)   | 0    | 10    | 0    | 0     | 0     | 0    | 0     | 94   | 0    | 0    | 0    | 84   |
| Lane Group Flow (vph)  | 34   | 990   | 0    | 93    | 851   | 0    | 107   | 27   | 0    | 0    | 65   | 20   |
| Heavy Vehicles (%)     | 0%   | 2%    | 2%   | 4%    | 2%    | 100% | 1%    | 0%   | 4%   | 0%   | 0%   | 1%   |
| Turn Type              | Prot | NA    |      | Prot  | NA    |      | Perm  | NA   |      | Perm | NA   | Perm |
| Protected Phases       | 1    | 6     |      | 5     | 2     |      |       | 8    |      |      | 4    |      |
| Permitted Phases       |      |       |      |       |       |      | 8     |      |      | 4    |      | 4    |
| Actuated Green, G (s)  | 4.3  | 31.6  |      | 8.1   | 35.4  |      | 13.3  | 13.3 |      |      | 13.3 | 13.3 |
| Effective Green, g (s) | 4.3  | 31.6  |      | 8.1   | 35.4  |      | 13.3  | 13.3 |      |      | 13.3 | 13.3 |
| Actuated g/C Ratio     | 0.06 | 0.45  |      | 0.12  | 0.51  |      | 0.19  | 0.19 |      |      | 0.19 | 0.19 |
| Clearance Time (s)     | 5.5  | 6.0   |      | 5.5   | 6.0   |      | 5.5   | 5.5  |      |      | 5.5  | 5.5  |
| Vehicle Extension (s)  | 3.5  | 4.0   |      | 3.5   | 4.0   |      | 4.0   | 4.0  |      |      | 4.0  | 4.0  |
| Lane Grp Cap (vph)     | 110  | 1565  |      | 200   | 1787  |      | 255   | 297  |      |      | 251  | 303  |
| v/s Ratio Prot         | 0.02 | c0.29 |      | c0.05 | c0.24 |      |       | 0.02 |      |      |      |      |
| v/s Ratio Perm         |      |       |      |       |       |      | c0.08 |      |      |      | 0.05 | 0.01 |
| v/c Ratio              | 0.31 | 0.63  |      | 0.47  | 0.48  |      | 0.42  | 0.09 |      |      | 0.26 | 0.07 |
| Uniform Delay, d1      | 31.4 | 14.7  |      | 28.9  | 11.3  |      | 25.0  | 23.4 |      |      | 24.2 | 23.3 |
| Progression Factor     | 1.00 | 1.00  |      | 1.00  | 1.00  |      | 1.00  | 1.00 |      |      | 1.00 | 1.00 |
| Incremental Delay, d2  | 1.9  | 0.9   |      | 2.0   | 0.3   |      | 1.5   | 0.2  |      |      | 0.8  | 0.1  |
| Delay (s)              | 33.3 | 15.7  |      | 30.9  | 11.5  |      | 26.5  | 23.5 |      |      | 24.9 | 23.4 |
| Level of Service       | C    | B     |      | C     | B     |      | C     | C    |      |      | C    | C    |
| Approach Delay (s)     |      | 16.3  |      |       | 13.4  |      |       | 24.9 |      |      | 24.0 |      |
| Approach LOS           |      | B     |      |       | B     |      |       | C    |      |      | C    |      |

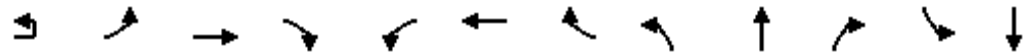
### Intersection Summary

|                                   |       |                           |      |
|-----------------------------------|-------|---------------------------|------|
| HCM 2000 Control Delay            | 16.5  | HCM 2000 Level of Service | B    |
| HCM 2000 Volume to Capacity ratio | 0.56  |                           |      |
| Actuated Cycle Length (s)         | 70.0  | Sum of lost time (s)      | 17.0 |
| Intersection Capacity Utilization | 58.2% | ICU Level of Service      | B    |
| Analysis Period (min)             | 15    |                           |      |
| c Critical Lane Group             |       |                           |      |

# HCM Unsignalized Intersection Capacity Analysis

## 3: Cherry Ct/Driveway & Route 11

12/8/2015



| Movement                          | EBU  | EBL  | EBT   | EBR  | WBL  | WBT                  | WBR  | NBL  | NBT  | NBR  | SBL  | SBT  |  |
|-----------------------------------|------|------|-------|------|------|----------------------|------|------|------|------|------|------|--|
| Lane Configurations               |      |      | ↔     |      | ↔    | ↔                    |      |      | ↔    |      |      | ↔    |  |
| Traffic Volume (veh/h)            | 1    | 5    | 943   | 3    | 1    | 742                  | 1    | 3    | 0    | 1    | 2    | 0    |  |
| Future Volume (Veh/h)             | 1    | 5    | 943   | 3    | 1    | 742                  | 1    | 3    | 0    | 1    | 2    | 0    |  |
| Sign Control                      |      |      | Free  |      |      | Free                 |      |      | Stop |      |      | Stop |  |
| Grade                             |      |      | 0%    |      |      | 0%                   |      |      | 0%   |      |      | 0%   |  |
| Peak Hour Factor                  | 0.93 | 0.93 | 0.93  | 0.93 | 0.93 | 0.93                 | 0.93 | 0.50 | 0.50 | 0.50 | 0.59 | 0.59 |  |
| Hourly flow rate (vph)            | 0    | 5    | 1014  | 3    | 1    | 798                  | 1    | 6    | 0    | 2    | 3    | 0    |  |
| Pedestrians                       |      |      |       |      |      |                      |      |      |      |      |      |      |  |
| Lane Width (ft)                   |      |      |       |      |      |                      |      |      |      |      |      |      |  |
| Walking Speed (ft/s)              |      |      |       |      |      |                      |      |      |      |      |      |      |  |
| Percent Blockage                  |      |      |       |      |      |                      |      |      |      |      |      |      |  |
| Right turn flare (veh)            |      |      |       |      |      |                      |      |      |      |      |      |      |  |
| Median type                       |      |      | None  |      |      | TWLTL                |      |      |      |      |      |      |  |
| Median storage (veh)              |      |      |       |      |      |                      |      |      |      |      |      | 2    |  |
| Upstream signal (ft)              |      |      | 779   |      |      |                      |      |      |      |      |      |      |  |
| pX, platoon unblocked             | 0.00 |      |       |      | 0.99 |                      |      | 0.99 | 0.99 | 0.99 | 0.99 | 0.99 |  |
| vC, conflicting volume            | 0    | 799  |       |      | 1017 |                      |      | 1456 | 1826 | 508  | 1320 | 1828 |  |
| vC1, stage 1 conf vol             |      |      |       |      |      |                      |      | 1026 | 1026 |      | 800  | 800  |  |
| vC2, stage 2 conf vol             |      |      |       |      |      |                      |      | 430  | 801  |      | 519  | 1027 |  |
| vCu, unblocked vol                | 0    | 799  |       |      | 990  |                      |      | 1435 | 1811 | 475  | 1297 | 1812 |  |
| tC, single (s)                    | 0.0  | 4.1  |       |      | 4.1  |                      |      | 7.5  | 6.5  | 6.9  | 7.5  | 6.5  |  |
| tC, 2 stage (s)                   |      |      |       |      |      |                      |      | 6.5  | 5.5  |      | 6.5  | 5.5  |  |
| tF (s)                            | 0.0  | 2.2  |       |      | 2.2  |                      |      | 3.5  | 4.0  | 3.3  | 3.5  | 4.0  |  |
| p0 queue free %                   | 0    | 99   |       |      | 100  |                      |      | 97   | 100  | 100  | 99   | 100  |  |
| cM capacity (veh/h)               | 0    | 833  |       |      | 697  |                      |      | 238  | 252  | 534  | 302  | 253  |  |
| Direction, Lane #                 | EB 1 | EB 2 | WB 1  | WB 2 | WB 3 | NB 1                 | SB 1 |      |      |      |      |      |  |
| Volume Total                      | 512  | 510  | 1     | 532  | 267  | 8                    | 32   |      |      |      |      |      |  |
| Volume Left                       | 5    | 0    | 1     | 0    | 0    | 6                    | 3    |      |      |      |      |      |  |
| Volume Right                      | 0    | 3    | 0     | 0    | 1    | 2                    | 29   |      |      |      |      |      |  |
| cSH                               | 833  | 1700 | 697   | 1700 | 1700 | 277                  | 541  |      |      |      |      |      |  |
| Volume to Capacity                | 0.01 | 0.30 | 0.00  | 0.31 | 0.16 | 0.03                 | 0.06 |      |      |      |      |      |  |
| Queue Length 95th (ft)            | 0    | 0    | 0     | 0    | 0    | 2                    | 5    |      |      |      |      |      |  |
| Control Delay (s)                 | 0.2  | 0.0  | 10.2  | 0.0  | 0.0  | 18.4                 | 12.1 |      |      |      |      |      |  |
| Lane LOS                          | A    |      | B     |      |      | C                    | B    |      |      |      |      |      |  |
| Approach Delay (s)                | 0.1  |      | 0.0   |      |      | 18.4                 | 12.1 |      |      |      |      |      |  |
| Approach LOS                      |      |      |       |      |      | C                    | B    |      |      |      |      |      |  |
| Intersection Summary              |      |      |       |      |      |                      |      |      |      |      |      |      |  |
| Average Delay                     |      |      | 0.3   |      |      |                      |      |      |      |      |      |      |  |
| Intersection Capacity Utilization |      |      | 40.4% |      |      | ICU Level of Service |      |      | A    |      |      |      |  |
| Analysis Period (min)             |      |      | 15    |      |      |                      |      |      |      |      |      |      |  |

# HCM Unsignalized Intersection Capacity Analysis

## 3: Cherry Ct/Driveway & Route 11

12/8/2015

| Movement               | SBR  |
|------------------------|------|
| Lane Configurations    |      |
| Traffic Volume (veh/h) | 17   |
| Future Volume (Veh/h)  | 17   |
| Sign Control           |      |
| Grade                  |      |
| Peak Hour Factor       | 0.59 |
| Hourly flow rate (vph) | 29   |
| Pedestrians            |      |
| Lane Width (ft)        |      |
| Walking Speed (ft/s)   |      |
| Percent Blockage       |      |
| Right turn flare (veh) |      |
| Median type            |      |
| Median storage (veh)   |      |
| Upstream signal (ft)   |      |
| pX, platoon unblocked  |      |
| vC, conflicting volume | 400  |
| vC1, stage 1 conf vol  |      |
| vC2, stage 2 conf vol  |      |
| vCu, unblocked vol     | 400  |
| tC, single (s)         | 7.0  |
| tC, 2 stage (s)        |      |
| tF (s)                 | 3.4  |
| p0 queue free %        | 95   |
| cM capacity (veh/h)    | 589  |
| Direction, Lane #      |      |

# HCM Unsignalized Intersection Capacity Analysis

## 4: Route 11 & Belspring Rd

12/8/2015

| Movement                          | EBL         | EBT         | EBR         | WBL         | WBT         | WBR         | NBL                  | NBT         | NBR  | SBL  | SBT  | SBR  |
|-----------------------------------|-------------|-------------|-------------|-------------|-------------|-------------|----------------------|-------------|------|------|------|------|
| Lane Configurations               |             |             |             |             |             |             |                      |             |      |      |      |      |
| Traffic Volume (veh/h)            | 7           | 969         | 1           | 1           | 773         | 178         | 0                    | 0           | 5    | 17   | 0    | 1    |
| Future Volume (Veh/h)             | 7           | 969         | 1           | 1           | 773         | 178         | 0                    | 0           | 5    | 17   | 0    | 1    |
| Sign Control                      | Free        |             |             | Free        |             |             | Stop                 |             |      | Stop |      |      |
| Grade                             | 0%          |             |             | 0%          |             |             | 0%                   |             |      | 0%   |      |      |
| Peak Hour Factor                  | 0.93        | 0.93        | 0.93        | 0.93        | 0.93        | 0.93        | 0.63                 | 0.63        | 0.63 | 0.65 | 0.65 | 0.65 |
| Hourly flow rate (vph)            | 8           | 1042        | 1           | 1           | 831         | 191         | 0                    | 0           | 8    | 26   | 0    | 2    |
| Pedestrians                       |             |             |             |             |             |             |                      |             |      |      |      |      |
| Lane Width (ft)                   |             |             |             |             |             |             |                      |             |      |      |      |      |
| Walking Speed (ft/s)              |             |             |             |             |             |             |                      |             |      |      |      |      |
| Percent Blockage                  |             |             |             |             |             |             |                      |             |      |      |      |      |
| Right turn flare (veh)            |             |             |             |             |             |             |                      |             |      |      |      |      |
| Median type                       | TWLTL       |             |             |             | TWLTL       |             |                      |             |      |      |      |      |
| Median storage (veh)              | 2           |             |             |             | 2           |             |                      |             |      |      |      |      |
| Upstream signal (ft)              |             |             |             |             | 842         |             |                      |             |      |      |      |      |
| pX, platoon unblocked             | 0.86        |             |             |             |             |             | 0.86                 |             |      | 0.86 |      |      |
| vC, conflicting volume            | 1022        |             |             | 1043        |             |             | 1478                 |             |      | 2082 |      |      |
| vC1, stage 1 conf vol             |             |             |             |             |             |             | 1058                 |             |      | 1058 |      |      |
| vC2, stage 2 conf vol             |             |             |             |             |             |             | 420                  |             |      | 1024 |      |      |
| vCu, unblocked vol                | 712         |             |             | 1043        |             |             | 1239                 |             |      | 1939 |      |      |
| tC, single (s)                    | 4.1         |             |             | 4.1         |             |             | 7.5                  |             |      | 6.5  |      |      |
| tC, 2 stage (s)                   |             |             |             |             |             |             | 6.5                  |             |      | 5.5  |      |      |
| tF (s)                            | 2.2         |             |             | 2.2         |             |             | 3.5                  |             |      | 4.0  |      |      |
| p0 queue free %                   | 99          |             |             | 100         |             |             | 100                  |             |      | 98   |      |      |
| cM capacity (veh/h)               | 776         |             |             | 675         |             |             | 232                  |             |      | 233  |      |      |
| <b>Direction, Lane #</b>          | <b>EB 1</b> | <b>EB 2</b> | <b>EB 3</b> | <b>WB 1</b> | <b>WB 2</b> | <b>WB 3</b> | <b>NB 1</b>          | <b>SB 1</b> |      |      |      |      |
| Volume Total                      | 8           | 695         | 348         | 1           | 554         | 468         | 8                    | 28          |      |      |      |      |
| Volume Left                       | 8           | 0           | 0           | 1           | 0           | 0           | 0                    | 26          |      |      |      |      |
| Volume Right                      | 0           | 0           | 1           | 0           | 0           | 191         | 8                    | 2           |      |      |      |      |
| cSH                               | 776         | 1700        | 1700        | 675         | 1700        | 1700        | 505                  | 321         |      |      |      |      |
| Volume to Capacity                | 0.01        | 0.41        | 0.20        | 0.00        | 0.33        | 0.28        | 0.02                 | 0.09        |      |      |      |      |
| Queue Length 95th (ft)            | 1           | 0           | 0           | 0           | 0           | 0           | 1                    | 7           |      |      |      |      |
| Control Delay (s)                 | 9.7         | 0.0         | 0.0         | 10.3        | 0.0         | 0.0         | 12.2                 | 17.3        |      |      |      |      |
| Lane LOS                          | A           |             |             | B           |             |             | B                    |             |      | C    |      |      |
| Approach Delay (s)                | 0.1         |             |             | 0.0         |             |             | 12.2                 |             |      | 17.3 |      |      |
| Approach LOS                      |             |             |             |             |             |             | B                    |             |      | C    |      |      |
| <b>Intersection Summary</b>       |             |             |             |             |             |             |                      |             |      |      |      |      |
| Average Delay                     |             |             |             | 0.3         |             |             |                      |             |      |      |      |      |
| Intersection Capacity Utilization |             |             |             | Err%        |             |             | ICU Level of Service |             |      | H    |      |      |
| Analysis Period (min)             |             |             |             | 15          |             |             |                      |             |      |      |      |      |

# HCM Unsignalized Intersection Capacity Analysis

## 5: Driveway/Nicewander Way & Route 11

12/8/2015

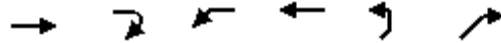


| Movement                          | EBL  | EBT   | EBR   | WBL  | WBT   | WBR  | NBL                  | NBT  | NBR  | SBL  | SBT  | SBR  |
|-----------------------------------|------|-------|-------|------|-------|------|----------------------|------|------|------|------|------|
| Lane Configurations               |      |       |       |      |       |      |                      |      |      |      |      |      |
| Traffic Volume (veh/h)            | 4    | 955   | 32    | 27   | 919   | 2    | 31                   | 0    | 28   | 2    | 0    | 2    |
| Future Volume (Veh/h)             | 4    | 955   | 32    | 27   | 919   | 2    | 31                   | 0    | 28   | 2    | 0    | 2    |
| Sign Control                      |      | Free  |       |      | Free  |      |                      | Stop |      |      | Stop |      |
| Grade                             |      | 0%    |       |      | 0%    |      |                      | 0%   |      |      | 0%   |      |
| Peak Hour Factor                  | 0.93 | 0.93  | 0.93  | 0.92 | 0.92  | 0.92 | 0.74                 | 0.74 | 0.74 | 0.50 | 0.50 | 0.50 |
| Hourly flow rate (vph)            | 4    | 1027  | 34    | 29   | 999   | 2    | 42                   | 0    | 38   | 4    | 0    | 4    |
| Pedestrians                       |      |       |       |      |       |      |                      |      |      |      |      |      |
| Lane Width (ft)                   |      |       |       |      |       |      |                      |      |      |      |      |      |
| Walking Speed (ft/s)              |      |       |       |      |       |      |                      |      |      |      |      |      |
| Percent Blockage                  |      |       |       |      |       |      |                      |      |      |      |      |      |
| Right turn flare (veh)            |      |       |       |      |       |      |                      |      |      |      |      |      |
| Median type                       |      | TWLTL |       |      | TWLTL |      |                      |      |      |      |      |      |
| Median storage (veh)              |      | 2     |       |      | 2     |      |                      |      |      |      |      |      |
| Upstream signal (ft)              |      |       |       |      | 710   |      |                      |      |      |      |      |      |
| pX, platoon unblocked             | 0.86 |       |       |      |       |      | 0.86                 | 0.86 |      | 0.86 | 0.86 | 0.86 |
| vC, conflicting volume            | 1001 |       |       | 1061 |       |      | 1614                 | 2111 | 530  | 1618 | 2127 | 500  |
| vC1, stage 1 conf vol             |      |       |       |      |       |      | 1052                 | 1052 |      | 1058 | 1058 |      |
| vC2, stage 2 conf vol             |      |       |       |      |       |      | 562                  | 1059 |      | 560  | 1069 |      |
| vCu, unblocked vol                | 663  |       |       | 1061 |       |      | 1379                 | 1961 | 530  | 1384 | 1979 | 78   |
| tC, single (s)                    | 4.1  |       |       | 4.2  |       |      | 7.6                  | 6.5  | 7.0  | 7.5  | 6.5  | 7.9  |
| tC, 2 stage (s)                   |      |       |       |      |       |      | 6.6                  | 5.5  |      | 6.5  | 5.5  |      |
| tF (s)                            | 2.2  |       |       | 2.2  |       |      | 3.5                  | 4.0  | 3.3  | 3.5  | 4.0  | 3.8  |
| p0 queue free %                   | 99   |       |       | 95   |       |      | 81                   | 100  | 92   | 98   | 100  | 99   |
| cM capacity (veh/h)               | 800  |       |       | 641  |       |      | 224                  | 228  | 491  | 256  | 213  | 711  |
| Direction, Lane #                 | EB 1 | EB 2  | EB 3  | WB 1 | WB 2  | WB 3 | NB 1                 | SB 1 |      |      |      |      |
| Volume Total                      | 4    | 685   | 376   | 29   | 666   | 335  | 80                   | 8    |      |      |      |      |
| Volume Left                       | 4    | 0     | 0     | 29   | 0     | 0    | 42                   | 4    |      |      |      |      |
| Volume Right                      | 0    | 0     | 34    | 0    | 0     | 2    | 38                   | 4    |      |      |      |      |
| cSH                               | 800  | 1700  | 1700  | 641  | 1700  | 1700 | 302                  | 376  |      |      |      |      |
| Volume to Capacity                | 0.01 | 0.40  | 0.22  | 0.05 | 0.39  | 0.20 | 0.26                 | 0.02 |      |      |      |      |
| Queue Length 95th (ft)            | 0    | 0     | 0     | 4    | 0     | 0    | 26                   | 2    |      |      |      |      |
| Control Delay (s)                 | 9.5  | 0.0   | 0.0   | 10.9 | 0.0   | 0.0  | 21.2                 | 14.8 |      |      |      |      |
| Lane LOS                          | A    |       |       | B    |       |      | C                    | B    |      |      |      |      |
| Approach Delay (s)                | 0.0  |       |       | 0.3  |       |      | 21.2                 | 14.8 |      |      |      |      |
| Approach LOS                      |      |       |       |      |       |      | C                    | B    |      |      |      |      |
| <b>Intersection Summary</b>       |      |       |       |      |       |      |                      |      |      |      |      |      |
| Average Delay                     |      |       |       | 1.0  |       |      |                      |      |      |      |      |      |
| Intersection Capacity Utilization |      |       | 38.5% |      |       |      | ICU Level of Service |      | A    |      |      |      |
| Analysis Period (min)             |      |       | 15    |      |       |      |                      |      |      |      |      |      |

# HCM Unsignalized Intersection Capacity Analysis

## 7: Kroger Rear & Route 11

12/8/2015



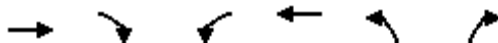
| Movement                          | EBT   | EBR  | WBL   | WBT                  | NEL  | NER  |
|-----------------------------------|-------|------|-------|----------------------|------|------|
| Lane Configurations               | ↑↑    |      | ↵     | ↑↑                   | ↵    |      |
| Traffic Volume (veh/h)            | 955   | 1    | 39    | 900                  | 0    | 56   |
| Future Volume (Veh/h)             | 955   | 1    | 39    | 900                  | 0    | 56   |
| Sign Control                      | Free  |      |       | Free                 | Stop |      |
| Grade                             | 0%    |      |       | 0%                   | 0%   |      |
| Peak Hour Factor                  | 0.95  | 0.95 | 0.90  | 0.90                 | 0.67 | 0.67 |
| Hourly flow rate (vph)            | 1005  | 1    | 43    | 1000                 | 0    | 84   |
| Pedestrians                       |       |      |       |                      |      |      |
| Lane Width (ft)                   |       |      |       |                      |      |      |
| Walking Speed (ft/s)              |       |      |       |                      |      |      |
| Percent Blockage                  |       |      |       |                      |      |      |
| Right turn flare (veh)            |       |      |       |                      |      |      |
| Median type                       | TWLTL |      | None  |                      |      |      |
| Median storage veh)               | 2     |      |       |                      |      |      |
| Upstream signal (ft)              | 254   |      |       |                      |      |      |
| pX, platoon unblocked             |       |      | 0.79  |                      | 0.79 | 0.79 |
| vC, conflicting volume            |       |      | 1006  |                      | 1592 | 503  |
| vC1, stage 1 conf vol             |       |      |       |                      | 1006 |      |
| vC2, stage 2 conf vol             |       |      |       |                      | 586  |      |
| vCu, unblocked vol                |       |      | 477   |                      | 1218 | 0    |
| tC, single (s)                    |       |      | 4.1   |                      | 6.8  | 6.9  |
| tC, 2 stage (s)                   |       |      |       |                      | 5.8  |      |
| tF (s)                            |       |      | 2.2   |                      | 3.5  | 3.3  |
| p0 queue free %                   |       |      | 95    |                      | 100  | 90   |
| cM capacity (veh/h)               |       |      | 855   |                      | 358  | 862  |
| Direction, Lane #                 | EB 1  | EB 2 | WB 1  | WB 2                 | WB 3 | NE 1 |
| Volume Total                      | 670   | 336  | 43    | 500                  | 500  | 84   |
| Volume Left                       | 0     | 0    | 43    | 0                    | 0    | 0    |
| Volume Right                      | 0     | 1    | 0     | 0                    | 0    | 84   |
| cSH                               | 1700  | 1700 | 855   | 1700                 | 1700 | 862  |
| Volume to Capacity                | 0.39  | 0.20 | 0.05  | 0.29                 | 0.29 | 0.10 |
| Queue Length 95th (ft)            | 0     | 0    | 4     | 0                    | 0    | 8    |
| Control Delay (s)                 | 0.0   | 0.0  | 9.4   | 0.0                  | 0.0  | 9.6  |
| Lane LOS                          |       |      | A     |                      |      | A    |
| Approach Delay (s)                | 0.0   |      | 0.4   |                      |      | 9.6  |
| Approach LOS                      |       |      |       |                      |      | A    |
| <b>Intersection Summary</b>       |       |      |       |                      |      |      |
| Average Delay                     |       |      | 0.6   |                      |      |      |
| Intersection Capacity Utilization |       |      | 42.5% | ICU Level of Service | A    |      |
| Analysis Period (min)             | 15    |      |       |                      |      |      |



# HCM Unsignalized Intersection Capacity Analysis

## 8: Wheatland & Route 11

12/8/2015



| Movement                          | EBT  | EBR  | WBL   | WBT                  | NBL  | NBR  |
|-----------------------------------|------|------|-------|----------------------|------|------|
| Lane Configurations               | ↑↑   |      | ↵     | ↑↑                   | ↵    |      |
| Traffic Volume (veh/h)            | 1002 | 9    | 8     | 932                  | 7    | 7    |
| Future Volume (Veh/h)             | 1002 | 9    | 8     | 932                  | 7    | 7    |
| Sign Control                      | Free |      |       | Free                 | Stop |      |
| Grade                             | 0%   |      |       | 0%                   | 0%   |      |
| Peak Hour Factor                  | 0.95 | 0.95 | 0.90  | 0.90                 | 0.70 | 0.70 |
| Hourly flow rate (vph)            | 1055 | 9    | 9     | 1036                 | 10   | 10   |
| Pedestrians                       |      |      |       |                      |      |      |
| Lane Width (ft)                   |      |      |       |                      |      |      |
| Walking Speed (ft/s)              |      |      |       |                      |      |      |
| Percent Blockage                  |      |      |       |                      |      |      |
| Right turn flare (veh)            |      |      |       |                      |      |      |
| Median type                       | None |      | TWLTL |                      |      |      |
| Median storage (veh)              | 2    |      |       |                      |      |      |
| Upstream signal (ft)              | 335  |      |       |                      |      |      |
| pX, platoon unblocked             |      |      | 0.80  |                      | 0.80 | 0.80 |
| vC, conflicting volume            |      |      | 1064  |                      | 1596 | 532  |
| vC1, stage 1 conf vol             |      |      |       |                      | 1060 |      |
| vC2, stage 2 conf vol             |      |      |       |                      | 536  |      |
| vCu, unblocked vol                |      |      | 568   |                      | 1236 | 0    |
| tC, single (s)                    |      |      | 4.1   |                      | 7.1  | 6.9  |
| tC, 2 stage (s)                   |      |      |       |                      | 6.1  |      |
| tF (s)                            |      |      | 2.2   |                      | 3.6  | 3.3  |
| p0 queue free %                   |      |      | 99    |                      | 97   | 99   |
| cM capacity (veh/h)               |      |      | 807   |                      | 328  | 868  |
| Direction, Lane #                 | EB 1 | EB 2 | WB 1  | WB 2                 | WB 3 | NB 1 |
| Volume Total                      | 703  | 361  | 9     | 518                  | 518  | 20   |
| Volume Left                       | 0    | 0    | 9     | 0                    | 0    | 10   |
| Volume Right                      | 0    | 9    | 0     | 0                    | 0    | 10   |
| cSH                               | 1700 | 1700 | 807   | 1700                 | 1700 | 476  |
| Volume to Capacity                | 0.41 | 0.21 | 0.01  | 0.30                 | 0.30 | 0.04 |
| Queue Length 95th (ft)            | 0    | 0    | 1     | 0                    | 0    | 3    |
| Control Delay (s)                 | 0.0  | 0.0  | 9.5   | 0.0                  | 0.0  | 12.9 |
| Lane LOS                          | A    |      |       | B                    |      |      |
| Approach Delay (s)                | 0.0  |      | 0.1   |                      |      | 12.9 |
| Approach LOS                      |      |      |       |                      |      | B    |
| Intersection Summary              |      |      |       |                      |      |      |
| Average Delay                     |      |      | 0.2   |                      |      |      |
| Intersection Capacity Utilization |      |      | 38.0% | ICU Level of Service |      | A    |
| Analysis Period (min)             |      |      | 15    |                      |      |      |

# HCM Unsignalized Intersection Capacity Analysis

## 9: University Park/Roca's & Route 11

12/8/2015



| Movement               | EBL  | EBT   | EBR  | WBL  | WBT  | WBR  | NBL  | NBT  | NBR  | SBL  | SBT  | SBR  |
|------------------------|------|-------|------|------|------|------|------|------|------|------|------|------|
| Lane Configurations    |      |       |      |      |      |      |      |      |      |      |      |      |
| Traffic Volume (veh/h) | 4    | 973   | 6    | 5    | 961  | 0    | 9    | 0    | 8    | 0    | 0    | 2    |
| Future Volume (Veh/h)  | 4    | 973   | 6    | 5    | 961  | 0    | 9    | 0    | 8    | 0    | 0    | 2    |
| Sign Control           |      | Free  |      |      | Free |      |      | Stop |      |      | Stop |      |
| Grade                  |      | 0%    |      |      | 0%   |      |      | 0%   |      |      | 0%   |      |
| Peak Hour Factor       | 0.96 | 0.96  | 0.96 | 0.84 | 0.84 | 0.84 | 0.63 | 0.63 | 0.63 | 0.50 | 0.50 | 0.50 |
| Hourly flow rate (vph) | 4    | 1014  | 6    | 6    | 1144 | 0    | 14   | 0    | 13   | 0    | 0    | 4    |
| Pedestrians            |      |       |      |      |      |      |      |      |      |      |      |      |
| Lane Width (ft)        |      |       |      |      |      |      |      |      |      |      |      |      |
| Walking Speed (ft/s)   |      |       |      |      |      |      |      |      |      |      |      |      |
| Percent Blockage       |      |       |      |      |      |      |      |      |      |      |      |      |
| Right turn flare (veh) |      |       |      |      |      |      |      |      |      |      |      |      |
| Median type            |      | TWLTL |      |      | None |      |      |      |      |      |      |      |
| Median storage (veh)   |      | 2     |      |      |      |      |      |      |      |      |      |      |
| Upstream signal (ft)   |      | 792   |      |      |      |      |      |      |      |      |      |      |
| pX, platoon unblocked  |      |       |      | 0.83 |      |      | 0.83 | 0.83 | 0.83 | 0.83 | 0.83 |      |
| vC, conflicting volume | 1144 |       |      | 1020 |      |      | 1613 | 2181 | 510  | 1684 | 2184 | 572  |
| vC1, stage 1 conf vol  |      |       |      |      |      |      | 1025 | 1025 |      | 1156 | 1156 |      |
| vC2, stage 2 conf vol  |      |       |      |      |      |      | 588  | 1156 |      | 528  | 1028 |      |
| vCu, unblocked vol     | 1144 |       |      | 613  |      |      | 1328 | 2013 | 0    | 1413 | 2016 | 572  |
| tC, single (s)         | 4.1  |       |      | 4.9  |      |      | 7.5  | 6.5  | 7.2  | 7.5  | 6.5  | 6.9  |
| tC, 2 stage (s)        |      |       |      |      |      |      | 6.5  | 5.5  |      | 6.5  | 5.5  |      |
| tF (s)                 | 2.2  |       |      | 2.6  |      |      | 3.5  | 4.0  | 3.4  | 3.5  | 4.0  | 3.3  |
| p0 queue free %        | 99   |       |      | 99   |      |      | 95   | 100  | 99   | 100  | 100  | 99   |
| cM capacity (veh/h)    | 618  |       |      | 617  |      |      | 294  | 219  | 871  | 202  | 220  | 468  |

| Direction, Lane #      | EB 1 | EB 2 | EB 3 | WB 1 | WB 2 | WB 3 | NB 1 | SB 1 |
|------------------------|------|------|------|------|------|------|------|------|
| Volume Total           | 4    | 676  | 344  | 6    | 763  | 381  | 27   | 4    |
| Volume Left            | 4    | 0    | 0    | 6    | 0    | 0    | 14   | 0    |
| Volume Right           | 0    | 0    | 6    | 0    | 0    | 0    | 13   | 4    |
| cSH                    | 618  | 1700 | 1700 | 617  | 1700 | 1700 | 431  | 468  |
| Volume to Capacity     | 0.01 | 0.40 | 0.20 | 0.01 | 0.45 | 0.22 | 0.06 | 0.01 |
| Queue Length 95th (ft) | 0    | 0    | 0    | 1    | 0    | 0    | 5    | 1    |
| Control Delay (s)      | 10.9 | 0.0  | 0.0  | 10.9 | 0.0  | 0.0  | 13.9 | 12.8 |
| Lane LOS               | B    |      |      | B    |      |      | B    | B    |
| Approach Delay (s)     | 0.0  |      |      | 0.1  |      |      | 13.9 | 12.8 |
| Approach LOS           |      |      |      |      |      |      | B    | B    |

### Intersection Summary

|                                   |       |
|-----------------------------------|-------|
| Average Delay                     | 0.2   |
| Intersection Capacity Utilization | 41.4% |
| ICU Level of Service              | A     |
| Analysis Period (min)             | 15    |

# HCM Unsignalized Intersection Capacity Analysis

## 10: Route 11 & Nesslerod

12/8/2015

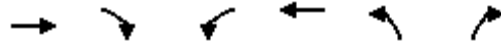


| Movement                          | EBL  | EBT    | WBT    | WBR  | SBL                  | SBR  |
|-----------------------------------|------|--------|--------|------|----------------------|------|
| Lane Configurations               |      |        |        |      |                      |      |
| Traffic Volume (veh/h)            | 3    | 978    | 985    | 1    | 0                    | 1    |
| Future Volume (Veh/h)             | 3    | 978    | 985    | 1    | 0                    | 1    |
| Sign Control                      |      | Free   | Free   |      | Stop                 |      |
| Grade                             |      | 0%     | 0%     |      | 0%                   |      |
| Peak Hour Factor                  | 0.96 | 0.96   | 0.84   | 0.84 | 0.25                 | 0.25 |
| Hourly flow rate (vph)            | 3    | 1019   | 1173   | 1    | 0                    | 4    |
| Pedestrians                       |      |        |        |      |                      |      |
| Lane Width (ft)                   |      |        |        |      |                      |      |
| Walking Speed (ft/s)              |      |        |        |      |                      |      |
| Percent Blockage                  |      |        |        |      |                      |      |
| Right turn flare (veh)            |      |        |        |      |                      |      |
| Median type                       |      | Raised | Raised |      |                      |      |
| Median storage (veh)              |      | 1      | 1      |      |                      |      |
| Upstream signal (ft)              |      | 1287   |        |      |                      |      |
| pX, platoon unblocked             |      |        |        |      | 0.87                 |      |
| vC, conflicting volume            | 1174 |        |        |      | 1689                 | 587  |
| vC1, stage 1 conf vol             |      |        |        |      | 1174                 |      |
| vC2, stage 2 conf vol             |      |        |        |      | 516                  |      |
| vCu, unblocked vol                | 1174 |        |        |      | 1495                 | 587  |
| tC, single (s)                    | 4.1  |        |        |      | 6.8                  | 6.9  |
| tC, 2 stage (s)                   |      |        |        |      | 5.8                  |      |
| tF (s)                            | 2.2  |        |        |      | 3.5                  | 3.3  |
| p0 queue free %                   | 100  |        |        |      | 100                  | 99   |
| cM capacity (veh/h)               | 602  |        |        |      | 209                  | 458  |
| Direction, Lane #                 | EB 1 | EB 2   | EB 3   | WB 1 | WB 2                 | SB 1 |
| Volume Total                      | 3    | 510    | 510    | 782  | 392                  | 4    |
| Volume Left                       | 3    | 0      | 0      | 0    | 0                    | 0    |
| Volume Right                      | 0    | 0      | 0      | 0    | 1                    | 4    |
| cSH                               | 602  | 1700   | 1700   | 1700 | 1700                 | 458  |
| Volume to Capacity                | 0.00 | 0.30   | 0.30   | 0.46 | 0.23                 | 0.01 |
| Queue Length 95th (ft)            | 0    | 0      | 0      | 0    | 0                    | 1    |
| Control Delay (s)                 | 11.0 | 0.0    | 0.0    | 0.0  | 0.0                  | 12.9 |
| Lane LOS                          | B    |        |        |      |                      | B    |
| Approach Delay (s)                | 0.0  |        |        | 0.0  |                      | 12.9 |
| Approach LOS                      |      |        |        |      |                      | B    |
| Intersection Summary              |      |        |        |      |                      |      |
| Average Delay                     |      |        | 0.0    |      |                      |      |
| Intersection Capacity Utilization |      |        | 37.3%  |      | ICU Level of Service | A    |
| Analysis Period (min)             |      |        | 15     |      |                      |      |

# HCM Unsignalized Intersection Capacity Analysis

## 11: Hazel Hollow Rd & Route 11

12/8/2015

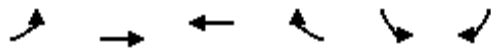


| Movement                          | EBT    | EBR  | WBL    | WBT                  | NBL  | NBR  |      |
|-----------------------------------|--------|------|--------|----------------------|------|------|------|
| Lane Configurations               | ↑↑     | ↑    | ↓      | ↑↑                   | ↓    |      |      |
| Traffic Volume (veh/h)            | 958    | 20   | 21     | 970                  | 16   | 27   |      |
| Future Volume (Veh/h)             | 958    | 20   | 21     | 970                  | 16   | 27   |      |
| Sign Control                      | Free   |      |        | Free                 | Stop |      |      |
| Grade                             | 0%     |      |        | 0%                   | 0%   |      |      |
| Peak Hour Factor                  | 0.96   | 0.96 | 0.84   | 0.84                 | 0.63 | 0.63 |      |
| Hourly flow rate (vph)            | 998    | 21   | 25     | 1155                 | 25   | 43   |      |
| <b>Pedestrians</b>                |        |      |        |                      |      |      |      |
| Lane Width (ft)                   |        |      |        |                      |      |      |      |
| Walking Speed (ft/s)              |        |      |        |                      |      |      |      |
| Percent Blockage                  |        |      |        |                      |      |      |      |
| Right turn flare (veh)            |        |      |        |                      |      |      |      |
| Median type                       | Raised |      | Raised |                      |      |      |      |
| Median storage (veh)              | 1      |      | 1      |                      |      |      |      |
| Upstream signal (ft)              |        |      |        |                      |      |      |      |
| pX, platoon unblocked             |        |      |        |                      |      |      |      |
| vC, conflicting volume            |        |      | 1019   |                      | 1626 | 499  |      |
| vC1, stage 1 conf vol             |        |      |        |                      | 998  |      |      |
| vC2, stage 2 conf vol             |        |      |        |                      | 628  |      |      |
| vCu, unblocked vol                |        |      | 1019   |                      | 1626 | 499  |      |
| tC, single (s)                    |        |      | 4.3    |                      | 6.9  | 6.9  |      |
| tC, 2 stage (s)                   |        |      |        |                      | 5.9  |      |      |
| tF (s)                            |        |      | 2.3    |                      | 3.6  | 3.3  |      |
| p0 queue free %                   |        |      | 96     |                      | 88   | 92   |      |
| cM capacity (veh/h)               |        |      | 631    |                      | 206  | 522  |      |
| Direction, Lane #                 | EB 1   | EB 2 | EB 3   | WB 1                 | WB 2 | WB 3 | NB 1 |
| Volume Total                      | 499    | 499  | 21     | 25                   | 578  | 578  | 68   |
| Volume Left                       | 0      | 0    | 0      | 25                   | 0    | 0    | 25   |
| Volume Right                      | 0      | 0    | 21     | 0                    | 0    | 0    | 43   |
| cSH                               | 1700   | 1700 | 1700   | 631                  | 1700 | 1700 | 334  |
| Volume to Capacity                | 0.29   | 0.29 | 0.01   | 0.04                 | 0.34 | 0.34 | 0.20 |
| Queue Length 95th (ft)            | 0      | 0    | 0      | 3                    | 0    | 0    | 19   |
| Control Delay (s)                 | 0.0    | 0.0  | 0.0    | 10.9                 | 0.0  | 0.0  | 18.5 |
| Lane LOS                          |        |      |        | B                    | C    |      |      |
| Approach Delay (s)                | 0.0    |      |        | 0.2                  |      |      | 18.5 |
| Approach LOS                      |        |      |        |                      |      |      | C    |
| <b>Intersection Summary</b>       |        |      |        |                      |      |      |      |
| Average Delay                     |        |      | 0.7    |                      |      |      |      |
| Intersection Capacity Utilization |        |      | 36.8%  | ICU Level of Service |      | A    |      |
| Analysis Period (min)             |        |      | 15     |                      |      |      |      |

# HCM Signalized Intersection Capacity Analysis

## 2: Route 11 & Route 114

12/8/2015



| Movement               | EBL   | EBT   | WBT   | WBR   | SBL   | SBR   |
|------------------------|-------|-------|-------|-------|-------|-------|
| Lane Configurations    |       |       |       |       |       |       |
| Traffic Volume (vph)   | 335   | 284   | 463   | 548   | 824   | 410   |
| Future Volume (vph)    | 335   | 284   | 463   | 548   | 824   | 410   |
| Ideal Flow (vphpl)     | 1900  | 1900  | 1900  | 1900  | 1900  | 1900  |
| Total Lost time (s)    | 5.5   | 6.0   | 6.0   | 4.0   | 5.5   | 4.0   |
| Lane Util. Factor      | 1.00  | *0.85 | *0.90 | 1.00  | *0.85 | 1.00  |
| Frt                    | 1.00  | 1.00  | 1.00  | 0.85  | 1.00  | 0.85  |
| Flt Protected          | 0.95  | 1.00  | 1.00  | 1.00  | 0.95  | 1.00  |
| Satd. Flow (prot)      | 1787  | 3167  | 3386  | 1599  | 3038  | 1583  |
| Flt Permitted          | 0.95  | 1.00  | 1.00  | 1.00  | 0.95  | 1.00  |
| Satd. Flow (perm)      | 1787  | 3167  | 3386  | 1599  | 3038  | 1583  |
| Peak-hour factor, PHF  | 0.96  | 0.96  | 0.90  | 0.90  | 0.90  | 0.90  |
| Adj. Flow (vph)        | 349   | 296   | 514   | 609   | 916   | 456   |
| RTOR Reduction (vph)   | 0     | 0     | 0     | 0     | 0     | 0     |
| Lane Group Flow (vph)  | 349   | 296   | 514   | 609   | 916   | 456   |
| Heavy Vehicles (%)     | 1%    | 2%    | 1%    | 1%    | 1%    | 2%    |
| Turn Type              | Prot  | NA    | NA    | Free  | Prot  | Free  |
| Protected Phases       | 1     | 6     | 2     |       | 4     |       |
| Permitted Phases       |       |       |       | Free  |       | Free  |
| Actuated Green, G (s)  | 31.8  | 65.7  | 28.4  | 124.7 | 47.5  | 124.7 |
| Effective Green, g (s) | 31.8  | 65.7  | 28.4  | 124.7 | 47.5  | 124.7 |
| Actuated g/C Ratio     | 0.26  | 0.53  | 0.23  | 1.00  | 0.38  | 1.00  |
| Clearance Time (s)     | 5.5   | 6.0   | 6.0   |       | 5.5   |       |
| Vehicle Extension (s)  | 4.0   | 4.5   | 4.5   |       | 4.5   |       |
| Lane Grp Cap (vph)     | 455   | 1668  | 771   | 1599  | 1157  | 1583  |
| v/s Ratio Prot         | c0.20 | 0.09  | c0.15 |       | c0.30 |       |
| v/s Ratio Perm         |       |       |       | 0.38  |       | 0.29  |
| v/c Ratio              | 0.77  | 0.18  | 0.67  | 0.38  | 0.79  | 0.29  |
| Uniform Delay, d1      | 43.0  | 15.4  | 43.8  | 0.0   | 34.2  | 0.0   |
| Progression Factor     | 1.00  | 1.00  | 1.00  | 1.00  | 1.00  | 1.00  |
| Incremental Delay, d2  | 8.0   | 0.1   | 2.6   | 0.7   | 4.2   | 0.5   |
| Delay (s)              | 51.1  | 15.5  | 46.5  | 0.7   | 38.4  | 0.5   |
| Level of Service       | D     | B     | D     | A     | D     | A     |
| Approach Delay (s)     |       | 34.7  | 21.6  |       | 25.8  |       |
| Approach LOS           |       | C     | C     |       | C     |       |

### Intersection Summary

|                                   |       |                           |      |
|-----------------------------------|-------|---------------------------|------|
| HCM 2000 Control Delay            | 26.2  | HCM 2000 Level of Service | C    |
| HCM 2000 Volume to Capacity ratio | 0.75  |                           |      |
| Actuated Cycle Length (s)         | 124.7 | Sum of lost time (s)      | 17.0 |
| Intersection Capacity Utilization | 69.0% | ICU Level of Service      | C    |
| Analysis Period (min)             | 15    |                           |      |
| c Critical Lane Group             |       |                           |      |

HCM Signalized Intersection Capacity Analysis  
6: Kroger/The Rocks & Route 11

12/8/2015



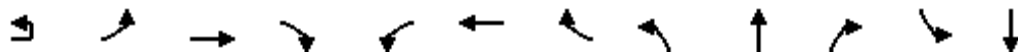
| Movement               | EBL  | EBT   | EBR  | WBL   | WBT   | WBR  | NBL   | NBT  | NBR  | SBL  | SBT  | SBR  |
|------------------------|------|-------|------|-------|-------|------|-------|------|------|------|------|------|
| Lane Configurations    |      |       |      |       |       |      |       |      |      |      |      |      |
| Traffic Volume (vph)   | 29   | 922   | 132  | 127   | 1187  | 6    | 152   | 6    | 159  | 36   | 7    | 65   |
| Future Volume (vph)    | 29   | 922   | 132  | 127   | 1187  | 6    | 152   | 6    | 159  | 36   | 7    | 65   |
| Ideal Flow (vphpl)     | 1900 | 1900  | 1900 | 1900  | 1900  | 1900 | 1900  | 1900 | 1900 | 1900 | 1900 | 1900 |
| Total Lost time (s)    | 5.5  | 6.0   |      | 5.5   | 6.0   |      | 5.5   | 5.5  |      |      | 5.5  | 5.5  |
| Lane Util. Factor      | 1.00 | *0.85 |      | 1.00  | *0.75 |      | 1.00  | 1.00 |      |      | 1.00 | 1.00 |
| Frt                    | 1.00 | 0.98  |      | 1.00  | 1.00  |      | 1.00  | 0.86 |      |      | 1.00 | 0.85 |
| Flt Protected          | 0.95 | 1.00  |      | 0.95  | 1.00  |      | 0.95  | 1.00 |      |      | 0.96 | 1.00 |
| Satd. Flow (prot)      | 1805 | 3134  |      | 1770  | 2820  |      | 1770  | 1610 |      |      | 1780 | 1615 |
| Flt Permitted          | 0.95 | 1.00  |      | 0.95  | 1.00  |      | 0.73  | 1.00 |      |      | 0.64 | 1.00 |
| Satd. Flow (perm)      | 1805 | 3134  |      | 1770  | 2820  |      | 1353  | 1610 |      |      | 1195 | 1615 |
| Peak-hour factor, PHF  | 0.93 | 0.93  | 0.93 | 0.89  | 0.89  | 0.89 | 0.84  | 0.84 | 0.84 | 0.93 | 0.93 | 0.93 |
| Adj. Flow (vph)        | 31   | 991   | 142  | 143   | 1334  | 7    | 181   | 7    | 189  | 39   | 8    | 70   |
| RTOR Reduction (vph)   | 0    | 8     | 0    | 0     | 0     | 0    | 0     | 150  | 0    | 0    | 0    | 56   |
| Lane Group Flow (vph)  | 31   | 1125  | 0    | 143   | 1341  | 0    | 181   | 46   | 0    | 0    | 47   | 14   |
| Heavy Vehicles (%)     | 0%   | 1%    | 2%   | 2%    | 1%    | 0%   | 2%    | 0%   | 1%   | 3%   | 0%   | 0%   |
| Turn Type              | Prot | NA    |      | Prot  | NA    |      | Perm  | NA   |      | Perm | NA   | Perm |
| Protected Phases       | 1    | 6     |      | 5     | 2     |      |       | 8    |      |      | 4    |      |
| Permitted Phases       |      |       |      |       |       |      | 8     |      |      | 4    |      | 4    |
| Actuated Green, G (s)  | 4.7  | 46.5  |      | 13.8  | 55.6  |      | 19.9  | 19.9 |      |      | 19.9 | 19.9 |
| Effective Green, g (s) | 4.7  | 46.5  |      | 13.8  | 55.6  |      | 19.9  | 19.9 |      |      | 19.9 | 19.9 |
| Actuated g/C Ratio     | 0.05 | 0.48  |      | 0.14  | 0.57  |      | 0.20  | 0.20 |      |      | 0.20 | 0.20 |
| Clearance Time (s)     | 5.5  | 6.0   |      | 5.5   | 6.0   |      | 5.5   | 5.5  |      |      | 5.5  | 5.5  |
| Vehicle Extension (s)  | 3.5  | 4.0   |      | 3.5   | 4.0   |      | 4.0   | 4.0  |      |      | 4.0  | 4.0  |
| Lane Grp Cap (vph)     | 87   | 1499  |      | 251   | 1613  |      | 277   | 329  |      |      | 244  | 330  |
| v/s Ratio Prot         | 0.02 | 0.36  |      | c0.08 | c0.48 |      |       | 0.03 |      |      |      |      |
| v/s Ratio Perm         |      |       |      |       |       |      | c0.13 |      |      |      | 0.04 | 0.01 |
| v/c Ratio              | 0.36 | 0.75  |      | 0.57  | 0.83  |      | 0.65  | 0.14 |      |      | 0.19 | 0.04 |
| Uniform Delay, d1      | 44.8 | 20.6  |      | 38.9  | 17.0  |      | 35.5  | 31.6 |      |      | 32.0 | 31.0 |
| Progression Factor     | 1.00 | 1.00  |      | 1.00  | 1.00  |      | 1.00  | 1.00 |      |      | 1.00 | 1.00 |
| Incremental Delay, d2  | 2.9  | 2.3   |      | 3.2   | 4.0   |      | 6.0   | 0.3  |      |      | 0.5  | 0.1  |
| Delay (s)              | 47.7 | 22.9  |      | 42.1  | 21.0  |      | 41.5  | 31.9 |      |      | 32.5 | 31.1 |
| Level of Service       | D    | C     |      | D     | C     |      | D     | C    |      |      | C    | C    |
| Approach Delay (s)     |      | 23.6  |      |       | 23.0  |      |       | 36.5 |      |      | 31.7 |      |
| Approach LOS           |      | C     |      |       | C     |      |       | D    |      |      | C    |      |

| Intersection Summary              |       |                             |
|-----------------------------------|-------|-----------------------------|
| HCM 2000 Control Delay            | 25.2  | HCM 2000 Level of Service C |
| HCM 2000 Volume to Capacity ratio | 0.79  |                             |
| Actuated Cycle Length (s)         | 97.2  | Sum of lost time (s) 17.0   |
| Intersection Capacity Utilization | 72.7% | ICU Level of Service C      |
| Analysis Period (min)             | 15    |                             |
| c Critical Lane Group             |       |                             |

# HCM Unsignalized Intersection Capacity Analysis

## 3: Cherry Ct/Driveway & Route 11

12/8/2015



| Movement               | EBU  | EBL  | EBT  | EBR  | WBL  | WBT   | WBR  | NBL  | NBT  | NBR  | SBL  | SBT  |
|------------------------|------|------|------|------|------|-------|------|------|------|------|------|------|
| Lane Configurations    |      |      |      |      |      |       |      |      |      |      |      |      |
| Traffic Volume (veh/h) | 2    | 12   | 1090 | 4    | 1    | 1040  | 0    | 3    | 0    | 0    | 5    | 0    |
| Future Volume (Veh/h)  | 2    | 12   | 1090 | 4    | 1    | 1040  | 0    | 3    | 0    | 0    | 5    | 0    |
| Sign Control           |      |      | Free |      |      | Free  |      |      | Stop |      |      | Stop |
| Grade                  |      |      | 0%   |      |      | 0%    |      |      | 0%   |      |      | 0%   |
| Peak Hour Factor       | 0.92 | 0.92 | 0.92 | 0.92 | 0.90 | 0.90  | 0.90 | 0.25 | 0.25 | 0.25 | 0.75 | 0.75 |
| Hourly flow rate (vph) | 0    | 13   | 1185 | 4    | 1    | 1156  | 0    | 12   | 0    | 0    | 7    | 0    |
| Pedestrians            |      |      |      |      |      |       |      |      |      |      |      |      |
| Lane Width (ft)        |      |      |      |      |      |       |      |      |      |      |      |      |
| Walking Speed (ft/s)   |      |      |      |      |      |       |      |      |      |      |      |      |
| Percent Blockage       |      |      |      |      |      |       |      |      |      |      |      |      |
| Right turn flare (veh) |      |      |      |      |      |       |      |      |      |      |      |      |
| Median type            |      |      | None |      |      | TWLTL |      |      |      |      |      |      |
| Median storage (veh)   |      |      |      |      |      | 2     |      |      |      |      |      |      |
| Upstream signal (ft)   |      |      | 779  |      |      |       |      |      |      |      |      |      |
| pX, platoon unblocked  | 0.00 |      |      |      | 0.98 |       |      | 0.98 | 0.98 | 0.98 | 0.98 | 0.98 |
| vC, conflicting volume | 0    | 1156 |      |      | 1189 |       |      | 1826 | 2371 | 594  | 1776 | 2373 |
| vC1, stage 1 conf vol  |      |      |      |      |      |       |      | 1213 | 1213 |      | 1158 | 1158 |
| vC2, stage 2 conf vol  |      |      |      |      |      |       |      | 613  | 1158 |      | 618  | 1215 |
| vCu, unblocked vol     | 0    | 1156 |      |      | 1143 |       |      | 1796 | 2355 | 533  | 1745 | 2357 |
| tC, single (s)         | 0.0  | 4.1  |      |      | 4.1  |       |      | 7.5  | 6.5  | 6.9  | 7.5  | 6.5  |
| tC, 2 stage (s)        |      |      |      |      |      |       |      | 6.5  | 5.5  |      | 6.5  | 5.5  |
| tF (s)                 | 0.0  | 2.2  |      |      | 2.2  |       |      | 3.5  | 4.0  | 3.3  | 3.5  | 4.0  |
| p0 queue free %        | 0    | 98   |      |      | 100  |       |      | 93   | 100  | 100  | 96   | 100  |
| cM capacity (veh/h)    | 0    | 612  |      |      | 603  |       |      | 178  | 179  | 484  | 192  | 183  |

| Direction, Lane #      | EB 1 | EB 2 | WB 1 | WB 2 | WB 3 | NB 1 | SB 1 |
|------------------------|------|------|------|------|------|------|------|
| Volume Total           | 606  | 596  | 1    | 771  | 385  | 12   | 40   |
| Volume Left            | 13   | 0    | 1    | 0    | 0    | 12   | 7    |
| Volume Right           | 0    | 4    | 0    | 0    | 0    | 0    | 33   |
| cSH                    | 612  | 1700 | 603  | 1700 | 1700 | 178  | 372  |
| Volume to Capacity     | 0.02 | 0.35 | 0.00 | 0.45 | 0.23 | 0.07 | 0.11 |
| Queue Length 95th (ft) | 2    | 0    | 0    | 0    | 0    | 5    | 9    |
| Control Delay (s)      | 0.6  | 0.0  | 11.0 | 0.0  | 0.0  | 26.7 | 15.9 |
| Lane LOS               | A    |      | B    |      |      | D    | C    |
| Approach Delay (s)     | 0.3  |      | 0.0  |      |      | 26.7 | 15.9 |
| Approach LOS           |      |      |      |      |      | D    | C    |

### Intersection Summary

|                                   |       |
|-----------------------------------|-------|
| Average Delay                     | 0.5   |
| Intersection Capacity Utilization | 50.1% |
| ICU Level of Service              | A     |
| Analysis Period (min)             | 15    |

# HCM Unsignalized Intersection Capacity Analysis

## 3: Cherry Ct/Driveway & Route 11

12/8/2015

| Movement               | SBR  |
|------------------------|------|
| Lane Configurations    |      |
| Traffic Volume (veh/h) | 25   |
| Future Volume (Veh/h)  | 25   |
| Sign Control           |      |
| Grade                  |      |
| Peak Hour Factor       | 0.75 |
| Hourly flow rate (vph) | 33   |
| Pedestrians            |      |
| Lane Width (ft)        |      |
| Walking Speed (ft/s)   |      |
| Percent Blockage       |      |
| Right turn flare (veh) |      |
| Median type            |      |
| Median storage (veh)   |      |
| Upstream signal (ft)   |      |
| pX, platoon unblocked  |      |
| vC, conflicting volume | 578  |
| vC1, stage 1 conf vol  |      |
| vC2, stage 2 conf vol  |      |
| vCu, unblocked vol     | 578  |
| tC, single (s)         | 6.9  |
| tC, 2 stage (s)        |      |
| tF (s)                 | 3.3  |
| p0 queue free %        | 93   |
| cM capacity (veh/h)    | 464  |
| Direction, Lane #      |      |



# HCM Unsignalized Intersection Capacity Analysis

## 4: Route 11 & Belspring Rd

12/8/2015



| Movement               | EBL  | EBT   | EBR  | WBL  | WBT   | WBR  | NBL  | NBT  | NBR  | SBL  | SBT  | SBR  |
|------------------------|------|-------|------|------|-------|------|------|------|------|------|------|------|
| Lane Configurations    | ↖    | ↕     |      | ↖    | ↕     |      |      | ↕    |      | ↖    |      |      |
| Traffic Volume (veh/h) | 1    | 1097  | 1    | 0    | 1096  | 305  | 0    | 0    | 2    | 18   | 0    | 5    |
| Future Volume (Veh/h)  | 1    | 1097  | 1    | 0    | 1096  | 305  | 0    | 0    | 2    | 18   | 0    | 5    |
| Sign Control           |      | Free  |      |      | Free  |      |      | Stop |      |      | Stop |      |
| Grade                  |      | 0%    |      |      | 0%    |      |      | 0%   |      |      | 0%   |      |
| Peak Hour Factor       | 0.96 | 0.96  | 0.96 | 0.89 | 0.89  | 0.89 | 0.50 | 0.50 | 0.50 | 0.64 | 0.64 | 0.64 |
| Hourly flow rate (vph) | 1    | 1143  | 1    | 0    | 1231  | 343  | 0    | 0    | 4    | 28   | 0    | 8    |
| Pedestrians            |      |       |      |      |       |      |      |      |      |      |      |      |
| Lane Width (ft)        |      |       |      |      |       |      |      |      |      |      |      |      |
| Walking Speed (ft/s)   |      |       |      |      |       |      |      |      |      |      |      |      |
| Percent Blockage       |      |       |      |      |       |      |      |      |      |      |      |      |
| Right turn flare (veh) |      |       |      |      |       |      |      |      |      |      |      |      |
| Median type            |      | TWLTL |      |      | TWLTL |      |      |      |      |      |      |      |
| Median storage (veh)   |      | 2     |      |      | 2     |      |      |      |      |      |      |      |
| Upstream signal (ft)   |      |       |      |      | 842   |      |      |      |      |      |      |      |
| pX, platoon unblocked  | 0.64 |       |      |      |       |      | 0.64 | 0.64 |      | 0.64 | 0.64 | 0.64 |
| vC, conflicting volume | 1574 |       |      | 1144 |       |      | 1769 | 2720 | 572  | 1980 | 2548 | 787  |
| vC1, stage 1 conf vol  |      |       |      |      |       |      | 1146 | 1146 |      | 1402 | 1402 |      |
| vC2, stage 2 conf vol  |      |       |      |      |       |      | 624  | 1574 |      | 578  | 1146 |      |
| vCu, unblocked vol     | 763  |       |      | 1144 |       |      | 1069 | 2560 | 572  | 1400 | 2292 | 0    |
| tC, single (s)         | 4.1  |       |      | 4.1  |       |      | 7.5  | 6.5  | 6.9  | 7.5  | 6.5  | 6.9  |
| tC, 2 stage (s)        |      |       |      |      |       |      | 6.5  | 5.5  |      | 6.5  | 5.5  |      |
| tF (s)                 | 2.2  |       |      | 2.2  |       |      | 3.5  | 4.0  | 3.3  | 3.5  | 4.0  | 3.3  |
| p0 queue free %        | 100  |       |      | 100  |       |      | 100  | 100  | 99   | 90   | 100  | 99   |
| cM capacity (veh/h)    | 547  |       |      | 618  |       |      | 207  | 181  | 468  | 273  | 208  | 695  |

| Direction, Lane #      | EB 1 | EB 2 | EB 3 | WB 1 | WB 2 | WB 3 | NB 1 | SB 1 |
|------------------------|------|------|------|------|------|------|------|------|
| Volume Total           | 1    | 762  | 382  | 0    | 821  | 753  | 4    | 36   |
| Volume Left            | 1    | 0    | 0    | 0    | 0    | 0    | 0    | 28   |
| Volume Right           | 0    | 0    | 1    | 0    | 0    | 343  | 4    | 8    |
| cSH                    | 547  | 1700 | 1700 | 1700 | 1700 | 1700 | 468  | 315  |
| Volume to Capacity     | 0.00 | 0.45 | 0.22 | 0.00 | 0.48 | 0.44 | 0.01 | 0.11 |
| Queue Length 95th (ft) | 0    | 0    | 0    | 0    | 0    | 0    | 1    | 10   |
| Control Delay (s)      | 11.6 | 0.0  | 0.0  | 0.0  | 0.0  | 0.0  | 12.8 | 17.9 |
| Lane LOS               | B    |      |      |      |      |      | B    | C    |
| Approach Delay (s)     | 0.0  |      |      | 0.0  |      |      | 12.8 | 17.9 |
| Approach LOS           |      |      |      |      |      |      | B    | C    |




















### Intersection Summary

|                                   |      |
|-----------------------------------|------|
| Average Delay                     | 0.3  |
| Intersection Capacity Utilization | Err% |
| ICU Level of Service              | H    |
| Analysis Period (min)             | 15   |

# HCM Unsignalized Intersection Capacity Analysis

## 5: Driveway/Nicewander Way & Route 11

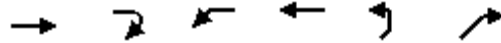
12/8/2015

|                                   |  |  |  |  |  |  |  |  |  |  |  |  |  |
|-----------------------------------|---|---|---|---|---|---|--|---|---|---|---|---|--|
| Movement                          | EBL   | EBT   | EBR   | WBL   | WBT   | WBR   | NBL  | NBT   | NBR   | SBL   | SBT   | SBR   |  |
| Lane Configurations               |  |  |   |  |  |   |  |  |   |   |  |  |  |
| Traffic Volume (veh/h)            | 4   | 1087  | 26  | 18  | 1379  | 5   | 18   | 0   | 18  | 4   | 0   | 4   |  |
| Future Volume (Veh/h)             | 4   | 1087  | 26  | 18  | 1379  | 5   | 18   | 0   | 18  | 4   | 0   | 4   |  |
| Sign Control                      |   | Free  |   |   | Free  |   |  | Stop  |   |   | Stop  |   |  |
| Grade                             |   | 0%  |   |   | 0%  |   |  | 0%  |   |   | 0%  |   |  |
| Peak Hour Factor                  | 0.96  | 0.96  | 0.96  | 0.89  | 0.89  | 0.89  | 0.82   | 0.82  | 0.82  | 0.67  | 0.67  | 0.67  |  |
| Hourly flow rate (vph)            | 4   | 1132  | 27  | 20  | 1549  | 6   | 22   | 0   | 22  | 6   | 0   | 6   |  |
| Pedestrians                       |   |   |   |   |   |   |  |   |   |   |   |   |  |
| Lane Width (ft)                   |   |   |   |   |   |   |  |   |   |   |   |   |  |
| Walking Speed (ft/s)              |   |   |   |   |   |   |  |   |   |   |   |   |  |
| Percent Blockage                  |   |   |   |   |   |   |  |   |   |   |   |   |  |
| Right turn flare (veh)            |   |   |   |   |   |   |  |   |   |   |   |   |  |
| Median type                       |   |   |   |   |   |   |  |   |   |   |   |   |  |
|                                   | TWLTL   |   |   |   |   | TWLTL   |  |   |   |   |   |   |  |
| Median storage (veh)              | 2   |   |   |   |   | 2   |  |   |   |   |   |   |  |
| Upstream signal (ft)              |   |   |   |   |   | 710   |  |   |   |   |   |   |  |
| pX, platoon unblocked             | 0.63  |   |   |   |   |   | 0.63   | 0.63  |   | 0.63  | 0.63  | 0.63  |  |
| vC, conflicting volume            | 1555  |   |   | 1159  |   |   | 1974   | 2748  | 580   | 2188  | 2759  | 778   |  |
| vC1, stage 1 conf vol             |   |   |   |   |   |   | 1154   | 1154  |   | 1592  | 1592  |   |  |
| vC2, stage 2 conf vol             |   |   |   |   |   |   | 820  | 1595  |   | 596   | 1167  |   |  |
| vCu, unblocked vol                | 716   |   |   | 1159  |   |   | 1378   | 2602  | 580   | 1716  | 2619  | 0   |  |
| tC, single (s)                    | 4.1   |   |   | 4.1   |   |   | 7.5  | 6.5   | 6.9   | 7.5   | 6.5   | 6.9   |  |
| tC, 2 stage (s)                   |   |   |   |   |   |   | 6.5  | 5.5   |   | 6.5   | 5.5   |   |  |
| tF (s)                            | 2.2   |   |   | 2.2   |   |   | 3.5  | 4.0   | 3.3   | 3.5   | 4.0   | 3.3   |  |
| p0 queue free %                   | 99  |   |   | 97  |   |   | 89   | 100   | 95  | 97  | 100   | 99  |  |
| cM capacity (veh/h)               | 566   |   |   | 610   |   |   | 199  | 173   | 463   | 191   | 167   | 690   |  |
| Direction, Lane #                 | EB 1  | EB 2  | EB 3  | WB 1  | WB 2  | WB 3  | NB 1   | SB 1  |   |   |   |   |  |
| Volume Total                      | 4   | 755   | 404   | 20  | 1033  | 522   | 44   | 12  |   |   |   |   |  |
| Volume Left                       | 4   | 0   | 0   | 20  | 0   | 0   | 22   | 6   |   |   |   |   |  |
| Volume Right                      | 0   | 0   | 27  | 0   | 0   | 6   | 22   | 6   |   |   |   |   |  |
| cSH                               | 566   | 1700  | 1700  | 610   | 1700  | 1700  | 278  | 299   |   |   |   |   |  |
| Volume to Capacity                | 0.01  | 0.44  | 0.24  | 0.03  | 0.61  | 0.31  | 0.16   | 0.04  |   |   |   |   |  |
| Queue Length 95th (ft)            | 1   | 0   | 0   | 3   | 0   | 0   | 14   | 3   |   |   |   |   |  |
| Control Delay (s)                 | 11.4  | 0.0   | 0.0   | 11.1  | 0.0   | 0.0   | 20.3   | 17.5  |   |   |   |   |  |
| Lane LOS                          | B   |   |   | B   |   |   | C  | C   |   |   |   |   |  |
| Approach Delay (s)                | 0.0   |   |   | 0.1   |   |   | 20.3   | 17.5  |   |   |   |   |  |
| Approach LOS                      |   |   |   |   |   |   | C  | C   |   |   |   |   |  |
| Intersection Summary              |   |   |   |   |   |   |  |   |   |   |   |   |  |
| Average Delay                     | 0.5   |   |   |   |   |   |  |   |   |   |   |   |  |
| Intersection Capacity Utilization | 48.3%   |   |   | ICU Level of Service  |   |   |  |   | A   |   |   |   |  |
| Analysis Period (min)             | 15  |   |   |   |   |   |  |   |   |   |   |   |  |

# HCM Unsignalized Intersection Capacity Analysis

## 7: Kroger Rear & Route 11

12/8/2015



| Movement                          | EBT   | EBR  | WBL   | WBT                  | NEL  | NER  |
|-----------------------------------|-------|------|-------|----------------------|------|------|
| Lane Configurations               | ↑↑    |      | ↵     | ↑↑                   | ↵    |      |
| Traffic Volume (veh/h)            | 1105  | 2    | 49    | 1348                 | 4    | 49   |
| Future Volume (Veh/h)             | 1105  | 2    | 49    | 1348                 | 4    | 49   |
| Sign Control                      | Free  |      |       | Free                 | Stop |      |
| Grade                             | 0%    |      |       | 0%                   | 0%   |      |
| Peak Hour Factor                  | 0.92  | 0.92 | 0.89  | 0.89                 | 0.78 | 0.78 |
| Hourly flow rate (vph)            | 1201  | 2    | 55    | 1515                 | 5    | 63   |
| Pedestrians                       |       |      |       |                      |      |      |
| Lane Width (ft)                   |       |      |       |                      |      |      |
| Walking Speed (ft/s)              |       |      |       |                      |      |      |
| Percent Blockage                  |       |      |       |                      |      |      |
| Right turn flare (veh)            |       |      |       |                      |      |      |
| Median type                       | TWLTL |      |       | None                 |      |      |
| Median storage veh)               | 2     |      |       |                      |      |      |
| Upstream signal (ft)              | 254   |      |       |                      |      |      |
| pX, platoon unblocked             |       |      | 0.72  |                      | 0.72 | 0.72 |
| vC, conflicting volume            |       |      | 1203  |                      | 2070 | 602  |
| vC1, stage 1 conf vol             |       |      |       |                      | 1202 |      |
| vC2, stage 2 conf vol             |       |      |       |                      | 868  |      |
| vCu, unblocked vol                |       |      | 496   |                      | 1703 | 0    |
| tC, single (s)                    |       |      | 4.1   |                      | 7.3  | 6.9  |
| tC, 2 stage (s)                   |       |      |       |                      | 6.3  |      |
| tF (s)                            |       |      | 2.2   |                      | 3.8  | 3.3  |
| p0 queue free %                   |       |      | 93    |                      | 98   | 92   |
| cM capacity (veh/h)               |       |      | 764   |                      | 227  | 783  |
| Direction, Lane #                 | EB 1  | EB 2 | WB 1  | WB 2                 | WB 3 | NE 1 |
| Volume Total                      | 801   | 402  | 55    | 758                  | 758  | 68   |
| Volume Left                       | 0     | 0    | 55    | 0                    | 0    | 5    |
| Volume Right                      | 0     | 2    | 0     | 0                    | 0    | 63   |
| cSH                               | 1700  | 1700 | 764   | 1700                 | 1700 | 664  |
| Volume to Capacity                | 0.47  | 0.24 | 0.07  | 0.45                 | 0.45 | 0.10 |
| Queue Length 95th (ft)            | 0     | 0    | 6     | 0                    | 0    | 9    |
| Control Delay (s)                 | 0.0   | 0.0  | 10.1  | 0.0                  | 0.0  | 11.0 |
| Lane LOS                          | B     |      |       | B                    |      |      |
| Approach Delay (s)                | 0.0   |      | 0.4   |                      |      | 11.0 |
| Approach LOS                      |       |      |       | B                    |      |      |
| Intersection Summary              |       |      |       |                      |      |      |
| Average Delay                     |       |      | 0.5   |                      |      |      |
| Intersection Capacity Utilization |       |      | 47.3% | ICU Level of Service |      | A    |
| Analysis Period (min)             | 15    |      |       |                      |      |      |

# HCM Unsignalized Intersection Capacity Analysis

## 8: Wheatland & Route 11

12/8/2015



| Movement                          | EBT  | EBR  | WBL   | WBT                  | NBL  | NBR  |
|-----------------------------------|------|------|-------|----------------------|------|------|
| Lane Configurations               | ↑↑   |      | ↵     | ↑↑                   | ↵    |      |
| Traffic Volume (veh/h)            | 1147 | 7    | 8     | 1390                 | 7    | 7    |
| Future Volume (Veh/h)             | 1147 | 7    | 8     | 1390                 | 7    | 7    |
| Sign Control                      | Free |      |       | Free                 | Stop |      |
| Grade                             | 0%   |      |       | 0%                   | 0%   |      |
| Peak Hour Factor                  | 0.92 | 0.92 | 0.89  | 0.89                 | 0.58 | 0.58 |
| Hourly flow rate (vph)            | 1247 | 8    | 9     | 1562                 | 12   | 12   |
| <b>Pedestrians</b>                |      |      |       |                      |      |      |
| Lane Width (ft)                   |      |      |       |                      |      |      |
| Walking Speed (ft/s)              |      |      |       |                      |      |      |
| Percent Blockage                  |      |      |       |                      |      |      |
| Right turn flare (veh)            |      |      |       |                      |      |      |
| Median type                       | None |      | TWLTL |                      |      |      |
| Median storage (veh)              | 2    |      |       |                      |      |      |
| Upstream signal (ft)              | 335  |      |       |                      |      |      |
| pX, platoon unblocked             |      |      | 0.72  |                      | 0.72 | 0.72 |
| vC, conflicting volume            |      |      | 1255  |                      | 2050 | 628  |
| vC1, stage 1 conf vol             |      |      |       |                      | 1251 |      |
| vC2, stage 2 conf vol             |      |      |       |                      | 799  |      |
| vCu, unblocked vol                |      |      | 582   |                      | 1684 | 0    |
| tC, single (s)                    |      |      | 4.1   |                      | 7.1  | 7.2  |
| tC, 2 stage (s)                   |      |      |       |                      | 6.1  |      |
| tF (s)                            |      |      | 2.2   |                      | 3.6  | 3.4  |
| p0 queue free %                   |      |      | 99    |                      | 95   | 98   |
| cM capacity (veh/h)               |      |      | 723   |                      | 253  | 755  |
| Direction, Lane #                 | EB 1 | EB 2 | WB 1  | WB 2                 | WB 3 | NB 1 |
| Volume Total                      | 831  | 424  | 9     | 781                  | 781  | 24   |
| Volume Left                       | 0    | 0    | 9     | 0                    | 0    | 12   |
| Volume Right                      | 0    | 8    | 0     | 0                    | 0    | 12   |
| cSH                               | 1700 | 1700 | 723   | 1700                 | 1700 | 379  |
| Volume to Capacity                | 0.49 | 0.25 | 0.01  | 0.46                 | 0.46 | 0.06 |
| Queue Length 95th (ft)            | 0    | 0    | 1     | 0                    | 0    | 5    |
| Control Delay (s)                 | 0.0  | 0.0  | 10.0  | 0.0                  | 0.0  | 15.1 |
| Lane LOS                          | B    |      |       | C                    |      |      |
| Approach Delay (s)                | 0.0  |      | 0.1   |                      |      | 15.1 |
| Approach LOS                      |      |      |       | C                    |      |      |
| <b>Intersection Summary</b>       |      |      |       |                      |      |      |
| Average Delay                     |      |      | 0.2   |                      |      |      |
| Intersection Capacity Utilization |      |      | 48.4% | ICU Level of Service |      | A    |
| Analysis Period (min)             |      |      | 15    |                      |      |      |

# HCM Unsignalized Intersection Capacity Analysis

## 9: University Park/Roca's & Route 11

12/8/2015



| Movement               | EBL   | EBT  | EBR  | WBL  | WBT  | WBR  | NBL  | NBT  | NBR  | SBL  | SBT  | SBR  |
|------------------------|-------|------|------|------|------|------|------|------|------|------|------|------|
| Lane Configurations    |       |      |      |      |      |      |      |      |      |      |      |      |
| Traffic Volume (veh/h) | 11    | 1146 | 4    | 4    | 1401 | 0    | 5    | 0    | 8    | 1    | 0    | 2    |
| Future Volume (Veh/h)  | 11    | 1146 | 4    | 4    | 1401 | 0    | 5    | 0    | 8    | 1    | 0    | 2    |
| Sign Control           |       | Free |      |      | Free |      |      | Stop |      |      | Stop |      |
| Grade                  |       | 0%   |      |      | 0%   |      |      | 0%   |      |      | 0%   |      |
| Peak Hour Factor       | 0.88  | 0.88 | 0.88 | 0.92 | 0.92 | 0.92 | 0.54 | 0.54 | 0.54 | 0.75 | 0.75 | 0.75 |
| Hourly flow rate (vph) | 13    | 1302 | 5    | 4    | 1523 | 0    | 9    | 0    | 15   | 1    | 0    | 3    |
| Pedestrians            |       |      |      |      |      |      |      |      |      |      |      |      |
| Lane Width (ft)        |       |      |      |      |      |      |      |      |      |      |      |      |
| Walking Speed (ft/s)   |       |      |      |      |      |      |      |      |      |      |      |      |
| Percent Blockage       |       |      |      |      |      |      |      |      |      |      |      |      |
| Right turn flare (veh) |       |      |      |      |      |      |      |      |      |      |      |      |
| Median type            | TWLTL |      |      |      |      | None |      |      |      |      |      |      |
| Median storage veh     | 2     |      |      |      |      |      |      |      |      |      |      |      |
| Upstream signal (ft)   | 792   |      |      |      |      |      |      |      |      |      |      |      |
| pX, platoon unblocked  |       |      |      | 0.74 |      |      | 0.74 | 0.74 | 0.74 | 0.74 | 0.74 | 0.74 |
| vC, conflicting volume | 1523  |      |      | 1307 |      |      | 2103 | 2862 | 654  | 2223 | 2864 | 762  |
| vC1, stage 1 conf vol  |       |      |      |      |      |      | 1330 | 1330 |      | 1531 | 1531 |      |
| vC2, stage 2 conf vol  |       |      |      |      |      |      | 772  | 1531 |      | 692  | 1333 |      |
| vCu, unblocked vol     | 1523  |      |      | 722  |      |      | 1793 | 2814 | 0    | 1955 | 2817 | 762  |
| tC, single (s)         | 4.1   |      |      | 4.1  |      |      | 7.5  | 6.5  | 6.9  | 7.5  | 6.5  | 6.9  |
| tC, 2 stage (s)        |       |      |      |      |      |      | 6.5  | 5.5  |      | 6.5  | 5.5  |      |
| tF (s)                 | 2.2   |      |      | 2.2  |      |      | 3.5  | 4.0  | 3.3  | 3.5  | 4.0  | 3.3  |
| p0 queue free %        | 97    |      |      | 99   |      |      | 96   | 100  | 98   | 99   | 100  | 99   |
| cM capacity (veh/h)    | 444   |      |      | 661  |      |      | 207  | 139  | 811  | 119  | 146  | 352  |

| Direction, Lane #      | EB 1 | EB 2 | EB 3 | WB 1 | WB 2 | WB 3 | NB 1 | SB 1 |
|------------------------|------|------|------|------|------|------|------|------|
| Volume Total           | 13   | 868  | 439  | 4    | 1015 | 508  | 24   | 4    |
| Volume Left            | 13   | 0    | 0    | 4    | 0    | 0    | 9    | 1    |
| Volume Right           | 0    | 0    | 5    | 0    | 0    | 0    | 15   | 3    |
| cSH                    | 444  | 1700 | 1700 | 661  | 1700 | 1700 | 387  | 237  |
| Volume to Capacity     | 0.03 | 0.51 | 0.26 | 0.01 | 0.60 | 0.30 | 0.06 | 0.02 |
| Queue Length 95th (ft) | 2    | 0    | 0    | 0    | 0    | 0    | 5    | 1    |
| Control Delay (s)      | 13.4 | 0.0  | 0.0  | 10.5 | 0.0  | 0.0  | 14.9 | 20.5 |
| Lane LOS               | B    |      |      | B    |      |      | B    | C    |
| Approach Delay (s)     | 0.1  |      |      | 0.0  |      |      | 14.9 | 20.5 |
| Approach LOS           |      |      |      |      |      |      | B    | C    |

### Intersection Summary

|                                   |       |
|-----------------------------------|-------|
| Average Delay                     | 0.2   |
| Intersection Capacity Utilization | 48.7% |
| ICU Level of Service              | A     |
| Analysis Period (min)             | 15    |

# HCM Unsignalized Intersection Capacity Analysis

## 10: Route 11 & Nesslerod

12/8/2015

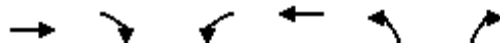


| Movement                          | EBL  | EBT    | WBT    | WBR  | SBL                  | SBR  |
|-----------------------------------|------|--------|--------|------|----------------------|------|
| Lane Configurations               |      |        |        |      |                      |      |
| Traffic Volume (veh/h)            | 8    | 1147   | 1413   | 1    | 2                    | 2    |
| Future Volume (Veh/h)             | 8    | 1147   | 1413   | 1    | 2                    | 2    |
| Sign Control                      |      | Free   | Free   |      | Stop                 |      |
| Grade                             |      | 0%     | 0%     |      | 0%                   |      |
| Peak Hour Factor                  | 0.88 | 0.88   | 0.92   | 0.92 | 0.50                 | 0.50 |
| Hourly flow rate (vph)            | 9    | 1303   | 1536   | 1    | 4                    | 4    |
| Pedestrians                       |      |        |        |      |                      |      |
| Lane Width (ft)                   |      |        |        |      |                      |      |
| Walking Speed (ft/s)              |      |        |        |      |                      |      |
| Percent Blockage                  |      |        |        |      |                      |      |
| Right turn flare (veh)            |      |        |        |      |                      |      |
| Median type                       |      | Raised | Raised |      |                      |      |
| Median storage veh                |      | 1      | 1      |      |                      |      |
| Upstream signal (ft)              |      | 1287   |        |      |                      |      |
| pX, platoon unblocked             |      |        |        |      | 0.77                 |      |
| vC, conflicting volume            | 1537 |        |        |      | 2206                 | 768  |
| vC1, stage 1 conf vol             |      |        |        |      | 1536                 |      |
| vC2, stage 2 conf vol             |      |        |        |      | 670                  |      |
| vCu, unblocked vol                | 1537 |        |        |      | 1969                 | 768  |
| tC, single (s)                    | 4.1  |        |        |      | 6.8                  | 6.9  |
| tC, 2 stage (s)                   |      |        |        |      | 5.8                  |      |
| tF (s)                            | 2.2  |        |        |      | 3.5                  | 3.3  |
| p0 queue free %                   | 98   |        |        |      | 97                   | 99   |
| cM capacity (veh/h)               | 438  |        |        |      | 136                  | 348  |
| Direction, Lane #                 | EB 1 | EB 2   | EB 3   | WB 1 | WB 2                 | SB 1 |
| Volume Total                      | 9    | 652    | 652    | 1024 | 513                  | 8    |
| Volume Left                       | 9    | 0      | 0      | 0    | 0                    | 4    |
| Volume Right                      | 0    | 0      | 0      | 0    | 1                    | 4    |
| cSH                               | 438  | 1700   | 1700   | 1700 | 1700                 | 195  |
| Volume to Capacity                | 0.02 | 0.38   | 0.38   | 0.60 | 0.30                 | 0.04 |
| Queue Length 95th (ft)            | 2    | 0      | 0      | 0    | 0                    | 3    |
| Control Delay (s)                 | 13.4 | 0.0    | 0.0    | 0.0  | 0.0                  | 24.2 |
| Lane LOS                          | B    |        |        |      |                      | C    |
| Approach Delay (s)                | 0.1  |        |        | 0.0  |                      | 24.2 |
| Approach LOS                      |      |        |        |      |                      | C    |
| Intersection Summary              |      |        |        |      |                      |      |
| Average Delay                     |      |        | 0.1    |      |                      |      |
| Intersection Capacity Utilization |      |        | 49.1%  |      | ICU Level of Service | A    |
| Analysis Period (min)             |      |        | 15     |      |                      |      |

# HCM Unsignalized Intersection Capacity Analysis

## 11: Hazel Hollow Rd & Route 11

12/8/2015



| Movement                          | EBT         | EBR         | WBL         | WBT                  | NBL         | NBR         |             |
|-----------------------------------|-------------|-------------|-------------|----------------------|-------------|-------------|-------------|
| Lane Configurations               | ↑↑          | ↑           | ↓           | ↑↑                   | ↓           |             |             |
| Traffic Volume (veh/h)            | 1116        | 33          | 43          | 1398                 | 16          | 31          |             |
| Future Volume (Veh/h)             | 1116        | 33          | 43          | 1398                 | 16          | 31          |             |
| Sign Control                      | Free        |             |             | Free                 | Stop        |             |             |
| Grade                             | 0%          |             |             | 0%                   | 0%          |             |             |
| Peak Hour Factor                  | 0.88        | 0.88        | 0.92        | 0.92                 | 0.78        | 0.78        |             |
| Hourly flow rate (vph)            | 1268        | 38          | 47          | 1520                 | 21          | 40          |             |
| <b>Pedestrians</b>                |             |             |             |                      |             |             |             |
| Lane Width (ft)                   |             |             |             |                      |             |             |             |
| Walking Speed (ft/s)              |             |             |             |                      |             |             |             |
| Percent Blockage                  |             |             |             |                      |             |             |             |
| Right turn flare (veh)            |             |             |             |                      |             |             |             |
| Median type                       | Raised      |             |             | Raised               |             |             |             |
| Median storage (veh)              | 1           |             |             | 1                    |             |             |             |
| Upstream signal (ft)              |             |             |             |                      |             |             |             |
| pX, platoon unblocked             |             |             |             |                      |             |             |             |
| vC, conflicting volume            |             |             | 1306        |                      | 2122        | 634         |             |
| vC1, stage 1 conf vol             |             |             |             |                      | 1268        |             |             |
| vC2, stage 2 conf vol             |             |             |             |                      | 854         |             |             |
| vCu, unblocked vol                |             |             | 1306        |                      | 2122        | 634         |             |
| tC, single (s)                    |             |             | 4.1         |                      | 6.8         | 7.0         |             |
| tC, 2 stage (s)                   |             |             |             |                      | 5.8         |             |             |
| tF (s)                            |             |             | 2.2         |                      | 3.5         | 3.3         |             |
| p0 queue free %                   |             |             | 91          |                      | 85          | 90          |             |
| cM capacity (veh/h)               |             |             | 537         |                      | 145         | 419         |             |
| <b>Direction, Lane #</b>          | <b>EB 1</b> | <b>EB 2</b> | <b>EB 3</b> | <b>WB 1</b>          | <b>WB 2</b> | <b>WB 3</b> | <b>NB 1</b> |
| Volume Total                      | 634         | 634         | 38          | 47                   | 760         | 760         | 61          |
| Volume Left                       | 0           | 0           | 0           | 47                   | 0           | 0           | 21          |
| Volume Right                      | 0           | 0           | 38          | 0                    | 0           | 0           | 40          |
| cSH                               | 1700        | 1700        | 1700        | 537                  | 1700        | 1700        | 254         |
| Volume to Capacity                | 0.37        | 0.37        | 0.02        | 0.09                 | 0.45        | 0.45        | 0.24        |
| Queue Length 95th (ft)            | 0           | 0           | 0           | 7                    | 0           | 0           | 23          |
| Control Delay (s)                 | 0.0         | 0.0         | 0.0         | 12.3                 | 0.0         | 0.0         | 23.6        |
| Lane LOS                          |             |             |             | B                    | C           |             |             |
| Approach Delay (s)                | 0.0         |             |             | 0.4                  |             |             | 23.6        |
| Approach LOS                      |             |             |             |                      |             |             | C           |
| <b>Intersection Summary</b>       |             |             |             |                      |             |             |             |
| Average Delay                     |             |             | 0.7         |                      |             |             |             |
| Intersection Capacity Utilization |             |             | 48.6%       | ICU Level of Service |             | A           |             |
| Analysis Period (min)             |             |             | 15          |                      |             |             |             |

# HCM Signalized Intersection Capacity Analysis

## 2: Route 11 & Route 114

12/8/2015



| Movement               | EBL   | EBT  | WBT   | WBR  | SBL   | SBR  |
|------------------------|-------|------|-------|------|-------|------|
| Lane Configurations    |       |      |       |      |       |      |
| Traffic Volume (vph)   | 364   | 407  | 209   | 256  | 488   | 281  |
| Future Volume (vph)    | 364   | 407  | 209   | 256  | 488   | 281  |
| Ideal Flow (vphpl)     | 1900  | 1900 | 1900  | 1900 | 1900  | 1900 |
| Total Lost time (s)    | 5.5   | 6.0  | 6.0   | 4.0  | 5.5   | 4.0  |
| Lane Util. Factor      | 1.00  | 0.95 | 0.95  | 1.00 | 0.97  | 1.00 |
| Frt                    | 1.00  | 1.00 | 1.00  | 0.85 | 1.00  | 0.85 |
| Flt Protected          | 0.95  | 1.00 | 1.00  | 1.00 | 0.95  | 1.00 |
| Satd. Flow (prot)      | 1719  | 3574 | 3539  | 1553 | 3467  | 1524 |
| Flt Permitted          | 0.95  | 1.00 | 1.00  | 1.00 | 0.95  | 1.00 |
| Satd. Flow (perm)      | 1719  | 3574 | 3539  | 1553 | 3467  | 1524 |
| Peak-hour factor, PHF  | 0.86  | 0.86 | 0.92  | 0.92 | 0.88  | 0.88 |
| Growth Factor (vph)    | 105%  | 105% | 105%  | 105% | 105%  | 105% |
| Adj. Flow (vph)        | 444   | 497  | 239   | 292  | 582   | 335  |
| RTOR Reduction (vph)   | 0     | 0    | 0     | 0    | 0     | 0    |
| Lane Group Flow (vph)  | 444   | 497  | 239   | 292  | 582   | 335  |
| Heavy Vehicles (%)     | 5%    | 1%   | 2%    | 4%   | 1%    | 6%   |
| Turn Type              | Prot  | NA   | NA    | Free | Prot  | Free |
| Protected Phases       | 1     | 6    | 2     |      | 4     |      |
| Permitted Phases       |       |      |       | Free |       | Free |
| Actuated Green, G (s)  | 39.9  | 60.2 | 14.8  | 95.0 | 23.3  | 95.0 |
| Effective Green, g (s) | 39.9  | 60.2 | 14.8  | 95.0 | 23.3  | 95.0 |
| Actuated g/C Ratio     | 0.42  | 0.63 | 0.16  | 1.00 | 0.25  | 1.00 |
| Clearance Time (s)     | 5.5   | 6.0  | 6.0   |      | 5.5   |      |
| Vehicle Extension (s)  | 4.0   | 4.5  | 4.5   |      | 4.5   |      |
| Lane Grp Cap (vph)     | 721   | 2264 | 551   | 1553 | 850   | 1524 |
| v/s Ratio Prot         | c0.26 | 0.14 | c0.07 |      | c0.17 |      |
| v/s Ratio Perm         |       |      |       | 0.19 |       | 0.22 |
| v/c Ratio              | 0.62  | 0.22 | 0.43  | 0.19 | 0.68  | 0.22 |
| Uniform Delay, d1      | 21.6  | 7.4  | 36.3  | 0.0  | 32.5  | 0.0  |
| Progression Factor     | 1.00  | 1.00 | 1.00  | 1.00 | 1.00  | 1.00 |
| Incremental Delay, d2  | 1.8   | 0.1  | 0.9   | 0.3  | 2.7   | 0.3  |
| Delay (s)              | 23.4  | 7.5  | 37.3  | 0.3  | 35.2  | 0.3  |
| Level of Service       | C     | A    | D     | A    | D     | A    |
| Approach Delay (s)     |       | 15.0 | 16.9  |      | 22.5  |      |
| Approach LOS           |       | B    | B     |      | C     |      |

### Intersection Summary

|                                   |       |                           |      |
|-----------------------------------|-------|---------------------------|------|
| HCM 2000 Control Delay            | 18.3  | HCM 2000 Level of Service | B    |
| HCM 2000 Volume to Capacity ratio | 0.60  |                           |      |
| Actuated Cycle Length (s)         | 95.0  | Sum of lost time (s)      | 17.0 |
| Intersection Capacity Utilization | 61.6% | ICU Level of Service      | B    |
| Analysis Period (min)             | 15    |                           |      |

c Critical Lane Group



# HCM Signalized Intersection Capacity Analysis

## 6: Kroger/The Rocks & Route 11

12/8/2015



| Movement               | EBL  | EBT   | EBR  | WBL   | WBT  | WBR  | NBL   | NBT  | NBR  | SBL  | SBT  | SBR  |
|------------------------|------|-------|------|-------|------|------|-------|------|------|------|------|------|
| Lane Configurations    | ↖    | ↗     |      | ↖     | ↗    |      | ↖     | ↗    |      |      | ↖    | ↗    |
| Traffic Volume (vph)   | 25   | 757   | 106  | 47    | 545  | 3    | 58    | 2    | 54   | 34   | 3    | 45   |
| Future Volume (vph)    | 25   | 757   | 106  | 47    | 545  | 3    | 58    | 2    | 54   | 34   | 3    | 45   |
| Ideal Flow (vphpl)     | 1900 | 1900  | 1900 | 1900  | 1900 | 1900 | 1900  | 1900 | 1900 | 1900 | 1900 | 1900 |
| Total Lost time (s)    | 5.5  | 6.0   |      | 5.5   | 6.0  |      | 5.5   | 5.5  |      |      | 5.5  | 5.5  |
| Lane Util. Factor      | 1.00 | 0.95  |      | 1.00  | 0.95 |      | 1.00  | 1.00 |      |      | 1.00 | 1.00 |
| Frt                    | 1.00 | 0.98  |      | 1.00  | 1.00 |      | 1.00  | 0.85 |      |      | 1.00 | 0.85 |
| Flt Protected          | 0.95 | 1.00  |      | 0.95  | 1.00 |      | 0.95  | 1.00 |      |      | 0.96 | 1.00 |
| Satd. Flow (prot)      | 1736 | 3470  |      | 1656  | 3537 |      | 1719  | 1570 |      |      | 1817 | 1583 |
| Flt Permitted          | 0.95 | 1.00  |      | 0.95  | 1.00 |      | 0.73  | 1.00 |      |      | 0.70 | 1.00 |
| Satd. Flow (perm)      | 1736 | 3470  |      | 1656  | 3537 |      | 1316  | 1570 |      |      | 1322 | 1583 |
| Peak-hour factor, PHF  | 0.88 | 0.88  | 0.88 | 0.93  | 0.93 | 0.93 | 0.86  | 0.86 | 0.86 | 0.85 | 0.85 | 0.85 |
| Growth Factor (vph)    | 105% | 105%  | 105% | 105%  | 105% | 105% | 105%  | 105% | 105% | 105% | 105% | 105% |
| Adj. Flow (vph)        | 30   | 903   | 126  | 53    | 615  | 3    | 71    | 2    | 66   | 42   | 4    | 56   |
| RTOR Reduction (vph)   | 0    | 7     | 0    | 0     | 0    | 0    | 0     | 57   | 0    | 0    | 0    | 49   |
| Lane Group Flow (vph)  | 30   | 1022  | 0    | 53    | 618  | 0    | 71    | 11   | 0    | 0    | 46   | 7    |
| Heavy Vehicles (%)     | 4%   | 2%    | 3%   | 9%    | 2%   | 0%   | 5%    | 50%  | 2%   | 0%   | 0%   | 2%   |
| Turn Type              | Prot | NA    |      | Prot  | NA   |      | Perm  | NA   |      | Perm | NA   | Perm |
| Protected Phases       | 1    | 6     |      | 5     | 2    |      |       | 8    |      |      | 4    |      |
| Permitted Phases       |      |       |      |       |      |      | 8     |      |      | 4    |      | 4    |
| Actuated Green, G (s)  | 2.7  | 35.3  |      | 4.9   | 37.5 |      | 8.7   | 8.7  |      |      | 8.7  | 8.7  |
| Effective Green, g (s) | 2.7  | 35.3  |      | 4.9   | 37.5 |      | 8.7   | 8.7  |      |      | 8.7  | 8.7  |
| Actuated g/C Ratio     | 0.04 | 0.54  |      | 0.07  | 0.57 |      | 0.13  | 0.13 |      |      | 0.13 | 0.13 |
| Clearance Time (s)     | 5.5  | 6.0   |      | 5.5   | 6.0  |      | 5.5   | 5.5  |      |      | 5.5  | 5.5  |
| Vehicle Extension (s)  | 3.5  | 4.0   |      | 3.5   | 4.0  |      | 4.0   | 4.0  |      |      | 4.0  | 4.0  |
| Lane Grp Cap (vph)     | 71   | 1858  |      | 123   | 2012 |      | 173   | 207  |      |      | 174  | 208  |
| v/s Ratio Prot         | 0.02 | c0.29 |      | c0.03 | 0.17 |      |       | 0.01 |      |      |      |      |
| v/s Ratio Perm         |      |       |      |       |      |      | c0.05 |      |      |      | 0.03 | 0.00 |
| v/c Ratio              | 0.42 | 0.55  |      | 0.43  | 0.31 |      | 0.41  | 0.05 |      |      | 0.26 | 0.04 |
| Uniform Delay, d1      | 30.8 | 10.1  |      | 29.2  | 7.4  |      | 26.2  | 25.0 |      |      | 25.7 | 24.9 |
| Progression Factor     | 1.00 | 1.00  |      | 1.00  | 1.00 |      | 1.00  | 1.00 |      |      | 1.00 | 1.00 |
| Incremental Delay, d2  | 4.7  | 0.4   |      | 2.8   | 0.1  |      | 2.2   | 0.1  |      |      | 1.1  | 0.1  |
| Delay (s)              | 35.6 | 10.5  |      | 32.0  | 7.5  |      | 28.4  | 25.1 |      |      | 26.8 | 25.0 |
| Level of Service       | D    | B     |      | C     | A    |      | C     | C    |      |      | C    | C    |
| Approach Delay (s)     |      | 11.2  |      |       | 9.5  |      |       | 26.8 |      |      | 25.8 |      |
| Approach LOS           |      | B     |      |       | A    |      |       | C    |      |      | C    |      |

### Intersection Summary

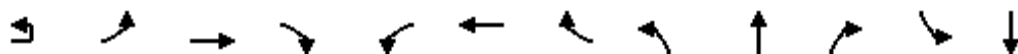
|                                   |       |                           |      |
|-----------------------------------|-------|---------------------------|------|
| HCM 2000 Control Delay            | 12.5  | HCM 2000 Level of Service | B    |
| HCM 2000 Volume to Capacity ratio | 0.51  |                           |      |
| Actuated Cycle Length (s)         | 65.9  | Sum of lost time (s)      | 17.0 |
| Intersection Capacity Utilization | 54.7% | ICU Level of Service      | A    |
| Analysis Period (min)             | 15    |                           |      |

c Critical Lane Group

# HCM Unsignalized Intersection Capacity Analysis

## 3: Cherry Ct/Driveway & Route 11

12/8/2015



| Movement               | EBU  | EBL  | EBT  | EBR  | WBL  | WBT   | WBR  | NBL  | NBT  | NBR  | SBL  | SBT  |
|------------------------|------|------|------|------|------|-------|------|------|------|------|------|------|
| Lane Configurations    |      |      | ↔    |      | ↔    | ↔     |      |      | ↔    |      |      | ↔    |
| Traffic Volume (veh/h) | 3    | 3    | 888  | 1    | 0    | 463   | 0    | 3    | 0    | 3    | 0    | 0    |
| Future Volume (Veh/h)  | 3    | 3    | 888  | 1    | 0    | 463   | 0    | 3    | 0    | 3    | 0    | 0    |
| Sign Control           |      |      | Free |      |      | Free  |      |      | Stop |      |      | Stop |
| Grade                  |      |      | 0%   |      |      | 0%    |      |      | 0%   |      |      | 0%   |
| Peak Hour Factor       | 0.87 | 0.87 | 0.87 | 0.87 | 0.92 | 0.92  | 0.92 | 0.30 | 0.30 | 0.30 | 0.38 | 0.38 |
| Hourly flow rate (vph) | 0    | 4    | 1072 | 1    | 0    | 528   | 0    | 11   | 0    | 11   | 0    | 0    |
| Pedestrians            |      |      |      |      |      |       |      |      |      |      |      |      |
| Lane Width (ft)        |      |      |      |      |      |       |      |      |      |      |      |      |
| Walking Speed (ft/s)   |      |      |      |      |      |       |      |      |      |      |      |      |
| Percent Blockage       |      |      |      |      |      |       |      |      |      |      |      |      |
| Right turn flare (veh) |      |      |      |      |      |       |      |      |      |      |      |      |
| Median type            |      |      | None |      |      | TWLTL |      |      |      |      |      |      |
| Median storage (veh)   |      |      |      |      |      | 2     |      |      |      |      |      |      |
| Upstream signal (ft)   |      |      | 779  |      |      |       |      |      |      |      |      |      |
| pX, platoon unblocked  | 0.00 |      |      |      | 0.96 |       |      | 0.96 | 0.96 | 0.96 | 0.96 | 0.96 |
| vC, conflicting volume | 0    | 528  |      |      | 1073 |       |      | 1352 | 1608 | 536  | 1083 | 1609 |
| vC1, stage 1 conf vol  |      |      |      |      |      |       |      | 1080 | 1080 |      | 528  | 528  |
| vC2, stage 2 conf vol  |      |      |      |      |      |       |      | 272  | 528  |      | 555  | 1081 |
| vCu, unblocked vol     | 0    | 528  |      |      | 990  |       |      | 1281 | 1548 | 430  | 1000 | 1549 |
| tC, single (s)         | 0.0  | 4.1  |      |      | 4.1  |       |      | 7.5  | 6.5  | 6.9  | 7.5  | 6.5  |
| tC, 2 stage (s)        |      |      |      |      |      |       |      | 6.5  | 5.5  |      | 6.5  | 5.5  |
| tF (s)                 | 0.0  | 2.2  |      |      | 2.2  |       |      | 3.5  | 4.0  | 3.3  | 3.5  | 4.0  |
| p0 queue free %        | 0    | 100  |      |      | 100  |       |      | 95   | 100  | 98   | 100  | 100  |
| cM capacity (veh/h)    | 0    | 1049 |      |      | 677  |       |      | 242  | 276  | 555  | 397  | 276  |

| Direction, Lane #      | EB 1 | EB 2 | WB 1 | WB 2 | WB 3 | NB 1 | SB 1 |
|------------------------|------|------|------|------|------|------|------|
| Volume Total           | 540  | 537  | 0    | 352  | 176  | 22   | 8    |
| Volume Left            | 4    | 0    | 0    | 0    | 0    | 11   | 0    |
| Volume Right           | 0    | 1    | 0    | 0    | 0    | 11   | 8    |
| cSH                    | 1049 | 1700 | 1700 | 1700 | 1700 | 337  | 741  |
| Volume to Capacity     | 0.00 | 0.32 | 0.00 | 0.21 | 0.10 | 0.07 | 0.01 |
| Queue Length 95th (ft) | 0    | 0    | 0    | 0    | 0    | 5    | 1    |
| Control Delay (s)      | 0.1  | 0.0  | 0.0  | 0.0  | 0.0  | 16.4 | 9.9  |
| Lane LOS               | A    |      |      |      |      | C    | A    |
| Approach Delay (s)     | 0.1  |      | 0.0  |      |      | 16.4 | 9.9  |
| Approach LOS           |      |      |      |      |      | C    | A    |

### Intersection Summary

|                                   |       |
|-----------------------------------|-------|
| Average Delay                     | 0.3   |
| Intersection Capacity Utilization | 40.2% |
| ICU Level of Service              | A     |
| Analysis Period (min)             | 15    |

# HCM Unsignalized Intersection Capacity Analysis

## 3: Cherry Ct/Driveway & Route 11

12/8/2015

| Movement               | SBR  |
|------------------------|------|
| Lane Configurations    |      |
| Traffic Volume (veh/h) | 3    |
| Future Volume (Veh/h)  | 3    |
| Sign Control           |      |
| Grade                  |      |
| Peak Hour Factor       | 0.38 |
| Hourly flow rate (vph) | 8    |
| Pedestrians            |      |
| Lane Width (ft)        |      |
| Walking Speed (ft/s)   |      |
| Percent Blockage       |      |
| Right turn flare (veh) |      |
| Median type            |      |
| Median storage veh)    |      |
| Upstream signal (ft)   |      |
| pX, platoon unblocked  |      |
| vC, conflicting volume | 264  |
| vC1, stage 1 conf vol  |      |
| vC2, stage 2 conf vol  |      |
| vCu, unblocked vol     | 264  |
| tC, single (s)         | 6.9  |
| tC, 2 stage (s)        |      |
| tF (s)                 | 3.3  |
| p0 queue free %        | 99   |
| cM capacity (veh/h)    | 741  |
| Direction, Lane #      |      |

# HCM Unsignalized Intersection Capacity Analysis

## 4: Route 11 & Belspring Rd

12/8/2015



| Movement               | EBL  | EBT   | EBR  | WBL  | WBT   | WBR  | NBL  | NBT  | NBR  | SBL  | SBT  | SBR  |
|------------------------|------|-------|------|------|-------|------|------|------|------|------|------|------|
| Lane Configurations    | ↖    | ↕     |      | ↖    | ↕     |      |      | ↕    |      | ↖    |      |      |
| Traffic Volume (veh/h) | 2    | 883   | 0    | 0    | 460   | 187  | 0    | 0    | 0    | 12   | 0    | 3    |
| Future Volume (Veh/h)  | 2    | 883   | 0    | 0    | 460   | 187  | 0    | 0    | 0    | 12   | 0    | 3    |
| Sign Control           |      | Free  |      |      | Free  |      |      | Stop |      |      | Stop |      |
| Grade                  |      | 0%    |      |      | 0%    |      |      | 0%   |      |      | 0%   |      |
| Peak Hour Factor       | 0.90 | 0.90  | 0.90 | 0.95 | 0.95  | 0.95 | 1.00 | 1.00 | 1.00 | 0.75 | 0.75 | 0.75 |
| Hourly flow rate (vph) | 2    | 1030  | 0    | 0    | 508   | 207  | 0    | 0    | 0    | 17   | 0    | 4    |
| Pedestrians            |      |       |      |      |       |      |      |      |      |      |      |      |
| Lane Width (ft)        |      |       |      |      |       |      |      |      |      |      |      |      |
| Walking Speed (ft/s)   |      |       |      |      |       |      |      |      |      |      |      |      |
| Percent Blockage       |      |       |      |      |       |      |      |      |      |      |      |      |
| Right turn flare (veh) |      |       |      |      |       |      |      |      |      |      |      |      |
| Median type            |      | TWLTL |      |      | TWLTL |      |      |      |      |      |      |      |
| Median storage (veh)   |      | 2     |      |      | 2     |      |      |      |      |      |      |      |
| Upstream signal (ft)   |      |       |      |      | 842   |      |      |      |      |      |      |      |
| pX, platoon unblocked  | 0.95 |       |      |      |       |      | 0.95 | 0.95 |      | 0.95 | 0.95 | 0.95 |
| vC, conflicting volume | 715  |       |      | 1030 |       |      | 1292 | 1749 | 515  | 1130 | 1646 | 358  |
| vC1, stage 1 conf vol  |      |       |      |      |       |      | 1034 | 1034 |      | 612  | 612  |      |
| vC2, stage 2 conf vol  |      |       |      |      |       |      | 258  | 715  |      | 519  | 1034 |      |
| vCu, unblocked vol     | 587  |       |      | 1030 |       |      | 1197 | 1679 | 515  | 1026 | 1570 | 210  |
| tC, single (s)         | 4.1  |       |      | 4.1  |       |      | 7.5  | 6.5  | 6.9  | 7.5  | 6.5  | 6.9  |
| tC, 2 stage (s)        |      |       |      |      |       |      | 6.5  | 5.5  |      | 6.5  | 5.5  |      |
| tF (s)                 | 2.2  |       |      | 2.2  |       |      | 3.5  | 4.0  | 3.3  | 3.5  | 4.0  | 3.3  |
| p0 queue free %        | 100  |       |      | 100  |       |      | 100  | 100  | 100  | 96   | 100  | 99   |
| cM capacity (veh/h)    | 945  |       |      | 682  |       |      | 243  | 266  | 510  | 393  | 276  | 760  |

| Direction, Lane #      | EB 1 | EB 2 | EB 3 | WB 1 | WB 2 | WB 3 | NB 1 | SB 1 |
|------------------------|------|------|------|------|------|------|------|------|
| Volume Total           | 2    | 687  | 343  | 0    | 339  | 376  | 0    | 21   |
| Volume Left            | 2    | 0    | 0    | 0    | 0    | 0    | 0    | 17   |
| Volume Right           | 0    | 0    | 0    | 0    | 0    | 207  | 0    | 4    |
| cSH                    | 945  | 1700 | 1700 | 1700 | 1700 | 1700 | 1700 | 433  |
| Volume to Capacity     | 0.00 | 0.40 | 0.20 | 0.00 | 0.20 | 0.22 | 0.00 | 0.05 |
| Queue Length 95th (ft) | 0    | 0    | 0    | 0    | 0    | 0    | 0    | 4    |
| Control Delay (s)      | 8.8  | 0.0  | 0.0  | 0.0  | 0.0  | 0.0  | 0.0  | 13.7 |
| Lane LOS               | A    |      |      |      |      |      | A    | B    |
| Approach Delay (s)     | 0.0  |      |      | 0.0  |      |      | 0.0  | 13.7 |
| Approach LOS           |      |      |      |      |      |      | A    | B    |

### Intersection Summary

|                                   |      |
|-----------------------------------|------|
| Average Delay                     | 0.2  |
| Intersection Capacity Utilization | Err% |
| ICU Level of Service              | H    |
| Analysis Period (min)             | 15   |

# HCM Unsignalized Intersection Capacity Analysis

## 5: Driveway/Nicewander Way & Route 11

12/8/2015

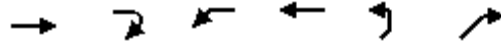


| Movement                          | EBL   | EBT  | EBR   | WBL                  | WBT  | WBR  | NBL  | NBT  | NBR  | SBL  | SBT  | SBR  |
|-----------------------------------|-------|------|-------|----------------------|------|------|------|------|------|------|------|------|
| Lane Configurations               | ↗     | ↕    |       | ↖                    | ↕    |      |      | ↕    |      |      | ↕    |      |
| Traffic Volume (veh/h)            | 4     | 885  | 6     | 1                    | 639  | 1    | 0    | 0    | 1    | 7    | 0    | 8    |
| Future Volume (Veh/h)             | 4     | 885  | 6     | 1                    | 639  | 1    | 0    | 0    | 1    | 7    | 0    | 8    |
| Sign Control                      |       | Free |       |                      | Free |      |      | Stop |      |      | Stop |      |
| Grade                             |       | 0%   |       |                      | 0%   |      |      | 0%   |      |      | 0%   |      |
| Peak Hour Factor                  | 0.90  | 0.90 | 0.90  | 0.95                 | 0.95 | 0.95 | 0.25 | 0.25 | 0.25 | 0.63 | 0.63 | 0.63 |
| Hourly flow rate (vph)            | 5     | 1033 | 7     | 1                    | 706  | 1    | 0    | 0    | 4    | 12   | 0    | 13   |
| Pedestrians                       |       |      |       |                      |      |      |      |      |      |      |      |      |
| Lane Width (ft)                   |       |      |       |                      |      |      |      |      |      |      |      |      |
| Walking Speed (ft/s)              |       |      |       |                      |      |      |      |      |      |      |      |      |
| Percent Blockage                  |       |      |       |                      |      |      |      |      |      |      |      |      |
| Right turn flare (veh)            |       |      |       |                      |      |      |      |      |      |      |      |      |
| Median type                       |       |      |       |                      |      |      |      |      |      |      |      |      |
|                                   | TWLTL |      |       | TWLTL                |      |      |      |      |      |      |      |      |
| Median storage veh                | 2     |      |       | 2                    |      |      |      |      |      |      |      |      |
| Upstream signal (ft)              |       |      |       | 710                  |      |      |      |      |      |      |      |      |
| pX, platoon unblocked             | 0.94  |      |       |                      |      |      | 0.94 | 0.94 |      | 0.94 | 0.94 | 0.94 |
| vC, conflicting volume            | 707   |      |       | 1040                 |      |      | 1414 | 1756 | 520  | 1239 | 1758 | 354  |
| vC1, stage 1 conf vol             |       |      |       |                      |      |      | 1046 | 1046 |      | 708  | 708  |      |
| vC2, stage 2 conf vol             |       |      |       |                      |      |      | 368  | 709  |      | 530  | 1050 |      |
| vCu, unblocked vol                | 550   |      |       | 1040                 |      |      | 1306 | 1670 | 520  | 1119 | 1674 | 173  |
| tC, single (s)                    | 4.1   |      |       | 4.1                  |      |      | 7.5  | 6.5  | 8.9  | 7.5  | 6.5  | 6.9  |
| tC, 2 stage (s)                   |       |      |       |                      |      |      | 6.5  | 5.5  |      | 6.5  | 5.5  |      |
| tF (s)                            | 2.2   |      |       | 2.2                  |      |      | 3.5  | 4.0  | 4.3  | 3.5  | 4.0  | 3.3  |
| p0 queue free %                   | 99    |      |       | 100                  |      |      | 100  | 100  | 99   | 97   | 100  | 98   |
| cM capacity (veh/h)               | 964   |      |       | 676                  |      |      | 235  | 264  | 311  | 359  | 263  | 793  |
| <b>Direction, Lane #</b>          |       |      |       |                      |      |      |      |      |      |      |      |      |
|                                   | EB 1  | EB 2 | EB 3  | WB 1                 | WB 2 | WB 3 | NB 1 | SB 1 |      |      |      |      |
| Volume Total                      | 5     | 689  | 351   | 1                    | 471  | 236  | 4    | 25   |      |      |      |      |
| Volume Left                       | 5     | 0    | 0     | 1                    | 0    | 0    | 0    | 12   |      |      |      |      |
| Volume Right                      | 0     | 0    | 7     | 0                    | 0    | 1    | 4    | 13   |      |      |      |      |
| cSH                               | 964   | 1700 | 1700  | 676                  | 1700 | 1700 | 311  | 501  |      |      |      |      |
| Volume to Capacity                | 0.01  | 0.41 | 0.21  | 0.00                 | 0.28 | 0.14 | 0.01 | 0.05 |      |      |      |      |
| Queue Length 95th (ft)            | 0     | 0    | 0     | 0                    | 0    | 0    | 1    | 4    |      |      |      |      |
| Control Delay (s)                 | 8.8   | 0.0  | 0.0   | 10.3                 | 0.0  | 0.0  | 16.7 | 12.6 |      |      |      |      |
| Lane LOS                          | A     |      |       | B                    |      |      | C    | B    |      |      |      |      |
| Approach Delay (s)                | 0.0   |      |       | 0.0                  |      |      | 16.7 | 12.6 |      |      |      |      |
| Approach LOS                      |       |      |       |                      |      |      | C    | B    |      |      |      |      |
| <b>Intersection Summary</b>       |       |      |       |                      |      |      |      |      |      |      |      |      |
| Average Delay                     |       |      | 0.2   |                      |      |      |      |      |      |      |      |      |
| Intersection Capacity Utilization |       |      | 40.1% | ICU Level of Service |      |      |      | A    |      |      |      |      |
| Analysis Period (min)             |       |      | 15    |                      |      |      |      |      |      |      |      |      |

# HCM Unsignalized Intersection Capacity Analysis

## 7: Kroger Rear & Route 11

12/8/2015



| Movement                          | EBT   | EBR  | WBL   | WBT                  | NEL  | NER  |
|-----------------------------------|-------|------|-------|----------------------|------|------|
| Lane Configurations               | ↑↑    |      | ↵     | ↑↑                   | ↵    |      |
| Traffic Volume (veh/h)            | 835   | 5    | 41    | 647                  | 3    | 53   |
| Future Volume (Veh/h)             | 835   | 5    | 41    | 647                  | 3    | 53   |
| Sign Control                      | Free  |      |       | Free                 | Stop |      |
| Grade                             | 0%    |      |       | 0%                   | 0%   |      |
| Peak Hour Factor                  | 0.80  | 0.88 | 0.92  | 0.92                 | 0.74 | 0.74 |
| Hourly flow rate (vph)            | 1096  | 6    | 47    | 738                  | 4    | 75   |
| Pedestrians                       |       |      |       |                      |      |      |
| Lane Width (ft)                   |       |      |       |                      |      |      |
| Walking Speed (ft/s)              |       |      |       |                      |      |      |
| Percent Blockage                  |       |      |       |                      |      |      |
| Right turn flare (veh)            |       |      |       |                      |      |      |
| Median type                       | TWLTL |      | None  |                      |      |      |
| Median storage (veh)              | 2     |      |       |                      |      |      |
| Upstream signal (ft)              | 254   |      |       |                      |      |      |
| pX, platoon unblocked             |       |      | 0.81  |                      | 0.81 | 0.81 |
| vC, conflicting volume            |       |      | 1102  |                      | 1562 | 551  |
| vC1, stage 1 conf vol             |       |      |       |                      | 1099 |      |
| vC2, stage 2 conf vol             |       |      |       |                      | 463  |      |
| vCu, unblocked vol                |       |      | 668   |                      | 1233 | 0    |
| tC, single (s)                    |       |      | 4.1   |                      | 7.5  | 6.9  |
| tC, 2 stage (s)                   |       |      |       |                      | 6.5  |      |
| tF (s)                            |       |      | 2.2   |                      | 3.8  | 3.3  |
| p0 queue free %                   |       |      | 94    |                      | 99   | 92   |
| cM capacity (veh/h)               |       |      | 747   |                      | 278  | 888  |
| Direction, Lane #                 | EB 1  | EB 2 | WB 1  | WB 2                 | WB 3 | NE 1 |
| Volume Total                      | 731   | 371  | 47    | 369                  | 369  | 79   |
| Volume Left                       | 0     | 0    | 47    | 0                    | 0    | 4    |
| Volume Right                      | 0     | 6    | 0     | 0                    | 0    | 75   |
| cSH                               | 1700  | 1700 | 747   | 1700                 | 1700 | 799  |
| Volume to Capacity                | 0.43  | 0.22 | 0.06  | 0.22                 | 0.22 | 0.10 |
| Queue Length 95th (ft)            | 0     | 0    | 5     | 0                    | 0    | 8    |
| Control Delay (s)                 | 0.0   | 0.0  | 10.1  | 0.0                  | 0.0  | 10.0 |
| Lane LOS                          |       |      | B     |                      |      | A    |
| Approach Delay (s)                | 0.0   |      | 0.6   |                      |      | 10.0 |
| Approach LOS                      |       |      |       |                      |      | A    |
| Intersection Summary              |       |      |       |                      |      |      |
| Average Delay                     |       |      | 0.6   |                      |      |      |
| Intersection Capacity Utilization |       |      | 41.4% | ICU Level of Service | A    |      |
| Analysis Period (min)             | 15    |      |       |                      |      |      |

# HCM Unsignalized Intersection Capacity Analysis

## 8: Wheatland & Route 11

12/8/2015



| Movement                          | EBT  | EBR  | WBL   | WBT  | NBL                  | NBR  |
|-----------------------------------|------|------|-------|------|----------------------|------|
| Lane Configurations               | ↑↑   |      | ↙     | ↑↑   | ↘                    |      |
| Traffic Volume (veh/h)            | 883  | 5    | 1     | 685  | 3                    | 3    |
| Future Volume (Veh/h)             | 883  | 5    | 1     | 685  | 3                    | 3    |
| Sign Control                      | Free |      |       | Free | Stop                 |      |
| Grade                             | 0%   |      |       | 0%   | 0%                   |      |
| Peak Hour Factor                  | 0.88 | 0.88 | 0.92  | 0.92 | 0.75                 | 0.75 |
| Hourly flow rate (vph)            | 1054 | 6    | 1     | 782  | 4                    | 4    |
| Pedestrians                       |      |      |       |      |                      |      |
| Lane Width (ft)                   |      |      |       |      |                      |      |
| Walking Speed (ft/s)              |      |      |       |      |                      |      |
| Percent Blockage                  |      |      |       |      |                      |      |
| Right turn flare (veh)            |      |      |       |      |                      |      |
| Median type                       | None |      | TWLTL |      |                      |      |
| Median storage (veh)              | 2    |      |       |      |                      |      |
| Upstream signal (ft)              | 335  |      |       |      |                      |      |
| pX, platoon unblocked             |      |      | 0.83  |      | 0.83                 | 0.83 |
| vC, conflicting volume            |      |      | 1060  |      | 1450                 | 530  |
| vC1, stage 1 conf vol             |      |      |       |      | 1057                 |      |
| vC2, stage 2 conf vol             |      |      |       |      | 393                  |      |
| vCu, unblocked vol                |      |      | 658   |      | 1129                 | 19   |
| tC, single (s)                    |      |      | 4.1   |      | 6.8                  | 7.6  |
| tC, 2 stage (s)                   |      |      |       |      | 5.8                  |      |
| tF (s)                            |      |      | 2.2   |      | 3.5                  | 3.6  |
| p0 queue free %                   |      |      | 100   |      | 99                   | 99   |
| cM capacity (veh/h)               |      |      | 778   |      | 361                  | 798  |
| Direction, Lane #                 | EB 1 | EB 2 | WB 1  | WB 2 | WB 3                 | NB 1 |
| Volume Total                      | 703  | 357  | 1     | 391  | 391                  | 8    |
| Volume Left                       | 0    | 0    | 1     | 0    | 0                    | 4    |
| Volume Right                      | 0    | 6    | 0     | 0    | 0                    | 4    |
| cSH                               | 1700 | 1700 | 778   | 1700 | 1700                 | 497  |
| Volume to Capacity                | 0.41 | 0.21 | 0.00  | 0.23 | 0.23                 | 0.02 |
| Queue Length 95th (ft)            | 0    | 0    | 0     | 0    | 0                    | 1    |
| Control Delay (s)                 | 0.0  | 0.0  | 9.6   | 0.0  | 0.0                  | 12.4 |
| Lane LOS                          | A    |      |       | B    |                      |      |
| Approach Delay (s)                | 0.0  |      | 0.0   |      | 12.4                 |      |
| Approach LOS                      |      |      |       | B    |                      |      |
| Intersection Summary              |      |      |       |      |                      |      |
| Average Delay                     |      |      | 0.1   |      |                      |      |
| Intersection Capacity Utilization |      |      | 35.8% |      | ICU Level of Service |      |
| Analysis Period (min)             |      |      | 15    |      |                      |      |
|                                   |      |      | A     |      |                      |      |

# HCM Unsignalized Intersection Capacity Analysis

## 9: University Park/Roca's & Route 11

12/8/2015



| Movement               | EBL  | EBT   | EBR  | WBL  | WBT  | WBR  | NBL  | NBT  | NBR  | SBL  | SBT  | SBR  |
|------------------------|------|-------|------|------|------|------|------|------|------|------|------|------|
| Lane Configurations    |      |       |      |      |      |      |      |      |      |      |      |      |
| Traffic Volume (veh/h) | 1    | 881   | 9    | 9    | 695  | 0    | 1    | 0    | 1    | 0    | 0    | 0    |
| Future Volume (Veh/h)  | 1    | 881   | 9    | 9    | 695  | 0    | 1    | 0    | 1    | 0    | 0    | 0    |
| Sign Control           |      | Free  |      |      | Free |      |      | Stop |      |      | Stop |      |
| Grade                  |      | 0%    |      |      | 0%   |      |      | 0%   |      |      | 0%   |      |
| Peak Hour Factor       | 0.90 | 0.90  | 0.90 | 0.98 | 0.98 | 0.98 | 0.25 | 0.25 | 0.25 | 1.00 | 1.00 | 1.00 |
| Hourly flow rate (vph) | 1    | 1028  | 11   | 10   | 745  | 0    | 4    | 0    | 4    | 0    | 0    | 0    |
| Pedestrians            |      |       |      |      |      |      |      |      |      |      |      |      |
| Lane Width (ft)        |      |       |      |      |      |      |      |      |      |      |      |      |
| Walking Speed (ft/s)   |      |       |      |      |      |      |      |      |      |      |      |      |
| Percent Blockage       |      |       |      |      |      |      |      |      |      |      |      |      |
| Right turn flare (veh) |      |       |      |      |      |      |      |      |      |      |      |      |
| Median type            |      | TWLTL |      |      | None |      |      |      |      |      |      |      |
| Median storage veh     |      | 2     |      |      |      |      |      |      |      |      |      |      |
| Upstream signal (ft)   |      | 792   |      |      |      |      |      |      |      |      |      |      |
| pX, platoon unblocked  |      |       |      | 0.86 |      |      | 0.86 | 0.86 | 0.86 | 0.86 | 0.86 |      |
| vC, conflicting volume | 745  |       |      | 1039 |      |      | 1428 | 1800 | 520  | 1285 | 1806 | 372  |
| vC1, stage 1 conf vol  |      |       |      |      |      |      | 1036 | 1036 |      | 765  | 765  |      |
| vC2, stage 2 conf vol  |      |       |      |      |      |      | 392  | 765  |      | 520  | 1041 |      |
| vCu, unblocked vol     | 745  |       |      | 730  |      |      | 1180 | 1611 | 129  | 1015 | 1618 | 372  |
| tC, single (s)         | 6.1  |       |      | 4.1  |      |      | 7.5  | 6.5  | 6.9  | 7.5  | 6.5  | 6.9  |
| tC, 2 stage (s)        |      |       |      |      |      |      | 6.5  | 5.5  |      | 6.5  | 5.5  |      |
| tF (s)                 | 3.2  |       |      | 2.2  |      |      | 3.5  | 4.0  | 3.3  | 3.5  | 4.0  | 3.3  |
| p0 queue free %        | 100  |       |      | 99   |      |      | 99   | 100  | 99   | 100  | 100  | 100  |
| cM capacity (veh/h)    | 435  |       |      | 763  |      |      | 302  | 283  | 781  | 337  | 278  | 631  |

| Direction, Lane #      | EB 1 | EB 2 | EB 3 | WB 1 | WB 2 | WB 3 | NB 1 | SB 1 |
|------------------------|------|------|------|------|------|------|------|------|
| Volume Total           | 1    | 685  | 354  | 10   | 497  | 248  | 8    | 0    |
| Volume Left            | 1    | 0    | 0    | 10   | 0    | 0    | 4    | 0    |
| Volume Right           | 0    | 0    | 11   | 0    | 0    | 0    | 4    | 0    |
| cSH                    | 435  | 1700 | 1700 | 763  | 1700 | 1700 | 435  | 1700 |
| Volume to Capacity     | 0.00 | 0.40 | 0.21 | 0.01 | 0.29 | 0.15 | 0.02 | 0.00 |
| Queue Length 95th (ft) | 0    | 0    | 0    | 1    | 0    | 0    | 1    | 0    |
| Control Delay (s)      | 13.3 | 0.0  | 0.0  | 9.8  | 0.0  | 0.0  | 13.4 | 0.0  |
| Lane LOS               | B    |      |      | A    |      |      | B    | A    |
| Approach Delay (s)     | 0.0  |      |      | 0.1  |      |      | 13.4 | 0.0  |
| Approach LOS           |      |      |      |      |      |      | B    | A    |

### Intersection Summary

|                                   |       |
|-----------------------------------|-------|
| Average Delay                     | 0.1   |
| Intersection Capacity Utilization | 35.9% |
| ICU Level of Service              | A     |
| Analysis Period (min)             | 15    |



# HCM Unsignalized Intersection Capacity Analysis

## 10: Route 11 & Nesslerod

12/8/2015

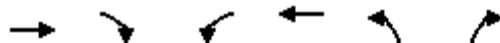


| Movement                          | EBL  | EBT    | WBT    | WBR  | SBL                  | SBR  |
|-----------------------------------|------|--------|--------|------|----------------------|------|
| Lane Configurations               |      |        |        |      |                      |      |
| Traffic Volume (veh/h)            | 1    | 881    | 704    | 0    | 1                    | 0    |
| Future Volume (Veh/h)             | 1    | 881    | 704    | 0    | 1                    | 0    |
| Sign Control                      |      | Free   | Free   |      | Stop                 |      |
| Grade                             |      | 0%     | 0%     |      | 0%                   |      |
| Peak Hour Factor                  | 0.90 | 0.90   | 0.98   | 0.98 | 0.25                 | 0.25 |
| Hourly flow rate (vph)            | 1    | 1028   | 754    | 0    | 4                    | 0    |
| Pedestrians                       |      |        |        |      |                      |      |
| Lane Width (ft)                   |      |        |        |      |                      |      |
| Walking Speed (ft/s)              |      |        |        |      |                      |      |
| Percent Blockage                  |      |        |        |      |                      |      |
| Right turn flare (veh)            |      |        |        |      |                      |      |
| Median type                       |      | Raised | Raised |      |                      |      |
| Median storage veh                |      | 1      | 1      |      |                      |      |
| Upstream signal (ft)              |      | 1287   |        |      |                      |      |
| pX, platoon unblocked             |      |        |        |      | 0.92                 |      |
| vC, conflicting volume            | 754  |        |        |      | 1270                 | 377  |
| vC1, stage 1 conf vol             |      |        |        |      | 754                  |      |
| vC2, stage 2 conf vol             |      |        |        |      | 516                  |      |
| vCu, unblocked vol                | 754  |        |        |      | 1111                 | 377  |
| tC, single (s)                    | 4.1  |        |        |      | 6.8                  | 6.9  |
| tC, 2 stage (s)                   |      |        |        |      | 5.8                  |      |
| tF (s)                            | 2.2  |        |        |      | 3.5                  | 3.3  |
| p0 queue free %                   | 100  |        |        |      | 99                   | 100  |
| cM capacity (veh/h)               | 865  |        |        |      | 320                  | 626  |
| Direction, Lane #                 | EB 1 | EB 2   | EB 3   | WB 1 | WB 2                 | SB 1 |
| Volume Total                      | 1    | 514    | 514    | 503  | 251                  | 4    |
| Volume Left                       | 1    | 0      | 0      | 0    | 0                    | 4    |
| Volume Right                      | 0    | 0      | 0      | 0    | 0                    | 0    |
| cSH                               | 865  | 1700   | 1700   | 1700 | 1700                 | 320  |
| Volume to Capacity                | 0.00 | 0.30   | 0.30   | 0.30 | 0.15                 | 0.01 |
| Queue Length 95th (ft)            | 0    | 0      | 0      | 0    | 0                    | 1    |
| Control Delay (s)                 | 9.2  | 0.0    | 0.0    | 0.0  | 0.0                  | 16.4 |
| Lane LOS                          | A    |        |        |      |                      | C    |
| Approach Delay (s)                | 0.0  |        |        | 0.0  |                      | 16.4 |
| Approach LOS                      |      |        |        |      |                      | C    |
| Intersection Summary              |      |        |        |      |                      |      |
| Average Delay                     |      |        | 0.0    |      |                      |      |
| Intersection Capacity Utilization |      |        | 35.6%  |      | ICU Level of Service | A    |
| Analysis Period (min)             |      |        | 15     |      |                      |      |

# HCM Unsignalized Intersection Capacity Analysis

## 11: Hazel Hollow Rd & Route 11

12/8/2015



| Movement                          | EBT    | EBR  | WBL    | WBT                  | NBL  | NBR  |      |
|-----------------------------------|--------|------|--------|----------------------|------|------|------|
| Lane Configurations               | ↑↑     | ↑    | ↓      | ↑↑                   | ↓    | ↓    |      |
| Traffic Volume (veh/h)            | 878    | 4    | 12     | 683                  | 21   | 28   |      |
| Future Volume (Veh/h)             | 878    | 4    | 12     | 683                  | 21   | 28   |      |
| Sign Control                      | Free   |      |        | Free                 | Stop |      |      |
| Grade                             | 0%     |      |        | 0%                   | 0%   |      |      |
| Peak Hour Factor                  | 0.90   | 0.90 | 0.98   | 0.98                 | 0.82 | 0.82 |      |
| Hourly flow rate (vph)            | 1024   | 5    | 13     | 732                  | 27   | 36   |      |
| Pedestrians                       |        |      |        |                      |      |      |      |
| Lane Width (ft)                   |        |      |        |                      |      |      |      |
| Walking Speed (ft/s)              |        |      |        |                      |      |      |      |
| Percent Blockage                  |        |      |        |                      |      |      |      |
| Right turn flare (veh)            |        |      |        |                      |      |      |      |
| Median type                       | Raised |      | Raised |                      |      |      |      |
| Median storage veh                | 1      |      | 1      |                      |      |      |      |
| Upstream signal (ft)              |        |      |        |                      |      |      |      |
| pX, platoon unblocked             |        |      |        |                      |      |      |      |
| vC, conflicting volume            |        |      | 1029   |                      | 1416 | 512  |      |
| vC1, stage 1 conf vol             |        |      |        |                      | 1024 |      |      |
| vC2, stage 2 conf vol             |        |      |        |                      | 392  |      |      |
| vCu, unblocked vol                |        |      | 1029   |                      | 1416 | 512  |      |
| tC, single (s)                    |        |      | 4.4    |                      | 6.8  | 6.9  |      |
| tC, 2 stage (s)                   |        |      |        |                      | 5.8  |      |      |
| tF (s)                            |        |      | 2.4    |                      | 3.5  | 3.3  |      |
| p0 queue free %                   |        |      | 98     |                      | 89   | 93   |      |
| cM capacity (veh/h)               |        |      | 588    |                      | 240  | 512  |      |
| Direction, Lane #                 | EB 1   | EB 2 | EB 3   | WB 1                 | WB 2 | WB 3 | NB 1 |
| Volume Total                      | 512    | 512  | 5      | 13                   | 366  | 366  | 63   |
| Volume Left                       | 0      | 0    | 0      | 13                   | 0    | 0    | 27   |
| Volume Right                      | 0      | 0    | 5      | 0                    | 0    | 0    | 36   |
| cSH                               | 1700   | 1700 | 1700   | 588                  | 1700 | 1700 | 345  |
| Volume to Capacity                | 0.30   | 0.30 | 0.00   | 0.02                 | 0.22 | 0.22 | 0.18 |
| Queue Length 95th (ft)            | 0      | 0    | 0      | 2                    | 0    | 0    | 16   |
| Control Delay (s)                 | 0.0    | 0.0  | 0.0    | 11.3                 | 0.0  | 0.0  | 17.7 |
| Lane LOS                          |        |      |        | B                    | C    |      |      |
| Approach Delay (s)                | 0.0    |      |        | 0.2                  |      |      | 17.7 |
| Approach LOS                      |        |      |        |                      |      |      | C    |
| Intersection Summary              |        |      |        |                      |      |      |      |
| Average Delay                     |        |      | 0.7    |                      |      |      |      |
| Intersection Capacity Utilization |        |      | 35.5%  | ICU Level of Service |      | A    |      |
| Analysis Period (min)             |        |      | 15     |                      |      |      |      |

# HCM Signalized Intersection Capacity Analysis

## 2: Route 11 & Route 114

12/8/2015



| Movement               | EBL   | EBT  | WBT  | WBR   | SBL   | SBR  |
|------------------------|-------|------|------|-------|-------|------|
| Lane Configurations    |       |      |      |       |       |      |
| Traffic Volume (vph)   | 214   | 292  | 256  | 488   | 660   | 179  |
| Future Volume (vph)    | 214   | 292  | 256  | 488   | 660   | 179  |
| Ideal Flow (vphpl)     | 1900  | 1900 | 1900 | 1900  | 1900  | 1900 |
| Total Lost time (s)    | 5.5   | 6.0  | 6.0  | 4.0   | 5.5   | 4.0  |
| Lane Util. Factor      | 1.00  | 0.95 | 0.95 | 1.00  | 0.97  | 1.00 |
| Frt                    | 1.00  | 1.00 | 1.00 | 0.85  | 1.00  | 0.85 |
| Flt Protected          | 0.95  | 1.00 | 1.00 | 1.00  | 0.95  | 1.00 |
| Satd. Flow (prot)      | 1719  | 3505 | 3505 | 1583  | 3433  | 1509 |
| Flt Permitted          | 0.95  | 1.00 | 1.00 | 1.00  | 0.95  | 1.00 |
| Satd. Flow (perm)      | 1719  | 3505 | 3505 | 1583  | 3433  | 1509 |
| Peak-hour factor, PHF  | 0.81  | 0.81 | 0.95 | 0.95  | 0.86  | 0.86 |
| Growth Factor (vph)    | 105%  | 105% | 105% | 105%  | 105%  | 105% |
| Adj. Flow (vph)        | 277   | 379  | 283  | 539   | 806   | 219  |
| RTOR Reduction (vph)   | 0     | 0    | 0    | 0     | 0     | 0    |
| Lane Group Flow (vph)  | 277   | 379  | 283  | 539   | 806   | 219  |
| Heavy Vehicles (%)     | 5%    | 3%   | 3%   | 2%    | 2%    | 7%   |
| Turn Type              | Prot  | NA   | NA   | Free  | Prot  | Free |
| Protected Phases       | 1     | 6    | 2    |       | 4     |      |
| Permitted Phases       |       |      |      | Free  |       | Free |
| Actuated Green, G (s)  | 22.0  | 43.9 | 16.4 | 86.0  | 30.6  | 86.0 |
| Effective Green, g (s) | 22.0  | 43.9 | 16.4 | 86.0  | 30.6  | 86.0 |
| Actuated g/C Ratio     | 0.26  | 0.51 | 0.19 | 1.00  | 0.36  | 1.00 |
| Clearance Time (s)     | 5.5   | 6.0  | 6.0  |       | 5.5   |      |
| Vehicle Extension (s)  | 4.0   | 4.5  | 4.5  |       | 4.5   |      |
| Lane Grp Cap (vph)     | 439   | 1789 | 668  | 1583  | 1221  | 1509 |
| v/s Ratio Prot         | c0.16 | 0.11 | 0.08 |       | c0.23 |      |
| v/s Ratio Perm         |       |      |      | c0.34 |       | 0.15 |
| v/c Ratio              | 0.63  | 0.21 | 0.42 | 0.34  | 0.66  | 0.15 |
| Uniform Delay, d1      | 28.4  | 11.6 | 30.6 | 0.0   | 23.3  | 0.0  |
| Progression Factor     | 1.00  | 1.00 | 1.00 | 1.00  | 1.00  | 1.00 |
| Incremental Delay, d2  | 3.3   | 0.1  | 0.8  | 0.6   | 1.6   | 0.2  |
| Delay (s)              | 31.7  | 11.7 | 31.4 | 0.6   | 24.9  | 0.2  |
| Level of Service       | C     | B    | C    | A     | C     | A    |
| Approach Delay (s)     |       | 20.1 | 11.2 |       | 19.7  |      |
| Approach LOS           |       | C    | B    |       | B     |      |

### Intersection Summary

|                                   |       |                           |      |
|-----------------------------------|-------|---------------------------|------|
| HCM 2000 Control Delay            | 17.0  | HCM 2000 Level of Service | B    |
| HCM 2000 Volume to Capacity ratio | 0.60  |                           |      |
| Actuated Cycle Length (s)         | 86.0  | Sum of lost time (s)      | 17.0 |
| Intersection Capacity Utilization | 58.1% | ICU Level of Service      | B    |
| Analysis Period (min)             | 15    |                           |      |

c Critical Lane Group

# HCM Signalized Intersection Capacity Analysis

## 6: Kroger/The Rocks & Route 11

12/8/2015



| Movement               | EBL  | EBT   | EBR  | WBL   | WBT   | WBR  | NBL   | NBT  | NBR  | SBL  | SBT  | SBR  |
|------------------------|------|-------|------|-------|-------|------|-------|------|------|------|------|------|
| Lane Configurations    | ↖    | ↗     |      | ↖     | ↗     |      | ↖     | ↗    |      |      | ↖    | ↗    |
| Traffic Volume (vph)   | 32   | 821   | 129  | 84    | 765   | 1    | 101   | 5    | 109  | 36   | 10   | 74   |
| Future Volume (vph)    | 32   | 821   | 129  | 84    | 765   | 1    | 101   | 5    | 109  | 36   | 10   | 74   |
| Ideal Flow (vphpl)     | 1900 | 1900  | 1900 | 1900  | 1900  | 1900 | 1900  | 1900 | 1900 | 1900 | 1900 | 1900 |
| Total Lost time (s)    | 5.5  | 6.0   |      | 5.5   | 6.0   |      | 5.5   | 5.5  |      |      | 5.5  | 5.5  |
| Lane Util. Factor      | 1.00 | 0.95  |      | 1.00  | 0.95  |      | 1.00  | 1.00 |      |      | 1.00 | 1.00 |
| Frt                    | 1.00 | 0.98  |      | 1.00  | 1.00  |      | 1.00  | 0.86 |      |      | 1.00 | 0.85 |
| Flt Protected          | 0.95 | 1.00  |      | 0.95  | 1.00  |      | 0.95  | 1.00 |      |      | 0.96 | 1.00 |
| Satd. Flow (prot)      | 1805 | 3467  |      | 1736  | 3535  |      | 1787  | 1569 |      |      | 1829 | 1599 |
| Flt Permitted          | 0.95 | 1.00  |      | 0.95  | 1.00  |      | 0.71  | 1.00 |      |      | 0.69 | 1.00 |
| Satd. Flow (perm)      | 1805 | 3467  |      | 1736  | 3535  |      | 1341  | 1569 |      |      | 1316 | 1599 |
| Peak-hour factor, PHF  | 0.95 | 0.95  | 0.95 | 0.90  | 0.90  | 0.90 | 0.94  | 0.94 | 0.94 | 0.71 | 0.71 | 0.71 |
| Growth Factor (vph)    | 105% | 105%  | 105% | 105%  | 105%  | 105% | 105%  | 105% | 105% | 105% | 105% | 105% |
| Adj. Flow (vph)        | 35   | 907   | 143  | 98    | 892   | 1    | 113   | 6    | 122  | 53   | 15   | 109  |
| RTOR Reduction (vph)   | 0    | 10    | 0    | 0     | 0     | 0    | 0     | 99   | 0    | 0    | 0    | 89   |
| Lane Group Flow (vph)  | 35   | 1040  | 0    | 98    | 894   | 0    | 113   | 29   | 0    | 0    | 68   | 20   |
| Heavy Vehicles (%)     | 0%   | 2%    | 2%   | 4%    | 2%    | 100% | 1%    | 0%   | 4%   | 0%   | 0%   | 1%   |
| Turn Type              | Prot | NA    |      | Prot  | NA    |      | Perm  | NA   |      | Perm | NA   | Perm |
| Protected Phases       | 1    | 6     |      | 5     | 2     |      |       | 8    |      |      | 4    |      |
| Permitted Phases       |      |       |      |       |       |      | 8     |      |      | 4    |      | 4    |
| Actuated Green, G (s)  | 4.4  | 34.9  |      | 8.5   | 39.0  |      | 13.9  | 13.9 |      |      | 13.9 | 13.9 |
| Effective Green, g (s) | 4.4  | 34.9  |      | 8.5   | 39.0  |      | 13.9  | 13.9 |      |      | 13.9 | 13.9 |
| Actuated g/C Ratio     | 0.06 | 0.47  |      | 0.11  | 0.52  |      | 0.19  | 0.19 |      |      | 0.19 | 0.19 |
| Clearance Time (s)     | 5.5  | 6.0   |      | 5.5   | 6.0   |      | 5.5   | 5.5  |      |      | 5.5  | 5.5  |
| Vehicle Extension (s)  | 3.5  | 4.0   |      | 3.5   | 4.0   |      | 4.0   | 4.0  |      |      | 4.0  | 4.0  |
| Lane Grp Cap (vph)     | 106  | 1628  |      | 198   | 1855  |      | 250   | 293  |      |      | 246  | 299  |
| v/s Ratio Prot         | 0.02 | c0.30 |      | c0.06 | c0.25 |      |       | 0.02 |      |      |      |      |
| v/s Ratio Perm         |      |       |      |       |       |      | c0.08 |      |      |      | 0.05 | 0.01 |
| v/c Ratio              | 0.33 | 0.64  |      | 0.49  | 0.48  |      | 0.45  | 0.10 |      |      | 0.28 | 0.07 |
| Uniform Delay, d1      | 33.5 | 14.9  |      | 30.9  | 11.2  |      | 26.8  | 25.0 |      |      | 25.9 | 24.9 |
| Progression Factor     | 1.00 | 1.00  |      | 1.00  | 1.00  |      | 1.00  | 1.00 |      |      | 1.00 | 1.00 |
| Incremental Delay, d2  | 2.2  | 0.9   |      | 2.3   | 0.3   |      | 1.8   | 0.2  |      |      | 0.8  | 0.1  |
| Delay (s)              | 35.7 | 15.9  |      | 33.2  | 11.5  |      | 28.6  | 25.2 |      |      | 26.7 | 25.0 |
| Level of Service       | D    | B     |      | C     | B     |      | C     | C    |      |      | C    | C    |
| Approach Delay (s)     |      | 16.5  |      |       | 13.6  |      |       | 26.8 |      |      | 25.7 |      |
| Approach LOS           |      | B     |      |       | B     |      |       | C    |      |      | C    |      |

### Intersection Summary

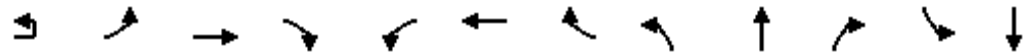
|                                   |       |                           |      |
|-----------------------------------|-------|---------------------------|------|
| HCM 2000 Control Delay            | 17.0  | HCM 2000 Level of Service | B    |
| HCM 2000 Volume to Capacity ratio | 0.58  |                           |      |
| Actuated Cycle Length (s)         | 74.3  | Sum of lost time (s)      | 17.0 |
| Intersection Capacity Utilization | 59.9% | ICU Level of Service      | B    |
| Analysis Period (min)             | 15    |                           |      |

c Critical Lane Group

# HCM Unsignalized Intersection Capacity Analysis

## 3: Cherry Ct/Driveway & Route 11

12/8/2015



| Movement                          | EBU         | EBL         | EBT         | EBR         | WBL         | WBT                  | WBR         | NBL  | NBT  | NBR  | SBL  | SBT  |
|-----------------------------------|-------------|-------------|-------------|-------------|-------------|----------------------|-------------|------|------|------|------|------|
| Lane Configurations               |             |             | ↔           |             | ↔           | ↔                    |             |      | ↔    |      |      | ↔    |
| Traffic Volume (veh/h)            | 1           | 5           | 943         | 3           | 1           | 742                  | 1           | 3    | 0    | 1    | 2    | 0    |
| Future Volume (Veh/h)             | 1           | 5           | 943         | 3           | 1           | 742                  | 1           | 3    | 0    | 1    | 2    | 0    |
| Sign Control                      |             |             | Free        |             |             | Free                 |             |      | Stop |      |      | Stop |
| Grade                             |             |             | 0%          |             |             | 0%                   |             |      | 0%   |      |      | 0%   |
| Peak Hour Factor                  | 0.93        | 0.93        | 0.93        | 0.93        | 0.93        | 0.93                 | 0.93        | 0.50 | 0.50 | 0.50 | 0.59 | 0.59 |
| Hourly flow rate (vph)            | 0           | 6           | 1065        | 3           | 1           | 838                  | 1           | 6    | 0    | 2    | 4    | 0    |
| Pedestrians                       |             |             |             |             |             |                      |             |      |      |      |      |      |
| Lane Width (ft)                   |             |             |             |             |             |                      |             |      |      |      |      |      |
| Walking Speed (ft/s)              |             |             |             |             |             |                      |             |      |      |      |      |      |
| Percent Blockage                  |             |             |             |             |             |                      |             |      |      |      |      |      |
| Right turn flare (veh)            |             |             |             |             |             |                      |             |      |      |      |      |      |
| Median type                       |             |             | None        |             |             | TWLTL                |             |      |      |      |      |      |
| Median storage (veh)              |             |             |             |             |             | 2                    |             |      |      |      |      |      |
| Upstream signal (ft)              |             |             | 779         |             |             |                      |             |      |      |      |      |      |
| pX, platoon unblocked             | 0.00        |             |             |             | 0.98        |                      |             | 0.98 | 0.98 | 0.98 | 0.98 | 0.98 |
| vC, conflicting volume            | 0           | 839         |             |             | 1068        |                      |             | 1530 | 1920 | 534  | 1387 | 1920 |
| vC1, stage 1 conf vol             |             |             |             |             |             |                      |             | 1078 | 1078 |      | 840  | 840  |
| vC2, stage 2 conf vol             |             |             |             |             |             |                      |             | 451  | 841  |      | 546  | 1080 |
| vCu, unblocked vol                | 0           | 839         |             |             | 1025        |                      |             | 1496 | 1895 | 479  | 1351 | 1896 |
| tC, single (s)                    | 0.0         | 4.1         |             |             | 4.1         |                      |             | 7.5  | 6.5  | 6.9  | 7.5  | 6.5  |
| tC, 2 stage (s)                   |             |             |             |             |             |                      |             | 6.5  | 5.5  |      | 6.5  | 5.5  |
| tF (s)                            | 0.0         | 2.2         |             |             | 2.2         |                      |             | 3.5  | 4.0  | 3.3  | 3.5  | 4.0  |
| p0 queue free %                   | 0           | 99          |             |             | 100         |                      |             | 97   | 100  | 100  | 99   | 100  |
| cM capacity (veh/h)               | 0           | 804         |             |             | 670         |                      |             | 224  | 239  | 527  | 287  | 239  |
| <b>Direction, Lane #</b>          | <b>EB 1</b> | <b>EB 2</b> | <b>WB 1</b> | <b>WB 2</b> | <b>WB 3</b> | <b>NB 1</b>          | <b>SB 1</b> |      |      |      |      |      |
| Volume Total                      | 538         | 536         | 1           | 559         | 280         | 8                    | 34          |      |      |      |      |      |
| Volume Left                       | 6           | 0           | 1           | 0           | 0           | 6                    | 4           |      |      |      |      |      |
| Volume Right                      | 0           | 3           | 0           | 0           | 1           | 2                    | 30          |      |      |      |      |      |
| cSH                               | 804         | 1700        | 670         | 1700        | 1700        | 262                  | 512         |      |      |      |      |      |
| Volume to Capacity                | 0.01        | 0.32        | 0.00        | 0.33        | 0.16        | 0.03                 | 0.07        |      |      |      |      |      |
| Queue Length 95th (ft)            | 1           | 0           | 0           | 0           | 0           | 2                    | 5           |      |      |      |      |      |
| Control Delay (s)                 | 0.2         | 0.0         | 10.4        | 0.0         | 0.0         | 19.2                 | 12.5        |      |      |      |      |      |
| Lane LOS                          | A           |             | B           |             |             | C                    | B           |      |      |      |      |      |
| Approach Delay (s)                | 0.1         |             | 0.0         |             |             | 19.2                 | 12.5        |      |      |      |      |      |
| Approach LOS                      |             |             |             |             |             | C                    | B           |      |      |      |      |      |
| <b>Intersection Summary</b>       |             |             |             |             |             |                      |             |      |      |      |      |      |
| Average Delay                     |             |             | 0.4         |             |             |                      |             |      |      |      |      |      |
| Intersection Capacity Utilization |             |             | 41.9%       |             |             | ICU Level of Service |             |      | A    |      |      |      |
| Analysis Period (min)             |             |             | 15          |             |             |                      |             |      |      |      |      |      |

# HCM Unsignalized Intersection Capacity Analysis

## 3: Cherry Ct/Driveway & Route 11

12/8/2015

| Movement               | SBR  |
|------------------------|------|
| Lane Configurations    |      |
| Traffic Volume (veh/h) | 17   |
| Future Volume (Veh/h)  | 17   |
| Sign Control           |      |
| Grade                  |      |
| Peak Hour Factor       | 0.59 |
| Hourly flow rate (vph) | 30   |
| Pedestrians            |      |
| Lane Width (ft)        |      |
| Walking Speed (ft/s)   |      |
| Percent Blockage       |      |
| Right turn flare (veh) |      |
| Median type            |      |
| Median storage veh     |      |
| Upstream signal (ft)   |      |
| pX, platoon unblocked  |      |
| vC, conflicting volume | 420  |
| vC1, stage 1 conf vol  |      |
| vC2, stage 2 conf vol  |      |
| vCu, unblocked vol     | 420  |
| tC, single (s)         | 7.0  |
| tC, 2 stage (s)        |      |
| tF (s)                 | 3.4  |
| p0 queue free %        | 95   |
| cM capacity (veh/h)    | 571  |
| Direction, Lane #      |      |

# HCM Unsignalized Intersection Capacity Analysis

## 4: Route 11 & Belspring Rd

12/8/2015



| Movement               | EBL   | EBT  | EBR  | WBL  | WBT   | WBR  | NBL  | NBT  | NBR  | SBL  | SBT  | SBR  |      |
|------------------------|-------|------|------|------|-------|------|------|------|------|------|------|------|------|
| Lane Configurations    |       |      |      |      |       |      |      |      |      |      |      |      |      |
| Traffic Volume (veh/h) | 7     | 969  | 1    | 1    | 773   | 178  | 0    | 0    | 5    | 17   | 0    | 1    |      |
| Future Volume (Veh/h)  | 7     | 969  | 1    | 1    | 773   | 178  | 0    | 0    | 5    | 17   | 0    | 1    |      |
| Sign Control           | Free  |      |      | Free |       |      | Stop |      |      | Stop |      |      |      |
| Grade                  | 0%    |      |      | 0%   |       |      | 0%   |      |      | 0%   |      |      |      |
| Peak Hour Factor       | 0.93  | 0.93 | 0.93 | 0.93 | 0.93  | 0.93 | 0.63 | 0.63 | 0.63 | 0.65 | 0.65 | 0.65 |      |
| Hourly flow rate (vph) | 8     | 1094 | 1    | 1    | 873   | 201  | 0    | 0    | 8    | 27   | 0    | 2    |      |
| Pedestrians            |       |      |      |      |       |      |      |      |      |      |      |      |      |
| Lane Width (ft)        |       |      |      |      |       |      |      |      |      |      |      |      |      |
| Walking Speed (ft/s)   |       |      |      |      |       |      |      |      |      |      |      |      |      |
| Percent Blockage       |       |      |      |      |       |      |      |      |      |      |      |      |      |
| Right turn flare (veh) |       |      |      |      |       |      |      |      |      |      |      |      |      |
| Median type            | TWLTL |      |      |      | TWLTL |      |      |      |      |      |      |      |      |
| Median storage (veh)   | 2     |      |      |      | 2     |      |      |      |      |      |      |      |      |
| Upstream signal (ft)   |       |      |      |      | 842   |      |      |      |      |      |      |      |      |
| pX, platoon unblocked  | 0.86  |      |      |      |       |      | 0.86 | 0.86 |      |      | 0.86 | 0.86 | 0.86 |
| vC, conflicting volume | 1074  |      |      |      | 1095  |      |      | 1551 | 2186 | 548  | 1546 | 2086 | 537  |
| vC1, stage 1 conf vol  |       |      |      |      |       |      | 1110 | 1110 |      |      | 976  | 976  |      |
| vC2, stage 2 conf vol  |       |      |      |      |       |      | 440  | 1076 |      |      | 571  | 1111 |      |
| vCu, unblocked vol     | 752   |      |      |      | 1095  |      |      | 1309 | 2050 | 548  | 1303 | 1934 | 125  |
| tC, single (s)         | 4.1   |      |      |      | 4.1   |      |      | 7.5  | 6.5  | 6.9  | 7.6  | 6.5  | 6.9  |
| tC, 2 stage (s)        |       |      |      |      |       |      | 6.5  | 5.5  |      |      | 6.6  | 5.5  |      |
| tF (s)                 | 2.2   |      |      |      | 2.2   |      |      | 3.5  | 4.0  | 3.3  | 3.6  | 4.0  | 3.3  |
| p0 queue free %        | 99    |      |      |      | 100   |      |      | 100  | 100  | 98   | 91   | 100  | 100  |
| cM capacity (veh/h)    | 743   |      |      |      | 645   |      |      | 216  | 219  | 486  | 289  | 231  | 778  |

| Direction, Lane #      | EB 1 | EB 2 | EB 3 | WB 1 | WB 2 | WB 3 | NB 1 | SB 1 |
|------------------------|------|------|------|------|------|------|------|------|
| Volume Total           | 8    | 729  | 366  | 1    | 582  | 492  | 8    | 29   |
| Volume Left            | 8    | 0    | 0    | 1    | 0    | 0    | 0    | 27   |
| Volume Right           | 0    | 0    | 1    | 0    | 0    | 201  | 8    | 2    |
| cSH                    | 743  | 1700 | 1700 | 645  | 1700 | 1700 | 486  | 303  |
| Volume to Capacity     | 0.01 | 0.43 | 0.22 | 0.00 | 0.34 | 0.29 | 0.02 | 0.10 |
| Queue Length 95th (ft) | 1    | 0    | 0    | 0    | 0    | 0    | 1    | 8    |
| Control Delay (s)      | 9.9  | 0.0  | 0.0  | 10.6 | 0.0  | 0.0  | 12.5 | 18.2 |
| Lane LOS               | A    |      |      | B    |      |      | B    | C    |
| Approach Delay (s)     | 0.1  |      |      | 0.0  |      |      | 12.5 | 18.2 |
| Approach LOS           |      |      |      |      |      |      | B    | C    |

| Intersection Summary              |      |                      |
|-----------------------------------|------|----------------------|
| Average Delay                     | 0.3  |                      |
| Intersection Capacity Utilization | Err% | ICU Level of Service |
| Analysis Period (min)             | 15   | H                    |

# HCM Unsignalized Intersection Capacity Analysis

## 5: Driveway/Nicewander Way & Route 11

12/8/2015



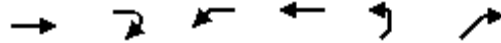
| Movement                          | EBL   | EBT  | EBR  | WBL                  | WBT  | WBR  | NBL  | NBT  | NBR  | SBL  | SBT  | SBR  |      |
|-----------------------------------|-------|------|------|----------------------|------|------|------|------|------|------|------|------|------|
| Lane Configurations               |       |      |      |                      |      |      |      |      |      |      |      |      |      |
| Traffic Volume (veh/h)            | 4     | 955  | 32   | 27                   | 919  | 2    | 31   | 0    | 28   | 2    | 0    | 2    |      |
| Future Volume (Veh/h)             | 4     | 955  | 32   | 27                   | 919  | 2    | 31   | 0    | 28   | 2    | 0    | 2    |      |
| Sign Control                      | Free  |      |      | Free                 |      |      | Stop |      |      | Stop |      |      |      |
| Grade                             | 0%    |      |      | 0%                   |      |      | 0%   |      |      | 0%   |      |      |      |
| Peak Hour Factor                  | 0.93  | 0.93 | 0.93 | 0.92                 | 0.92 | 0.92 | 0.74 | 0.74 | 0.74 | 0.50 | 0.50 | 0.50 |      |
| Hourly flow rate (vph)            | 5     | 1078 | 36   | 31                   | 1049 | 2    | 44   | 0    | 40   | 4    | 0    | 4    |      |
| Pedestrians                       |       |      |      |                      |      |      |      |      |      |      |      |      |      |
| Lane Width (ft)                   |       |      |      |                      |      |      |      |      |      |      |      |      |      |
| Walking Speed (ft/s)              |       |      |      |                      |      |      |      |      |      |      |      |      |      |
| Percent Blockage                  |       |      |      |                      |      |      |      |      |      |      |      |      |      |
| Right turn flare (veh)            |       |      |      |                      |      |      |      |      |      |      |      |      |      |
| Median type                       |       |      |      |                      |      |      |      |      |      |      |      |      |      |
| Median storage veh                |       |      |      |                      |      |      |      |      |      |      |      |      |      |
| Upstream signal (ft)              |       |      |      |                      |      |      |      |      |      |      |      |      |      |
| pX, platoon unblocked             | 0.85  |      |      |                      |      |      | 0.85 |      |      | 0.85 |      |      | 0.85 |
| vC, conflicting volume            | 1051  |      |      | 1114                 |      |      | 1696 |      |      | 2219 |      |      | 526  |
| vC1, stage 1 conf vol             |       |      |      |                      |      |      | 1106 |      |      | 1106 |      |      |      |
| vC2, stage 2 conf vol             |       |      |      |                      |      |      | 590  |      |      | 1113 |      |      |      |
| vCu, unblocked vol                | 704   |      |      | 1114                 |      |      | 1465 |      |      | 2080 |      |      | 85   |
| tC, single (s)                    | 4.1   |      |      | 4.2                  |      |      | 7.6  |      |      | 6.5  |      |      | 7.9  |
| tC, 2 stage (s)                   |       |      |      |                      |      |      | 6.6  |      |      | 5.5  |      |      |      |
| tF (s)                            | 2.2   |      |      | 2.2                  |      |      | 3.5  |      |      | 4.0  |      |      | 3.8  |
| p0 queue free %                   | 99    |      |      | 95                   |      |      | 79   |      |      | 100  |      |      | 99   |
| cM capacity (veh/h)               | 767   |      |      | 611                  |      |      | 207  |      |      | 212  |      |      | 697  |
| Direction, Lane #                 | EB 1  | EB 2 | EB 3 | WB 1                 | WB 2 | WB 3 | NB 1 | SB 1 |      |      |      |      |      |
| Volume Total                      | 5     | 719  | 395  | 31                   | 699  | 352  | 84   | 8    |      |      |      |      |      |
| Volume Left                       | 5     | 0    | 0    | 31                   | 0    | 0    | 44   | 4    |      |      |      |      |      |
| Volume Right                      | 0     | 0    | 36   | 0                    | 0    | 2    | 40   | 4    |      |      |      |      |      |
| cSH                               | 767   | 1700 | 1700 | 611                  | 1700 | 1700 | 283  | 353  |      |      |      |      |      |
| Volume to Capacity                | 0.01  | 0.42 | 0.23 | 0.05                 | 0.41 | 0.21 | 0.30 | 0.02 |      |      |      |      |      |
| Queue Length 95th (ft)            | 0     | 0    | 0    | 4                    | 0    | 0    | 30   | 2    |      |      |      |      |      |
| Control Delay (s)                 | 9.7   | 0.0  | 0.0  | 11.2                 | 0.0  | 0.0  | 23.0 | 15.4 |      |      |      |      |      |
| Lane LOS                          | A     |      |      | B                    |      |      | C    |      |      | C    |      |      |      |
| Approach Delay (s)                | 0.0   |      |      | 0.3                  |      |      | 23.0 |      |      | 15.4 |      |      |      |
| Approach LOS                      |       |      |      |                      |      |      | C    |      |      | C    |      |      |      |
| Intersection Summary              |       |      |      |                      |      |      |      |      |      |      |      |      |      |
| Average Delay                     | 1.1   |      |      |                      |      |      |      |      |      |      |      |      |      |
| Intersection Capacity Utilization | 40.0% |      |      | ICU Level of Service |      |      |      |      | A    |      |      |      |      |
| Analysis Period (min)             | 15    |      |      |                      |      |      |      |      |      |      |      |      |      |



# HCM Unsignalized Intersection Capacity Analysis

## 7: Kroger Rear & Route 11

12/8/2015



| Movement                          | EBT   | EBR  | WBL   | WBT  | NEL                  | NER  |
|-----------------------------------|-------|------|-------|------|----------------------|------|
| Lane Configurations               | ↑↑    |      | ↵     | ↑↑   | ↵                    |      |
| Traffic Volume (veh/h)            | 955   | 1    | 39    | 900  | 0                    | 56   |
| Future Volume (Veh/h)             | 955   | 1    | 39    | 900  | 0                    | 56   |
| Sign Control                      | Free  |      |       | Free | Stop                 |      |
| Grade                             | 0%    |      |       | 0%   | 0%                   |      |
| Peak Hour Factor                  | 0.95  | 0.95 | 0.90  | 0.90 | 0.67                 | 0.67 |
| Hourly flow rate (vph)            | 1056  | 1    | 46    | 1050 | 0                    | 88   |
| Pedestrians                       |       |      |       |      |                      |      |
| Lane Width (ft)                   |       |      |       |      |                      |      |
| Walking Speed (ft/s)              |       |      |       |      |                      |      |
| Percent Blockage                  |       |      |       |      |                      |      |
| Right turn flare (veh)            |       |      |       |      |                      |      |
| Median type                       | TWLTL |      |       | None |                      |      |
| Median storage veh)               | 2     |      |       |      |                      |      |
| Upstream signal (ft)              | 254   |      |       |      |                      |      |
| pX, platoon unblocked             |       |      | 0.78  |      | 0.78                 | 0.78 |
| vC, conflicting volume            |       |      | 1057  |      | 1674                 | 528  |
| vC1, stage 1 conf vol             |       |      |       |      | 1056                 |      |
| vC2, stage 2 conf vol             |       |      |       |      | 617                  |      |
| vCu, unblocked vol                |       |      | 513   |      | 1302                 | 0    |
| tC, single (s)                    |       |      | 4.1   |      | 6.8                  | 6.9  |
| tC, 2 stage (s)                   |       |      |       |      | 5.8                  |      |
| tF (s)                            |       |      | 2.2   |      | 3.5                  | 3.3  |
| p0 queue free %                   |       |      | 94    |      | 100                  | 90   |
| cM capacity (veh/h)               |       |      | 819   |      | 338                  | 852  |
| Direction, Lane #                 | EB 1  | EB 2 | WB 1  | WB 2 | WB 3                 | NE 1 |
| Volume Total                      | 704   | 353  | 46    | 525  | 525                  | 88   |
| Volume Left                       | 0     | 0    | 46    | 0    | 0                    | 0    |
| Volume Right                      | 0     | 1    | 0     | 0    | 0                    | 88   |
| cSH                               | 1700  | 1700 | 819   | 1700 | 1700                 | 852  |
| Volume to Capacity                | 0.41  | 0.21 | 0.06  | 0.31 | 0.31                 | 0.10 |
| Queue Length 95th (ft)            | 0     | 0    | 4     | 0    | 0                    | 9    |
| Control Delay (s)                 | 0.0   | 0.0  | 9.7   | 0.0  | 0.0                  | 9.7  |
| Lane LOS                          |       |      | A     |      |                      | A    |
| Approach Delay (s)                | 0.0   |      | 0.4   |      |                      | 9.7  |
| Approach LOS                      |       |      |       |      |                      | A    |
| <b>Intersection Summary</b>       |       |      |       |      |                      |      |
| Average Delay                     |       |      | 0.6   |      |                      |      |
| Intersection Capacity Utilization |       |      | 44.3% |      | ICU Level of Service | A    |
| Analysis Period (min)             |       |      | 15    |      |                      |      |

# HCM Unsignalized Intersection Capacity Analysis

## 8: Wheatland & Route 11

12/8/2015



| Movement                          | EBT  | EBR  | WBL   | WBT                  | NBL  | NBR  |
|-----------------------------------|------|------|-------|----------------------|------|------|
| Lane Configurations               | ↑↑   |      | ↵     | ↑↑                   | ↵    |      |
| Traffic Volume (veh/h)            | 1002 | 9    | 8     | 932                  | 7    | 7    |
| Future Volume (Veh/h)             | 1002 | 9    | 8     | 932                  | 7    | 7    |
| Sign Control                      | Free |      |       | Free                 | Stop |      |
| Grade                             | 0%   |      |       | 0%                   | 0%   |      |
| Peak Hour Factor                  | 0.95 | 0.95 | 0.90  | 0.90                 | 0.70 | 0.70 |
| Hourly flow rate (vph)            | 1107 | 10   | 9     | 1087                 | 11   | 11   |
| Pedestrians                       |      |      |       |                      |      |      |
| Lane Width (ft)                   |      |      |       |                      |      |      |
| Walking Speed (ft/s)              |      |      |       |                      |      |      |
| Percent Blockage                  |      |      |       |                      |      |      |
| Right turn flare (veh)            |      |      |       |                      |      |      |
| Median type                       | None |      | TWLTL |                      |      |      |
| Median storage (veh)              | 2    |      |       |                      |      |      |
| Upstream signal (ft)              | 335  |      |       |                      |      |      |
| pX, platoon unblocked             |      |      | 0.79  |                      | 0.79 | 0.79 |
| vC, conflicting volume            |      |      | 1117  |                      | 1674 | 558  |
| vC1, stage 1 conf vol             |      |      |       |                      | 1112 |      |
| vC2, stage 2 conf vol             |      |      |       |                      | 562  |      |
| vCu, unblocked vol                |      |      | 606   |                      | 1313 | 0    |
| tC, single (s)                    |      |      | 4.1   |                      | 7.1  | 6.9  |
| tC, 2 stage (s)                   |      |      |       |                      | 6.1  |      |
| tF (s)                            |      |      | 2.2   |                      | 3.6  | 3.3  |
| p0 queue free %                   |      |      | 99    |                      | 96   | 99   |
| cM capacity (veh/h)               |      |      | 772   |                      | 309  | 858  |
| Direction, Lane #                 | EB 1 | EB 2 | WB 1  | WB 2                 | WB 3 | NB 1 |
| Volume Total                      | 738  | 379  | 9     | 544                  | 544  | 22   |
| Volume Left                       | 0    | 0    | 9     | 0                    | 0    | 11   |
| Volume Right                      | 0    | 10   | 0     | 0                    | 0    | 11   |
| cSH                               | 1700 | 1700 | 772   | 1700                 | 1700 | 454  |
| Volume to Capacity                | 0.43 | 0.22 | 0.01  | 0.32                 | 0.32 | 0.05 |
| Queue Length 95th (ft)            | 0    | 0    | 1     | 0                    | 0    | 4    |
| Control Delay (s)                 | 0.0  | 0.0  | 9.7   | 0.0                  | 0.0  | 13.3 |
| Lane LOS                          | A    |      |       | B                    |      |      |
| Approach Delay (s)                | 0.0  |      | 0.1   |                      |      | 13.3 |
| Approach LOS                      |      |      |       | B                    |      |      |
| Intersection Summary              |      |      |       |                      |      |      |
| Average Delay                     |      |      | 0.2   |                      |      |      |
| Intersection Capacity Utilization |      |      | 39.4% | ICU Level of Service |      | A    |
| Analysis Period (min)             |      |      | 15    |                      |      |      |

# HCM Unsignalized Intersection Capacity Analysis

## 9: University Park/Roca's & Route 11

12/8/2015



| Movement               | EBL  | EBT   | EBR  | WBL  | WBT  | WBR  | NBL  | NBT  | NBR  | SBL  | SBT  | SBR  |
|------------------------|------|-------|------|------|------|------|------|------|------|------|------|------|
| Lane Configurations    |      |       |      |      |      |      |      |      |      |      |      |      |
| Traffic Volume (veh/h) | 4    | 973   | 6    | 5    | 961  | 0    | 9    | 0    | 8    | 0    | 0    | 2    |
| Future Volume (Veh/h)  | 4    | 973   | 6    | 5    | 961  | 0    | 9    | 0    | 8    | 0    | 0    | 2    |
| Sign Control           |      | Free  |      |      | Free |      |      | Stop |      |      | Stop |      |
| Grade                  |      | 0%    |      |      | 0%   |      |      | 0%   |      |      | 0%   |      |
| Peak Hour Factor       | 0.96 | 0.96  | 0.96 | 0.84 | 0.84 | 0.84 | 0.63 | 0.63 | 0.63 | 0.50 | 0.50 | 0.50 |
| Hourly flow rate (vph) | 4    | 1064  | 7    | 6    | 1201 | 0    | 15   | 0    | 13   | 0    | 0    | 4    |
| Pedestrians            |      |       |      |      |      |      |      |      |      |      |      |      |
| Lane Width (ft)        |      |       |      |      |      |      |      |      |      |      |      |      |
| Walking Speed (ft/s)   |      |       |      |      |      |      |      |      |      |      |      |      |
| Percent Blockage       |      |       |      |      |      |      |      |      |      |      |      |      |
| Right turn flare (veh) |      |       |      |      |      |      |      |      |      |      |      |      |
| Median type            |      | TWLTL |      |      | None |      |      |      |      |      |      |      |
| Median storage (veh)   |      | 2     |      |      |      |      |      |      |      |      |      |      |
| Upstream signal (ft)   |      | 792   |      |      |      |      |      |      |      |      |      |      |
| pX, platoon unblocked  |      |       |      | 0.82 |      |      | 0.82 | 0.82 | 0.82 | 0.82 | 0.82 |      |
| vC, conflicting volume | 1201 |       |      | 1071 |      |      | 1692 | 2288 | 536  | 1766 | 2292 | 600  |
| vC1, stage 1 conf vol  |      |       |      |      |      |      | 1076 | 1076 |      | 1213 | 1213 |      |
| vC2, stage 2 conf vol  |      |       |      |      |      |      | 616  | 1213 |      | 553  | 1079 |      |
| vCu, unblocked vol     | 1201 |       |      | 640  |      |      | 1400 | 2129 | 0    | 1490 | 2134 | 600  |
| tC, single (s)         | 4.1  |       |      | 4.9  |      |      | 7.5  | 6.5  | 7.2  | 7.5  | 6.5  | 6.9  |
| tC, 2 stage (s)        |      |       |      |      |      |      | 6.5  | 5.5  |      | 6.5  | 5.5  |      |
| tF (s)                 | 2.2  |       |      | 2.6  |      |      | 3.5  | 4.0  | 3.4  | 3.5  | 4.0  | 3.3  |
| p0 queue free %        | 99   |       |      | 99   |      |      | 95   | 100  | 98   | 100  | 100  | 99   |
| cM capacity (veh/h)    | 588  |       |      | 592  |      |      | 278  | 205  | 858  | 187  | 206  | 449  |

| Direction, Lane #      | EB 1 | EB 2 | EB 3 | WB 1 | WB 2 | WB 3 | NB 1 | SB 1 |
|------------------------|------|------|------|------|------|------|------|------|
| Volume Total           | 4    | 709  | 362  | 6    | 801  | 400  | 28   | 4    |
| Volume Left            | 4    | 0    | 0    | 6    | 0    | 0    | 15   | 0    |
| Volume Right           | 0    | 0    | 7    | 0    | 0    | 0    | 13   | 4    |
| cSH                    | 588  | 1700 | 1700 | 592  | 1700 | 1700 | 405  | 449  |
| Volume to Capacity     | 0.01 | 0.42 | 0.21 | 0.01 | 0.47 | 0.24 | 0.07 | 0.01 |
| Queue Length 95th (ft) | 1    | 0    | 0    | 1    | 0    | 0    | 6    | 1    |
| Control Delay (s)      | 11.2 | 0.0  | 0.0  | 11.1 | 0.0  | 0.0  | 14.6 | 13.1 |
| Lane LOS               | B    |      |      | B    |      |      | B    | B    |
| Approach Delay (s)     | 0.0  |      |      | 0.1  |      |      | 14.6 | 13.1 |
| Approach LOS           |      |      |      |      |      |      | B    | B    |

### Intersection Summary

|                                   |       |
|-----------------------------------|-------|
| Average Delay                     | 0.2   |
| Intersection Capacity Utilization | 42.8% |
| ICU Level of Service              | A     |
| Analysis Period (min)             | 15    |

# HCM Unsignalized Intersection Capacity Analysis

## 10: Route 11 & Nesslerod

12/8/2015

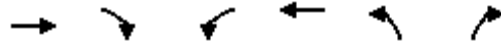


| Movement                          | EBL  | EBT    | WBT    | WBR  | SBL                  | SBR  |
|-----------------------------------|------|--------|--------|------|----------------------|------|
| Lane Configurations               |      |        |        |      |                      |      |
| Traffic Volume (veh/h)            | 3    | 978    | 985    | 1    | 0                    | 1    |
| Future Volume (Veh/h)             | 3    | 978    | 985    | 1    | 0                    | 1    |
| Sign Control                      |      | Free   | Free   |      | Stop                 |      |
| Grade                             |      | 0%     | 0%     |      | 0%                   |      |
| Peak Hour Factor                  | 0.96 | 0.96   | 0.84   | 0.84 | 0.25                 | 0.25 |
| Hourly flow rate (vph)            | 3    | 1070   | 1231   | 1    | 0                    | 4    |
| Pedestrians                       |      |        |        |      |                      |      |
| Lane Width (ft)                   |      |        |        |      |                      |      |
| Walking Speed (ft/s)              |      |        |        |      |                      |      |
| Percent Blockage                  |      |        |        |      |                      |      |
| Right turn flare (veh)            |      |        |        |      |                      |      |
| Median type                       |      | Raised | Raised |      |                      |      |
| Median storage veh                |      | 1      | 1      |      |                      |      |
| Upstream signal (ft)              |      | 1287   |        |      |                      |      |
| pX, platoon unblocked             |      |        |        |      | 0.85                 |      |
| vC, conflicting volume            | 1232 |        |        |      | 1772                 | 616  |
| vC1, stage 1 conf vol             |      |        |        |      | 1232                 |      |
| vC2, stage 2 conf vol             |      |        |        |      | 541                  |      |
| vCu, unblocked vol                | 1232 |        |        |      | 1563                 | 616  |
| tC, single (s)                    | 4.1  |        |        |      | 6.8                  | 6.9  |
| tC, 2 stage (s)                   |      |        |        |      | 5.8                  |      |
| tF (s)                            | 2.2  |        |        |      | 3.5                  | 3.3  |
| p0 queue free %                   | 99   |        |        |      | 100                  | 99   |
| cM capacity (veh/h)               | 573  |        |        |      | 195                  | 438  |
| Direction, Lane #                 | EB 1 | EB 2   | EB 3   | WB 1 | WB 2                 | SB 1 |
| Volume Total                      | 3    | 535    | 535    | 821  | 411                  | 4    |
| Volume Left                       | 3    | 0      | 0      | 0    | 0                    | 0    |
| Volume Right                      | 0    | 0      | 0      | 0    | 1                    | 4    |
| cSH                               | 573  | 1700   | 1700   | 1700 | 1700                 | 438  |
| Volume to Capacity                | 0.01 | 0.31   | 0.31   | 0.48 | 0.24                 | 0.01 |
| Queue Length 95th (ft)            | 0    | 0      | 0      | 0    | 0                    | 1    |
| Control Delay (s)                 | 11.3 | 0.0    | 0.0    | 0.0  | 0.0                  | 13.3 |
| Lane LOS                          | B    |        |        |      |                      | B    |
| Approach Delay (s)                | 0.0  |        |        | 0.0  |                      | 13.3 |
| Approach LOS                      |      |        |        |      |                      | B    |
| Intersection Summary              |      |        |        |      |                      |      |
| Average Delay                     |      |        | 0.0    |      |                      |      |
| Intersection Capacity Utilization |      |        | 38.6%  |      | ICU Level of Service | A    |
| Analysis Period (min)             |      |        | 15     |      |                      |      |

# HCM Unsignalized Intersection Capacity Analysis

## 11: Hazel Hollow Rd & Route 11

12/8/2015

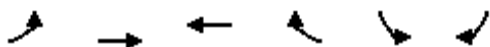


| Movement                          | EBT    | EBR  | WBL    | WBT                  | NBL  | NBR  |      |
|-----------------------------------|--------|------|--------|----------------------|------|------|------|
| Lane Configurations               | ↑↑     | ↑    | ↓      | ↑↑                   | ↓    |      |      |
| Traffic Volume (veh/h)            | 958    | 20   | 21     | 970                  | 16   | 27   |      |
| Future Volume (Veh/h)             | 958    | 20   | 21     | 970                  | 16   | 27   |      |
| Sign Control                      | Free   |      |        | Free                 | Stop |      |      |
| Grade                             | 0%     |      |        | 0%                   | 0%   |      |      |
| Peak Hour Factor                  | 0.96   | 0.96 | 0.84   | 0.84                 | 0.63 | 0.63 |      |
| Hourly flow rate (vph)            | 1048   | 22   | 26     | 1213                 | 27   | 45   |      |
| Pedestrians                       |        |      |        |                      |      |      |      |
| Lane Width (ft)                   |        |      |        |                      |      |      |      |
| Walking Speed (ft/s)              |        |      |        |                      |      |      |      |
| Percent Blockage                  |        |      |        |                      |      |      |      |
| Right turn flare (veh)            |        |      |        |                      |      |      |      |
| Median type                       | Raised |      | Raised |                      |      |      |      |
| Median storage (veh)              | 1      |      | 1      |                      |      |      |      |
| Upstream signal (ft)              |        |      |        |                      |      |      |      |
| pX, platoon unblocked             |        |      |        |                      |      |      |      |
| vC, conflicting volume            |        |      | 1070   |                      | 1706 | 524  |      |
| vC1, stage 1 conf vol             |        |      |        |                      | 1048 |      |      |
| vC2, stage 2 conf vol             |        |      |        |                      | 658  |      |      |
| vCu, unblocked vol                |        |      | 1070   |                      | 1706 | 524  |      |
| tC, single (s)                    |        |      | 4.3    |                      | 6.9  | 6.9  |      |
| tC, 2 stage (s)                   |        |      |        |                      | 5.9  |      |      |
| tF (s)                            |        |      | 2.3    |                      | 3.6  | 3.3  |      |
| p0 queue free %                   |        |      | 96     |                      | 86   | 91   |      |
| cM capacity (veh/h)               |        |      | 602    |                      | 193  | 503  |      |
| Direction, Lane #                 | EB 1   | EB 2 | EB 3   | WB 1                 | WB 2 | WB 3 | NB 1 |
| Volume Total                      | 524    | 524  | 22     | 26                   | 606  | 606  | 72   |
| Volume Left                       | 0      | 0    | 0      | 26                   | 0    | 0    | 27   |
| Volume Right                      | 0      | 0    | 22     | 0                    | 0    | 0    | 45   |
| cSH                               | 1700   | 1700 | 1700   | 602                  | 1700 | 1700 | 314  |
| Volume to Capacity                | 0.31   | 0.31 | 0.01   | 0.04                 | 0.36 | 0.36 | 0.23 |
| Queue Length 95th (ft)            | 0      | 0    | 0      | 3                    | 0    | 0    | 22   |
| Control Delay (s)                 | 0.0    | 0.0  | 0.0    | 11.3                 | 0.0  | 0.0  | 19.9 |
| Lane LOS                          |        |      |        | B                    | C    |      |      |
| Approach Delay (s)                | 0.0    |      |        | 0.2                  |      |      | 19.9 |
| Approach LOS                      |        |      |        |                      |      |      | C    |
| Intersection Summary              |        |      |        |                      |      |      |      |
| Average Delay                     |        |      | 0.7    |                      |      |      |      |
| Intersection Capacity Utilization |        |      | 38.2%  | ICU Level of Service |      | A    |      |
| Analysis Period (min)             |        |      | 15     |                      |      |      |      |

# HCM Signalized Intersection Capacity Analysis

## 2: Route 11 & Route 114

12/8/2015



| Movement               | EBL   | EBT   | WBT   | WBR   | SBL   | SBR   |
|------------------------|-------|-------|-------|-------|-------|-------|
| Lane Configurations    |       |       |       |       |       |       |
| Traffic Volume (vph)   | 335   | 284   | 463   | 548   | 824   | 410   |
| Future Volume (vph)    | 335   | 284   | 463   | 548   | 824   | 410   |
| Ideal Flow (vphpl)     | 1900  | 1900  | 1900  | 1900  | 1900  | 1900  |
| Total Lost time (s)    | 5.5   | 6.0   | 6.0   | 4.0   | 5.5   | 4.0   |
| Lane Util. Factor      | 1.00  | *0.85 | *0.90 | 1.00  | *0.85 | 1.00  |
| Frt                    | 1.00  | 1.00  | 1.00  | 0.85  | 1.00  | 0.85  |
| Flt Protected          | 0.95  | 1.00  | 1.00  | 1.00  | 0.95  | 1.00  |
| Satd. Flow (prot)      | 1787  | 3167  | 3386  | 1599  | 3038  | 1583  |
| Flt Permitted          | 0.95  | 1.00  | 1.00  | 1.00  | 0.95  | 1.00  |
| Satd. Flow (perm)      | 1787  | 3167  | 3386  | 1599  | 3038  | 1583  |
| Peak-hour factor, PHF  | 0.96  | 0.96  | 0.90  | 0.90  | 0.90  | 0.90  |
| Growth Factor (vph)    | 105%  | 105%  | 105%  | 105%  | 105%  | 105%  |
| Adj. Flow (vph)        | 366   | 311   | 540   | 639   | 961   | 478   |
| RTOR Reduction (vph)   | 0     | 0     | 0     | 0     | 0     | 0     |
| Lane Group Flow (vph)  | 366   | 311   | 540   | 639   | 961   | 478   |
| Heavy Vehicles (%)     | 1%    | 2%    | 1%    | 1%    | 1%    | 2%    |
| Turn Type              | Prot  | NA    | NA    | Free  | Prot  | Free  |
| Protected Phases       | 1     | 6     | 2     |       | 4     |       |
| Permitted Phases       |       |       |       | Free  |       | Free  |
| Actuated Green, G (s)  | 33.7  | 70.0  | 30.8  | 132.5 | 51.0  | 132.5 |
| Effective Green, g (s) | 33.7  | 70.0  | 30.8  | 132.5 | 51.0  | 132.5 |
| Actuated g/C Ratio     | 0.25  | 0.53  | 0.23  | 1.00  | 0.38  | 1.00  |
| Clearance Time (s)     | 5.5   | 6.0   | 6.0   |       | 5.5   |       |
| Vehicle Extension (s)  | 4.0   | 4.5   | 4.5   |       | 4.5   |       |
| Lane Grp Cap (vph)     | 454   | 1673  | 787   | 1599  | 1169  | 1583  |
| v/s Ratio Prot         | c0.20 | 0.10  | c0.16 |       | c0.32 |       |
| v/s Ratio Perm         |       |       |       | 0.40  |       | 0.30  |
| v/c Ratio              | 0.81  | 0.19  | 0.69  | 0.40  | 0.82  | 0.30  |
| Uniform Delay, d1      | 46.3  | 16.3  | 46.4  | 0.0   | 36.7  | 0.0   |
| Progression Factor     | 1.00  | 1.00  | 1.00  | 1.00  | 1.00  | 1.00  |
| Incremental Delay, d2  | 10.6  | 0.1   | 2.9   | 0.7   | 5.2   | 0.5   |
| Delay (s)              | 56.9  | 16.4  | 49.4  | 0.7   | 41.9  | 0.5   |
| Level of Service       | E     | B     | D     | A     | D     | A     |
| Approach Delay (s)     |       | 38.3  | 23.0  |       | 28.1  |       |
| Approach LOS           |       | D     | C     |       | C     |       |

### Intersection Summary

|                                   |       |                           |      |
|-----------------------------------|-------|---------------------------|------|
| HCM 2000 Control Delay            | 28.4  | HCM 2000 Level of Service | C    |
| HCM 2000 Volume to Capacity ratio | 0.78  |                           |      |
| Actuated Cycle Length (s)         | 132.5 | Sum of lost time (s)      | 17.0 |
| Intersection Capacity Utilization | 71.8% | ICU Level of Service      | C    |
| Analysis Period (min)             | 15    |                           |      |

c Critical Lane Group

# HCM Signalized Intersection Capacity Analysis

## 6: Kroger/The Rocks & Route 11

12/8/2015



| Movement               | EBL  | EBT   | EBR  | WBL   | WBT   | WBR  | NBL   | NBT  | NBR  | SBL  | SBT  | SBR  |
|------------------------|------|-------|------|-------|-------|------|-------|------|------|------|------|------|
| Lane Configurations    |      |       |      |       |       |      |       |      |      |      |      |      |
| Traffic Volume (vph)   | 29   | 922   | 132  | 127   | 1187  | 6    | 152   | 6    | 159  | 36   | 7    | 65   |
| Future Volume (vph)    | 29   | 922   | 132  | 127   | 1187  | 6    | 152   | 6    | 159  | 36   | 7    | 65   |
| Ideal Flow (vphpl)     | 1900 | 1900  | 1900 | 1900  | 1900  | 1900 | 1900  | 1900 | 1900 | 1900 | 1900 | 1900 |
| Total Lost time (s)    | 5.5  | 6.0   |      | 5.5   | 6.0   |      | 5.5   | 5.5  |      |      | 5.5  | 5.5  |
| Lane Util. Factor      | 1.00 | *0.85 |      | 1.00  | *0.75 |      | 1.00  | 1.00 |      |      | 1.00 | 1.00 |
| Frt                    | 1.00 | 0.98  |      | 1.00  | 1.00  |      | 1.00  | 0.86 |      |      | 1.00 | 0.85 |
| Flt Protected          | 0.95 | 1.00  |      | 0.95  | 1.00  |      | 0.95  | 1.00 |      |      | 0.96 | 1.00 |
| Satd. Flow (prot)      | 1805 | 3134  |      | 1770  | 2820  |      | 1770  | 1611 |      |      | 1779 | 1615 |
| Flt Permitted          | 0.95 | 1.00  |      | 0.95  | 1.00  |      | 0.73  | 1.00 |      |      | 0.61 | 1.00 |
| Satd. Flow (perm)      | 1805 | 3134  |      | 1770  | 2820  |      | 1351  | 1611 |      |      | 1127 | 1615 |
| Peak-hour factor, PHF  | 0.93 | 0.93  | 0.93 | 0.89  | 0.89  | 0.89 | 0.84  | 0.84 | 0.84 | 0.93 | 0.93 | 0.93 |
| Growth Factor (vph)    | 105% | 105%  | 105% | 105%  | 105%  | 105% | 105%  | 105% | 105% | 105% | 105% | 105% |
| Adj. Flow (vph)        | 33   | 1041  | 149  | 150   | 1400  | 7    | 190   | 8    | 199  | 41   | 8    | 73   |
| RTOR Reduction (vph)   | 0    | 8     | 0    | 0     | 0     | 0    | 0     | 158  | 0    | 0    | 0    | 58   |
| Lane Group Flow (vph)  | 33   | 1182  | 0    | 150   | 1407  | 0    | 190   | 49   | 0    | 0    | 49   | 15   |
| Heavy Vehicles (%)     | 0%   | 1%    | 2%   | 2%    | 1%    | 0%   | 2%    | 0%   | 1%   | 3%   | 0%   | 0%   |
| Turn Type              | Prot | NA    |      | Prot  | NA    |      | Perm  | NA   |      | Perm | NA   | Perm |
| Protected Phases       | 1    | 6     |      | 5     | 2     |      |       | 8    |      |      | 4    |      |
| Permitted Phases       |      |       |      |       |       |      | 8     |      |      | 4    |      | 4    |
| Actuated Green, G (s)  | 4.8  | 48.3  |      | 14.2  | 57.7  |      | 20.7  | 20.7 |      |      | 20.7 | 20.7 |
| Effective Green, g (s) | 4.8  | 48.3  |      | 14.2  | 57.7  |      | 20.7  | 20.7 |      |      | 20.7 | 20.7 |
| Actuated g/C Ratio     | 0.05 | 0.48  |      | 0.14  | 0.58  |      | 0.21  | 0.21 |      |      | 0.21 | 0.21 |
| Clearance Time (s)     | 5.5  | 6.0   |      | 5.5   | 6.0   |      | 5.5   | 5.5  |      |      | 5.5  | 5.5  |
| Vehicle Extension (s)  | 3.5  | 4.0   |      | 3.5   | 4.0   |      | 4.0   | 4.0  |      |      | 4.0  | 4.0  |
| Lane Grp Cap (vph)     | 86   | 1510  |      | 250   | 1623  |      | 279   | 332  |      |      | 232  | 333  |
| v/s Ratio Prot         | 0.02 | 0.38  |      | c0.08 | c0.50 |      |       | 0.03 |      |      |      |      |
| v/s Ratio Perm         |      |       |      |       |       |      | c0.14 |      |      |      | 0.04 | 0.01 |
| v/c Ratio              | 0.38 | 0.78  |      | 0.60  | 0.87  |      | 0.68  | 0.15 |      |      | 0.21 | 0.05 |
| Uniform Delay, d1      | 46.3 | 21.6  |      | 40.3  | 18.0  |      | 36.7  | 32.5 |      |      | 33.0 | 31.8 |
| Progression Factor     | 1.00 | 1.00  |      | 1.00  | 1.00  |      | 1.00  | 1.00 |      |      | 1.00 | 1.00 |
| Incremental Delay, d2  | 3.3  | 2.9   |      | 4.1   | 5.3   |      | 7.2   | 0.3  |      |      | 0.6  | 0.1  |
| Delay (s)              | 49.6 | 24.5  |      | 44.4  | 23.3  |      | 43.9  | 32.8 |      |      | 33.6 | 31.9 |
| Level of Service       | D    | C     |      | D     | C     |      | D     | C    |      |      | C    | C    |
| Approach Delay (s)     |      | 25.2  |      |       | 25.4  |      |       | 38.1 |      |      | 32.6 |      |
| Approach LOS           |      | C     |      |       | C     |      |       | D    |      |      | C    |      |

### Intersection Summary

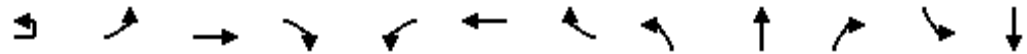
|                                   |       |                           |      |
|-----------------------------------|-------|---------------------------|------|
| HCM 2000 Control Delay            | 27.1  | HCM 2000 Level of Service | C    |
| HCM 2000 Volume to Capacity ratio | 0.83  |                           |      |
| Actuated Cycle Length (s)         | 100.2 | Sum of lost time (s)      | 17.0 |
| Intersection Capacity Utilization | 74.9% | ICU Level of Service      | D    |
| Analysis Period (min)             | 15    |                           |      |

c Critical Lane Group

# HCM Unsignalized Intersection Capacity Analysis

## 3: Cherry Ct/Driveway & Route 11

12/8/2015



| Movement                          | EBU         | EBL         | EBT         | EBR         | WBL         | WBT                  | WBR         | NBL  | NBT  | NBR  | SBL  | SBT  |
|-----------------------------------|-------------|-------------|-------------|-------------|-------------|----------------------|-------------|------|------|------|------|------|
| Lane Configurations               |             |             |             |             |             |                      |             |      |      |      |      |      |
| Traffic Volume (veh/h)            | 2           | 12          | 1090        | 4           | 1           | 1040                 | 0           | 3    | 0    | 0    | 5    | 0    |
| Future Volume (Veh/h)             | 2           | 12          | 1090        | 4           | 1           | 1040                 | 0           | 3    | 0    | 0    | 5    | 0    |
| Sign Control                      |             |             | Free        |             |             | Free                 |             |      | Stop |      |      | Stop |
| Grade                             |             |             | 0%          |             |             | 0%                   |             |      | 0%   |      |      | 0%   |
| Peak Hour Factor                  | 0.92        | 0.92        | 0.92        | 0.92        | 0.90        | 0.90                 | 0.90        | 0.25 | 0.25 | 0.25 | 0.75 | 0.75 |
| Hourly flow rate (vph)            | 0           | 14          | 1244        | 5           | 1           | 1213                 | 0           | 13   | 0    | 0    | 7    | 0    |
| Pedestrians                       |             |             |             |             |             |                      |             |      |      |      |      |      |
| Lane Width (ft)                   |             |             |             |             |             |                      |             |      |      |      |      |      |
| Walking Speed (ft/s)              |             |             |             |             |             |                      |             |      |      |      |      |      |
| Percent Blockage                  |             |             |             |             |             |                      |             |      |      |      |      |      |
| Right turn flare (veh)            |             |             |             |             |             |                      |             |      |      |      |      |      |
| Median type                       |             |             | None        |             |             | TWLTL                |             |      |      |      |      |      |
| Median storage (veh)              |             |             |             |             |             | 2                    |             |      |      |      |      |      |
| Upstream signal (ft)              |             |             | 779         |             |             |                      |             |      |      |      |      |      |
| pX, platoon unblocked             | 0.00        |             |             |             | 0.97        |                      |             | 0.97 | 0.97 | 0.97 | 0.97 | 0.97 |
| vC, conflicting volume            | 0           | 1213        |             |             | 1249        |                      |             | 1918 | 2490 | 624  | 1865 | 2492 |
| vC1, stage 1 conf vol             |             |             |             |             |             |                      |             | 1274 | 1274 |      | 1215 | 1215 |
| vC2, stage 2 conf vol             |             |             |             |             |             |                      |             | 644  | 1215 |      | 650  | 1277 |
| vCu, unblocked vol                | 0           | 1213        |             |             | 1194        |                      |             | 1884 | 2473 | 550  | 1829 | 2476 |
| tC, single (s)                    | 0.0         | 4.1         |             |             | 4.1         |                      |             | 7.5  | 6.5  | 6.9  | 7.5  | 6.5  |
| tC, 2 stage (s)                   |             |             |             |             |             |                      |             | 6.5  | 5.5  |      | 6.5  | 5.5  |
| tF (s)                            | 0.0         | 2.2         |             |             | 2.2         |                      |             | 3.5  | 4.0  | 3.3  | 3.5  | 4.0  |
| p0 queue free %                   | 0           | 98          |             |             | 100         |                      |             | 92   | 100  | 100  | 96   | 100  |
| cM capacity (veh/h)               | 0           | 582         |             |             | 574         |                      |             | 164  | 166  | 469  | 177  | 170  |
| <b>Direction, Lane #</b>          | <b>EB 1</b> | <b>EB 2</b> | <b>WB 1</b> | <b>WB 2</b> | <b>WB 3</b> | <b>NB 1</b>          | <b>SB 1</b> |      |      |      |      |      |
| Volume Total                      | 636         | 627         | 1           | 809         | 404         | 13                   | 42          |      |      |      |      |      |
| Volume Left                       | 14          | 0           | 1           | 0           | 0           | 13                   | 7           |      |      |      |      |      |
| Volume Right                      | 0           | 5           | 0           | 0           | 0           | 0                    | 35          |      |      |      |      |      |
| cSH                               | 582         | 1700        | 574         | 1700        | 1700        | 164                  | 355         |      |      |      |      |      |
| Volume to Capacity                | 0.02        | 0.37        | 0.00        | 0.48        | 0.24        | 0.08                 | 0.12        |      |      |      |      |      |
| Queue Length 95th (ft)            | 2           | 0           | 0           | 0           | 0           | 6                    | 10          |      |      |      |      |      |
| Control Delay (s)                 | 0.7         | 0.0         | 11.3        | 0.0         | 0.0         | 28.9                 | 16.5        |      |      |      |      |      |
| Lane LOS                          | A           |             | B           |             |             | D                    | C           |      |      |      |      |      |
| Approach Delay (s)                | 0.3         |             | 0.0         |             |             | 28.9                 | 16.5        |      |      |      |      |      |
| Approach LOS                      |             |             |             |             |             | D                    | C           |      |      |      |      |      |
| <b>Intersection Summary</b>       |             |             |             |             |             |                      |             |      |      |      |      |      |
| Average Delay                     |             |             | 0.6         |             |             |                      |             |      |      |      |      |      |
| Intersection Capacity Utilization |             |             | 52.1%       |             |             | ICU Level of Service |             |      | A    |      |      |      |
| Analysis Period (min)             |             |             | 15          |             |             |                      |             |      |      |      |      |      |



# HCM Unsignalized Intersection Capacity Analysis

## 3: Cherry Ct/Driveway & Route 11

12/8/2015

| Movement               | SBR  |
|------------------------|------|
| Lane Configurations    |      |
| Traffic Volume (veh/h) | 25   |
| Future Volume (Veh/h)  | 25   |
| Sign Control           |      |
| Grade                  |      |
| Peak Hour Factor       | 0.75 |
| Hourly flow rate (vph) | 35   |
| Pedestrians            |      |
| Lane Width (ft)        |      |
| Walking Speed (ft/s)   |      |
| Percent Blockage       |      |
| Right turn flare (veh) |      |
| Median type            |      |
| Median storage (veh)   |      |
| Upstream signal (ft)   |      |
| pX, platoon unblocked  |      |
| vC, conflicting volume | 606  |
| vC1, stage 1 conf vol  |      |
| vC2, stage 2 conf vol  |      |
| vCu, unblocked vol     | 606  |
| tC, single (s)         | 6.9  |
| tC, 2 stage (s)        |      |
| tF (s)                 | 3.3  |
| p0 queue free %        | 92   |
| cM capacity (veh/h)    | 445  |
| Direction, Lane #      |      |

# HCM Unsignalized Intersection Capacity Analysis

## 4: Route 11 & Belspring Rd

12/8/2015



| Movement               | EBL  | EBT   | EBR  | WBL  | WBT   | WBR  | NBL  | NBT  | NBR  | SBL  | SBT  | SBR  |
|------------------------|------|-------|------|------|-------|------|------|------|------|------|------|------|
| Lane Configurations    |      |       |      |      |       |      |      |      |      |      |      |      |
| Traffic Volume (veh/h) | 1    | 1097  | 1    | 0    | 1096  | 305  | 0    | 0    | 2    | 18   | 0    | 5    |
| Future Volume (Veh/h)  | 1    | 1097  | 1    | 0    | 1096  | 305  | 0    | 0    | 2    | 18   | 0    | 5    |
| Sign Control           |      | Free  |      |      | Free  |      |      | Stop |      |      | Stop |      |
| Grade                  |      | 0%    |      |      | 0%    |      |      | 0%   |      |      | 0%   |      |
| Peak Hour Factor       | 0.96 | 0.96  | 0.96 | 0.89 | 0.89  | 0.89 | 0.50 | 0.50 | 0.50 | 0.64 | 0.64 | 0.64 |
| Hourly flow rate (vph) | 1    | 1200  | 1    | 0    | 1293  | 360  | 0    | 0    | 4    | 30   | 0    | 8    |
| Pedestrians            |      |       |      |      |       |      |      |      |      |      |      |      |
| Lane Width (ft)        |      |       |      |      |       |      |      |      |      |      |      |      |
| Walking Speed (ft/s)   |      |       |      |      |       |      |      |      |      |      |      |      |
| Percent Blockage       |      |       |      |      |       |      |      |      |      |      |      |      |
| Right turn flare (veh) |      |       |      |      |       |      |      |      |      |      |      |      |
| Median type            |      | TWLTL |      |      | TWLTL |      |      |      |      |      |      |      |
| Median storage (veh)   |      | 2     |      |      | 2     |      |      |      |      |      |      |      |
| Upstream signal (ft)   |      |       |      |      | 842   |      |      |      |      |      |      |      |
| pX, platoon unblocked  | 0.60 |       |      |      |       |      | 0.60 | 0.60 |      | 0.60 | 0.60 | 0.60 |
| vC, conflicting volume | 1653 |       |      | 1201 |       |      | 1857 | 2856 | 600  | 2079 | 2676 | 826  |
| vC1, stage 1 conf vol  |      |       |      |      |       |      | 1202 | 1202 |      | 1473 | 1473 |      |
| vC2, stage 2 conf vol  |      |       |      |      |       |      | 654  | 1653 |      | 606  | 1203 |      |
| vCu, unblocked vol     | 755  |       |      | 1201 |       |      | 1095 | 2759 | 600  | 1465 | 2460 | 0    |
| tC, single (s)         | 4.1  |       |      | 4.1  |       |      | 7.5  | 6.5  | 6.9  | 7.5  | 6.5  | 6.9  |
| tC, 2 stage (s)        |      |       |      |      |       |      | 6.5  | 5.5  |      | 6.5  | 5.5  |      |
| tF (s)                 | 2.2  |       |      | 2.2  |       |      | 3.5  | 4.0  | 3.3  | 3.5  | 4.0  | 3.3  |
| p0 queue free %        | 100  |       |      | 100  |       |      | 100  | 100  | 99   | 89   | 100  | 99   |
| cM capacity (veh/h)    | 519  |       |      | 588  |       |      | 191  | 170  | 449  | 265  | 197  | 654  |

| Direction, Lane #      | EB 1 | EB 2 | EB 3 | WB 1 | WB 2 | WB 3 | NB 1 | SB 1 |
|------------------------|------|------|------|------|------|------|------|------|
| Volume Total           | 1    | 800  | 401  | 0    | 862  | 791  | 4    | 38   |
| Volume Left            | 1    | 0    | 0    | 0    | 0    | 0    | 0    | 30   |
| Volume Right           | 0    | 0    | 1    | 0    | 0    | 360  | 4    | 8    |
| cSH                    | 519  | 1700 | 1700 | 1700 | 1700 | 1700 | 449  | 303  |
| Volume to Capacity     | 0.00 | 0.47 | 0.24 | 0.00 | 0.51 | 0.47 | 0.01 | 0.13 |
| Queue Length 95th (ft) | 0    | 0    | 0    | 0    | 0    | 0    | 1    | 11   |
| Control Delay (s)      | 12.0 | 0.0  | 0.0  | 0.0  | 0.0  | 0.0  | 13.1 | 18.6 |
| Lane LOS               | B    |      |      |      |      |      | B    | C    |
| Approach Delay (s)     | 0.0  |      |      | 0.0  |      |      | 13.1 | 18.6 |
| Approach LOS           |      |      |      |      |      |      | B    | C    |



















### Intersection Summary

|                                   |      |
|-----------------------------------|------|
| Average Delay                     | 0.3  |
| Intersection Capacity Utilization | Err% |
| ICU Level of Service              | H    |
| Analysis Period (min)             | 15   |

# HCM Unsignalized Intersection Capacity Analysis

## 5: Driveway/Nicewander Way & Route 11

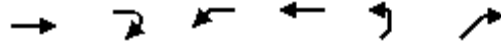
12/8/2015

|                                   |  |  |  |  |  |  |  |  |  |  |  |  |
|-----------------------------------|---|---|---|---|---|---|--|---|---|---|---|---|
| Movement                          | EBL   | EBT   | EBR   | WBL   | WBT   | WBR   | NBL  | NBT   | NBR   | SBL   | SBT   | SBR   |
| Lane Configurations               |  |  |   |  |  |   |  |  |   |   |  |   |
| Traffic Volume (veh/h)            | 4   | 1087  | 26  | 18  | 1379  | 5   | 18   | 0   | 18  | 4   | 0   | 4   |
| Future Volume (Veh/h)             | 4   | 1087  | 26  | 18  | 1379  | 5   | 18   | 0   | 18  | 4   | 0   | 4   |
| Sign Control                      |   | Free  |   |   | Free  |   |  | Stop  |   |   | Stop  |   |
| Grade                             |   | 0%  |   |   | 0%  |   |  | 0%  |   |   | 0%  |   |
| Peak Hour Factor                  | 0.96  | 0.96  | 0.96  | 0.89  | 0.89  | 0.89  | 0.82   | 0.82  | 0.82  | 0.67  | 0.67  | 0.67  |
| Hourly flow rate (vph)            | 4   | 1189  | 28  | 21  | 1627  | 6   | 23   | 0   | 23  | 6   | 0   | 6   |
| Pedestrians                       |   |   |   |   |   |   |  |   |   |   |   |   |
| Lane Width (ft)                   |   |   |   |   |   |   |  |   |   |   |   |   |
| Walking Speed (ft/s)              |   |   |   |   |   |   |  |   |   |   |   |   |
| Percent Blockage                  |   |   |   |   |   |   |  |   |   |   |   |   |
| Right turn flare (veh)            |   |   |   |   |   |   |  |   |   |   |   |   |
| Median type                       |   | TWLTL   |   |   | TWLTL   |   |  |   |   |   |   |   |
| Median storage (veh)              |   | 2   |   |   | 2   |   |  |   |   |   |   |   |
| Upstream signal (ft)              |   |   |   |   | 710   |   |  |   |   |   |   |   |
| pX, platoon unblocked             | 0.60  |   |   |   |   |   | 0.60   | 0.60  |   | 0.60  | 0.60  | 0.60  |
| vC, conflicting volume            | 1633  |   |   | 1217  |   |   | 2072   | 2886  | 608   | 2298  | 2897  | 816   |
| vC1, stage 1 conf vol             |   |   |   |   |   |   | 1211   | 1211  |   | 1672  | 1672  |   |
| vC2, stage 2 conf vol             |   |   |   |   |   |   | 862  | 1675  |   | 626   | 1225  |   |
| vCu, unblocked vol                | 704   |   |   | 1217  |   |   | 1442   | 2809  | 608   | 1820  | 2827  | 0   |
| tC, single (s)                    | 4.1   |   |   | 4.1   |   |   | 7.5  | 6.5   | 6.9   | 7.5   | 6.5   | 6.9   |
| tC, 2 stage (s)                   |   |   |   |   |   |   | 6.5  | 5.5   |   | 6.5   | 5.5   |   |
| tF (s)                            | 2.2   |   |   | 2.2   |   |   | 3.5  | 4.0   | 3.3   | 3.5   | 4.0   | 3.3   |
| p0 queue free %                   | 99  |   |   | 96  |   |   | 87   | 100   | 95  | 97  | 100   | 99  |
| cM capacity (veh/h)               | 538   |   |   | 580   |   |   | 183  | 161   | 443   | 179   | 155   | 650   |
| <b>Direction, Lane #</b>          | <b>EB 1</b>   | <b>EB 2</b>   | <b>EB 3</b>   | <b>WB 1</b>   | <b>WB 2</b>   | <b>WB 3</b>   | <b>NB 1</b>  | <b>SB 1</b>   |   |   |   |   |
| Volume Total                      | 4   | 793   | 424   | 21  | 1085  | 548   | 46   | 12  |   |   |   |   |
| Volume Left                       | 4   | 0   | 0   | 21  | 0   | 0   | 23   | 6   |   |   |   |   |
| Volume Right                      | 0   | 0   | 28  | 0   | 0   | 6   | 23   | 6   |   |   |   |   |
| cSH                               | 538   | 1700  | 1700  | 580   | 1700  | 1700  | 259  | 281   |   |   |   |   |
| Volume to Capacity                | 0.01  | 0.47  | 0.25  | 0.04  | 0.64  | 0.32  | 0.18   | 0.04  |   |   |   |   |
| Queue Length 95th (ft)            | 1   | 0   | 0   | 3   | 0   | 0   | 16   | 3   |   |   |   |   |
| Control Delay (s)                 | 11.7  | 0.0   | 0.0   | 11.4  | 0.0   | 0.0   | 21.9   | 18.4  |   |   |   |   |
| Lane LOS                          | B   |   |   | B   |   |   | C  | C   |   |   |   |   |
| Approach Delay (s)                | 0.0   |   |   | 0.1   |   |   | 21.9   | 18.4  |   |   |   |   |
| Approach LOS                      |   |   |   |   |   |   | C  | C   |   |   |   |   |
| <b>Intersection Summary</b>       |   |   |   |   |   |   |  |   |   |   |   |   |
| Average Delay                     |   |   | 0.5   |   |   |   |  |   |   |   |   |   |
| Intersection Capacity Utilization |   |   | 50.2%   |   | ICU Level of Service  |   |  | A   |   |   |   |   |
| Analysis Period (min)             |   |   | 15  |   |   |   |  |   |   |   |   |   |

# HCM Unsignalized Intersection Capacity Analysis

## 7: Kroger Rear & Route 11

12/8/2015



| Movement                          | EBT   | EBR  | WBL   | WBT                  | NEL  | NER  |
|-----------------------------------|-------|------|-------|----------------------|------|------|
| Lane Configurations               | ↑↑    |      | ↵     | ↑↑                   | ↵    |      |
| Traffic Volume (veh/h)            | 1105  | 2    | 49    | 1348                 | 4    | 49   |
| Future Volume (Veh/h)             | 1105  | 2    | 49    | 1348                 | 4    | 49   |
| Sign Control                      | Free  |      |       | Free                 | Stop |      |
| Grade                             | 0%    |      |       | 0%                   | 0%   |      |
| Peak Hour Factor                  | 0.92  | 0.92 | 0.89  | 0.89                 | 0.78 | 0.78 |
| Hourly flow rate (vph)            | 1261  | 2    | 58    | 1590                 | 5    | 66   |
| Pedestrians                       |       |      |       |                      |      |      |
| Lane Width (ft)                   |       |      |       |                      |      |      |
| Walking Speed (ft/s)              |       |      |       |                      |      |      |
| Percent Blockage                  |       |      |       |                      |      |      |
| Right turn flare (veh)            |       |      |       |                      |      |      |
| Median type                       | TWLTL |      | None  |                      |      |      |
| Median storage veh)               | 2     |      |       |                      |      |      |
| Upstream signal (ft)              | 254   |      |       |                      |      |      |
| pX, platoon unblocked             |       |      | 0.70  |                      | 0.70 | 0.70 |
| vC, conflicting volume            |       |      | 1263  |                      | 2173 | 632  |
| vC1, stage 1 conf vol             |       |      |       |                      | 1262 |      |
| vC2, stage 2 conf vol             |       |      |       |                      | 911  |      |
| vCu, unblocked vol                |       |      | 502   |                      | 1811 | 0    |
| tC, single (s)                    |       |      | 4.1   |                      | 7.3  | 6.9  |
| tC, 2 stage (s)                   |       |      |       |                      | 6.3  |      |
| tF (s)                            |       |      | 2.2   |                      | 3.8  | 3.3  |
| p0 queue free %                   |       |      | 92    |                      | 98   | 91   |
| cM capacity (veh/h)               |       |      | 736   |                      | 213  | 759  |
| Direction, Lane #                 | EB 1  | EB 2 | WB 1  | WB 2                 | WB 3 | NE 1 |
| Volume Total                      | 841   | 422  | 58    | 795                  | 795  | 71   |
| Volume Left                       | 0     | 0    | 58    | 0                    | 0    | 5    |
| Volume Right                      | 0     | 2    | 0     | 0                    | 0    | 66   |
| cSH                               | 1700  | 1700 | 736   | 1700                 | 1700 | 643  |
| Volume to Capacity                | 0.49  | 0.25 | 0.08  | 0.47                 | 0.47 | 0.11 |
| Queue Length 95th (ft)            | 0     | 0    | 6     | 0                    | 0    | 9    |
| Control Delay (s)                 | 0.0   | 0.0  | 10.3  | 0.0                  | 0.0  | 11.3 |
| Lane LOS                          |       |      | B     |                      |      | B    |
| Approach Delay (s)                | 0.0   |      | 0.4   |                      |      | 11.3 |
| Approach LOS                      |       |      |       |                      |      | B    |
| Intersection Summary              |       |      |       |                      |      |      |
| Average Delay                     |       |      | 0.5   |                      |      |      |
| Intersection Capacity Utilization |       |      | 49.2% | ICU Level of Service | A    |      |
| Analysis Period (min)             | 15    |      |       |                      |      |      |

# HCM Unsignalized Intersection Capacity Analysis

## 8: Wheatland & Route 11

12/8/2015



| Movement                          | EBT  | EBR  | WBL   | WBT                  | NBL  | NBR  |
|-----------------------------------|------|------|-------|----------------------|------|------|
| Lane Configurations               | ↑↑   |      | ↙     | ↑↑                   | ↘    |      |
| Traffic Volume (veh/h)            | 1147 | 7    | 8     | 1390                 | 7    | 7    |
| Future Volume (Veh/h)             | 1147 | 7    | 8     | 1390                 | 7    | 7    |
| Sign Control                      | Free |      |       | Free                 | Stop |      |
| Grade                             | 0%   |      |       | 0%                   | 0%   |      |
| Peak Hour Factor                  | 0.92 | 0.92 | 0.89  | 0.89                 | 0.58 | 0.58 |
| Hourly flow rate (vph)            | 1309 | 8    | 9     | 1640                 | 13   | 13   |
| Pedestrians                       |      |      |       |                      |      |      |
| Lane Width (ft)                   |      |      |       |                      |      |      |
| Walking Speed (ft/s)              |      |      |       |                      |      |      |
| Percent Blockage                  |      |      |       |                      |      |      |
| Right turn flare (veh)            |      |      |       |                      |      |      |
| Median type                       | None |      | TWLTL |                      |      |      |
| Median storage (veh)              | 2    |      |       |                      |      |      |
| Upstream signal (ft)              | 335  |      |       |                      |      |      |
| pX, platoon unblocked             |      |      | 0.70  |                      | 0.70 | 0.70 |
| vC, conflicting volume            |      |      | 1317  |                      | 2151 | 658  |
| vC1, stage 1 conf vol             |      |      |       |                      | 1313 |      |
| vC2, stage 2 conf vol             |      |      |       |                      | 838  |      |
| vCu, unblocked vol                |      |      | 593   |                      | 1786 | 0    |
| tC, single (s)                    |      |      | 4.1   |                      | 7.1  | 7.2  |
| tC, 2 stage (s)                   |      |      |       |                      | 6.1  |      |
| tF (s)                            |      |      | 2.2   |                      | 3.6  | 3.4  |
| p0 queue free %                   |      |      | 99    |                      | 95   | 98   |
| cM capacity (veh/h)               |      |      | 694   |                      | 239  | 732  |
| Direction, Lane #                 | EB 1 | EB 2 | WB 1  | WB 2                 | WB 3 | NB 1 |
| Volume Total                      | 873  | 444  | 9     | 820                  | 820  | 26   |
| Volume Left                       | 0    | 0    | 9     | 0                    | 0    | 13   |
| Volume Right                      | 0    | 8    | 0     | 0                    | 0    | 13   |
| cSH                               | 1700 | 1700 | 694   | 1700                 | 1700 | 361  |
| Volume to Capacity                | 0.51 | 0.26 | 0.01  | 0.48                 | 0.48 | 0.07 |
| Queue Length 95th (ft)            | 0    | 0    | 1     | 0                    | 0    | 6    |
| Control Delay (s)                 | 0.0  | 0.0  | 10.3  | 0.0                  | 0.0  | 15.8 |
| Lane LOS                          | B    |      |       | C                    |      |      |
| Approach Delay (s)                | 0.0  |      | 0.1   |                      |      | 15.8 |
| Approach LOS                      |      |      |       | C                    |      |      |
| Intersection Summary              |      |      |       |                      |      |      |
| Average Delay                     |      |      | 0.2   |                      |      |      |
| Intersection Capacity Utilization |      |      | 50.3% | ICU Level of Service |      | A    |
| Analysis Period (min)             |      |      | 15    |                      |      |      |

# HCM Unsignalized Intersection Capacity Analysis

## 9: University Park/Roca's & Route 11

12/8/2015



| Movement               | EBL   | EBT  | EBR  | WBL  | WBT  | WBR  | NBL  | NBT  | NBR  | SBL  | SBT  | SBR  |
|------------------------|-------|------|------|------|------|------|------|------|------|------|------|------|
| Lane Configurations    |       |      |      |      |      |      |      |      |      |      |      |      |
| Traffic Volume (veh/h) | 11    | 1146 | 4    | 4    | 1401 | 0    | 5    | 0    | 8    | 1    | 0    | 2    |
| Future Volume (Veh/h)  | 11    | 1146 | 4    | 4    | 1401 | 0    | 5    | 0    | 8    | 1    | 0    | 2    |
| Sign Control           |       | Free |      |      | Free |      |      | Stop |      |      | Stop |      |
| Grade                  |       | 0%   |      |      | 0%   |      |      | 0%   |      |      | 0%   |      |
| Peak Hour Factor       | 0.88  | 0.88 | 0.88 | 0.92 | 0.92 | 0.92 | 0.54 | 0.54 | 0.54 | 0.75 | 0.75 | 0.75 |
| Hourly flow rate (vph) | 13    | 1367 | 5    | 5    | 1599 | 0    | 10   | 0    | 16   | 1    | 0    | 3    |
| Pedestrians            |       |      |      |      |      |      |      |      |      |      |      |      |
| Lane Width (ft)        |       |      |      |      |      |      |      |      |      |      |      |      |
| Walking Speed (ft/s)   |       |      |      |      |      |      |      |      |      |      |      |      |
| Percent Blockage       |       |      |      |      |      |      |      |      |      |      |      |      |
| Right turn flare (veh) |       |      |      |      |      |      |      |      |      |      |      |      |
| Median type            | TWLTL |      |      |      |      | None |      |      |      |      |      |      |
| Median storage veh     | 2     |      |      |      |      |      |      |      |      |      |      |      |
| Upstream signal (ft)   | 792   |      |      |      |      |      |      |      |      |      |      |      |
| pX, platoon unblocked  |       |      |      | 0.72 |      |      | 0.72 | 0.72 | 0.72 | 0.72 | 0.72 |      |
| vC, conflicting volume | 1599  |      |      | 1372 |      |      | 2208 | 3004 | 686  | 2334 | 3007 | 800  |
| vC1, stage 1 conf vol  |       |      |      |      |      |      | 1396 | 1396 |      | 1609 | 1609 |      |
| vC2, stage 2 conf vol  |       |      |      |      |      |      | 812  | 1609 |      | 726  | 1398 |      |
| vCu, unblocked vol     | 1599  |      |      | 738  |      |      | 1900 | 3006 | 0    | 2075 | 3010 | 800  |
| tC, single (s)         | 4.1   |      |      | 4.1  |      |      | 7.5  | 6.5  | 6.9  | 7.5  | 6.5  | 6.9  |
| tC, 2 stage (s)        |       |      |      |      |      |      | 6.5  | 5.5  |      | 6.5  | 5.5  |      |
| tF (s)                 | 2.2   |      |      | 2.2  |      |      | 3.5  | 4.0  | 3.3  | 3.5  | 4.0  | 3.3  |
| p0 queue free %        | 97    |      |      | 99   |      |      | 95   | 100  | 98   | 99   | 100  | 99   |
| cM capacity (veh/h)    | 415   |      |      | 631  |      |      | 194  | 127  | 785  | 107  | 134  | 332  |

| Direction, Lane #      | EB 1 | EB 2 | EB 3 | WB 1 | WB 2 | WB 3 | NB 1 | SB 1 |
|------------------------|------|------|------|------|------|------|------|------|
| Volume Total           | 13   | 911  | 461  | 5    | 1066 | 533  | 26   | 4    |
| Volume Left            | 13   | 0    | 0    | 5    | 0    | 0    | 10   | 1    |
| Volume Right           | 0    | 0    | 5    | 0    | 0    | 0    | 16   | 3    |
| cSH                    | 415  | 1700 | 1700 | 631  | 1700 | 1700 | 361  | 217  |
| Volume to Capacity     | 0.03 | 0.54 | 0.27 | 0.01 | 0.63 | 0.31 | 0.07 | 0.02 |
| Queue Length 95th (ft) | 2    | 0    | 0    | 1    | 0    | 0    | 6    | 1    |
| Control Delay (s)      | 14.0 | 0.0  | 0.0  | 10.7 | 0.0  | 0.0  | 15.7 | 21.9 |
| Lane LOS               | B    |      |      | B    |      |      | C    | C    |
| Approach Delay (s)     | 0.1  |      |      | 0.0  |      |      | 15.7 | 21.9 |
| Approach LOS           |      |      |      |      |      |      | C    | C    |

### Intersection Summary

|                                   |       |
|-----------------------------------|-------|
| Average Delay                     | 0.2   |
| Intersection Capacity Utilization | 50.7% |
| ICU Level of Service              | A     |
| Analysis Period (min)             | 15    |

# HCM Unsignalized Intersection Capacity Analysis

## 10: Route 11 & Nesslerod

12/8/2015



| Movement                          | EBL  | EBT    | WBT    | WBR  | SBL                  | SBR  |
|-----------------------------------|------|--------|--------|------|----------------------|------|
| Lane Configurations               |      |        |        |      |                      |      |
| Traffic Volume (veh/h)            | 8    | 1147   | 1413   | 1    | 2                    | 2    |
| Future Volume (Veh/h)             | 8    | 1147   | 1413   | 1    | 2                    | 2    |
| Sign Control                      |      | Free   | Free   |      | Stop                 |      |
| Grade                             |      | 0%     | 0%     |      | 0%                   |      |
| Peak Hour Factor                  | 0.88 | 0.88   | 0.92   | 0.92 | 0.50                 | 0.50 |
| Hourly flow rate (vph)            | 10   | 1369   | 1613   | 1    | 4                    | 4    |
| Pedestrians                       |      |        |        |      |                      |      |
| Lane Width (ft)                   |      |        |        |      |                      |      |
| Walking Speed (ft/s)              |      |        |        |      |                      |      |
| Percent Blockage                  |      |        |        |      |                      |      |
| Right turn flare (veh)            |      |        |        |      |                      |      |
| Median type                       |      | Raised | Raised |      |                      |      |
| Median storage (veh)              |      | 1      | 1      |      |                      |      |
| Upstream signal (ft)              |      | 1287   |        |      |                      |      |
| pX, platoon unblocked             |      |        |        |      | 0.75                 |      |
| vC, conflicting volume            | 1614 |        |        |      | 2318                 | 807  |
| vC1, stage 1 conf vol             |      |        |        |      | 1614                 |      |
| vC2, stage 2 conf vol             |      |        |        |      | 704                  |      |
| vCu, unblocked vol                | 1614 |        |        |      | 2085                 | 807  |
| tC, single (s)                    | 4.1  |        |        |      | 6.8                  | 6.9  |
| tC, 2 stage (s)                   |      |        |        |      | 5.8                  |      |
| tF (s)                            | 2.2  |        |        |      | 3.5                  | 3.3  |
| p0 queue free %                   | 98   |        |        |      | 97                   | 99   |
| cM capacity (veh/h)               | 410  |        |        |      | 123                  | 329  |
| Direction, Lane #                 | EB 1 | EB 2   | EB 3   | WB 1 | WB 2                 | SB 1 |
| Volume Total                      | 10   | 684    | 684    | 1075 | 539                  | 8    |
| Volume Left                       | 10   | 0      | 0      | 0    | 0                    | 4    |
| Volume Right                      | 0    | 0      | 0      | 0    | 1                    | 4    |
| cSH                               | 410  | 1700   | 1700   | 1700 | 1700                 | 179  |
| Volume to Capacity                | 0.02 | 0.40   | 0.40   | 0.63 | 0.32                 | 0.04 |
| Queue Length 95th (ft)            | 2    | 0      | 0      | 0    | 0                    | 3    |
| Control Delay (s)                 | 14.0 | 0.0    | 0.0    | 0.0  | 0.0                  | 26.0 |
| Lane LOS                          | B    |        |        |      |                      | D    |
| Approach Delay (s)                | 0.1  |        |        | 0.0  |                      | 26.0 |
| Approach LOS                      |      |        |        |      |                      | D    |
| Intersection Summary              |      |        |        |      |                      |      |
| Average Delay                     |      |        | 0.1    |      |                      |      |
| Intersection Capacity Utilization |      |        | 51.0%  |      | ICU Level of Service | A    |
| Analysis Period (min)             |      |        | 15     |      |                      |      |

# HCM Unsignalized Intersection Capacity Analysis

## 11: Hazel Hollow Rd & Route 11

12/8/2015



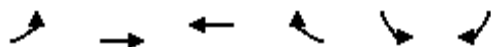
| Movement                          | EBT         | EBR         | WBL         | WBT                  | NBL         | NBR         |             |
|-----------------------------------|-------------|-------------|-------------|----------------------|-------------|-------------|-------------|
| Lane Configurations               | ↑↑          | ↑           | ↓           | ↑↑                   | ↓           |             |             |
| Traffic Volume (veh/h)            | 1116        | 33          | 43          | 1398                 | 16          | 31          |             |
| Future Volume (Veh/h)             | 1116        | 33          | 43          | 1398                 | 16          | 31          |             |
| Sign Control                      | Free        |             |             | Free                 | Stop        |             |             |
| Grade                             | 0%          |             |             | 0%                   | 0%          |             |             |
| Peak Hour Factor                  | 0.88        | 0.88        | 0.92        | 0.92                 | 0.78        | 0.78        |             |
| Hourly flow rate (vph)            | 1332        | 39          | 49          | 1596                 | 22          | 42          |             |
| <b>Pedestrians</b>                |             |             |             |                      |             |             |             |
| Lane Width (ft)                   |             |             |             |                      |             |             |             |
| Walking Speed (ft/s)              |             |             |             |                      |             |             |             |
| Percent Blockage                  |             |             |             |                      |             |             |             |
| Right turn flare (veh)            |             |             |             |                      |             |             |             |
| Median type                       | Raised      |             | Raised      |                      |             |             |             |
| Median storage (veh)              | 1           |             | 1           |                      |             |             |             |
| Upstream signal (ft)              |             |             |             |                      |             |             |             |
| pX, platoon unblocked             |             |             |             |                      |             |             |             |
| vC, conflicting volume            |             |             | 1371        |                      | 2228        | 666         |             |
| vC1, stage 1 conf vol             |             |             |             |                      | 1332        |             |             |
| vC2, stage 2 conf vol             |             |             |             |                      | 896         |             |             |
| vCu, unblocked vol                |             |             | 1371        |                      | 2228        | 666         |             |
| tC, single (s)                    |             |             | 4.1         |                      | 6.8         | 7.0         |             |
| tC, 2 stage (s)                   |             |             |             |                      | 5.8         |             |             |
| tF (s)                            |             |             | 2.2         |                      | 3.5         | 3.3         |             |
| p0 queue free %                   |             |             | 90          |                      | 83          | 89          |             |
| cM capacity (veh/h)               |             |             | 507         |                      | 133         | 400         |             |
| <b>Direction, Lane #</b>          | <b>EB 1</b> | <b>EB 2</b> | <b>EB 3</b> | <b>WB 1</b>          | <b>WB 2</b> | <b>WB 3</b> | <b>NB 1</b> |
| Volume Total                      | 666         | 666         | 39          | 49                   | 798         | 798         | 64          |
| Volume Left                       | 0           | 0           | 0           | 49                   | 0           | 0           | 22          |
| Volume Right                      | 0           | 0           | 39          | 0                    | 0           | 0           | 42          |
| cSH                               | 1700        | 1700        | 1700        | 507                  | 1700        | 1700        | 237         |
| Volume to Capacity                | 0.39        | 0.39        | 0.02        | 0.10                 | 0.47        | 0.47        | 0.27        |
| Queue Length 95th (ft)            | 0           | 0           | 0           | 8                    | 0           | 0           | 26          |
| Control Delay (s)                 | 0.0         | 0.0         | 0.0         | 12.9                 | 0.0         | 0.0         | 25.8        |
| Lane LOS                          |             |             |             | B                    | D           |             |             |
| Approach Delay (s)                | 0.0         |             |             | 0.4                  | 25.8        |             |             |
| Approach LOS                      |             |             |             | D                    |             |             |             |
| <b>Intersection Summary</b>       |             |             |             |                      |             |             |             |
| Average Delay                     |             |             | 0.7         |                      |             |             |             |
| Intersection Capacity Utilization |             |             | 50.6%       | ICU Level of Service |             | A           |             |
| Analysis Period (min)             |             |             | 15          |                      |             |             |             |



# HCM Signalized Intersection Capacity Analysis

## 2: Route 11 & Route 114

12/8/2015



| Movement               | EBL   | EBT  | WBT  | WBR  | SBL   | SBR   |
|------------------------|-------|------|------|------|-------|-------|
| Lane Configurations    |       |      |      |      |       |       |
| Traffic Volume (vph)   | 364   | 407  | 209  | 256  | 488   | 281   |
| Future Volume (vph)    | 364   | 407  | 209  | 256  | 488   | 281   |
| Ideal Flow (vphpl)     | 1900  | 1900 | 1900 | 1900 | 1900  | 1900  |
| Total Lost time (s)    | 5.5   | 6.0  | 6.0  | 4.0  | 5.5   | 4.0   |
| Lane Util. Factor      | 0.97  | 0.95 | 0.95 | 1.00 | 0.97  | 1.00  |
| Frt                    | 1.00  | 1.00 | 1.00 | 0.85 | 1.00  | 0.85  |
| Flt Protected          | 0.95  | 1.00 | 1.00 | 1.00 | 0.95  | 1.00  |
| Satd. Flow (prot)      | 3335  | 3574 | 3539 | 1553 | 3467  | 1524  |
| Flt Permitted          | 0.95  | 1.00 | 1.00 | 1.00 | 0.95  | 1.00  |
| Satd. Flow (perm)      | 3335  | 3574 | 3539 | 1553 | 3467  | 1524  |
| Peak-hour factor, PHF  | 0.86  | 0.86 | 0.92 | 0.92 | 0.88  | 0.88  |
| Growth Factor (vph)    | 105%  | 105% | 105% | 105% | 105%  | 105%  |
| Adj. Flow (vph)        | 444   | 497  | 239  | 292  | 582   | 335   |
| RTOR Reduction (vph)   | 0     | 0    | 0    | 0    | 0     | 0     |
| Lane Group Flow (vph)  | 444   | 497  | 239  | 292  | 582   | 335   |
| Heavy Vehicles (%)     | 5%    | 1%   | 2%   | 4%   | 1%    | 6%    |
| Turn Type              | Prot  | NA   | NA   | Free | Prot  | Free  |
| Protected Phases       | 1     | 6    | 2    |      | 4     |       |
| Permitted Phases       |       |      |      | Free |       | Free  |
| Actuated Green, G (s)  | 15.6  | 35.6 | 14.5 | 66.0 | 18.9  | 66.0  |
| Effective Green, g (s) | 15.6  | 35.6 | 14.5 | 66.0 | 18.9  | 66.0  |
| Actuated g/C Ratio     | 0.24  | 0.54 | 0.22 | 1.00 | 0.29  | 1.00  |
| Clearance Time (s)     | 5.5   | 6.0  | 6.0  |      | 5.5   |       |
| Vehicle Extension (s)  | 4.0   | 4.5  | 4.5  |      | 4.5   |       |
| Lane Grp Cap (vph)     | 788   | 1927 | 777  | 1553 | 992   | 1524  |
| v/s Ratio Prot         | c0.13 | 0.14 | 0.07 |      | c0.17 |       |
| v/s Ratio Perm         |       |      |      | 0.19 |       | c0.22 |
| v/c Ratio              | 0.56  | 0.26 | 0.31 | 0.19 | 0.59  | 0.22  |
| Uniform Delay, d1      | 22.2  | 8.1  | 21.5 | 0.0  | 20.2  | 0.0   |
| Progression Factor     | 1.00  | 1.00 | 1.00 | 1.00 | 1.00  | 1.00  |
| Incremental Delay, d2  | 1.1   | 0.1  | 0.4  | 0.3  | 1.2   | 0.3   |
| Delay (s)              | 23.3  | 8.3  | 21.9 | 0.3  | 21.4  | 0.3   |
| Level of Service       | C     | A    | C    | A    | C     | A     |
| Approach Delay (s)     |       | 15.4 | 10.0 |      | 13.7  |       |
| Approach LOS           |       | B    | B    |      | B     |       |

### Intersection Summary

|                                   |       |                           |      |
|-----------------------------------|-------|---------------------------|------|
| HCM 2000 Control Delay            | 13.5  | HCM 2000 Level of Service | B    |
| HCM 2000 Volume to Capacity ratio | 0.50  |                           |      |
| Actuated Cycle Length (s)         | 66.0  | Sum of lost time (s)      | 17.0 |
| Intersection Capacity Utilization | 51.4% | ICU Level of Service      | A    |
| Analysis Period (min)             | 15    |                           |      |

c Critical Lane Group

# HCM Signalized Intersection Capacity Analysis

## 6: Kroger/The Rocks & Route 11

12/8/2015



| Movement               | EBL  | EBT   | EBR  | WBL   | WBT  | WBR  | NBL   | NBT  | NBR  | SBL  | SBT  | SBR  |
|------------------------|------|-------|------|-------|------|------|-------|------|------|------|------|------|
| Lane Configurations    | ↖    | ↗↗    | ↖    | ↖     | ↗↗   |      | ↖     | ↗    |      |      | ↖↗   | ↖    |
| Traffic Volume (vph)   | 25   | 757   | 106  | 47    | 545  | 3    | 58    | 2    | 54   | 34   | 3    | 45   |
| Future Volume (vph)    | 25   | 757   | 106  | 47    | 545  | 3    | 58    | 2    | 54   | 34   | 3    | 45   |
| Ideal Flow (vphpl)     | 1900 | 1900  | 1900 | 1900  | 1900 | 1900 | 1900  | 1900 | 1900 | 1900 | 1900 | 1900 |
| Total Lost time (s)    | 5.5  | 6.0   | 6.0  | 5.5   | 6.0  |      | 5.5   | 5.5  |      |      | 5.5  | 5.5  |
| Lane Util. Factor      | 1.00 | 0.95  | 1.00 | 1.00  | 0.95 |      | 1.00  | 1.00 |      |      | 1.00 | 1.00 |
| Frt                    | 1.00 | 1.00  | 0.85 | 1.00  | 1.00 |      | 1.00  | 0.85 |      |      | 1.00 | 0.85 |
| Flt Protected          | 0.95 | 1.00  | 1.00 | 0.95  | 1.00 |      | 0.95  | 1.00 |      |      | 0.96 | 1.00 |
| Satd. Flow (prot)      | 1736 | 3539  | 1568 | 1656  | 3537 |      | 1719  | 1570 |      |      | 1817 | 1583 |
| Flt Permitted          | 0.95 | 1.00  | 1.00 | 0.95  | 1.00 |      | 0.42  | 1.00 |      |      | 0.70 | 1.00 |
| Satd. Flow (perm)      | 1736 | 3539  | 1568 | 1656  | 3537 |      | 759   | 1570 |      |      | 1322 | 1583 |
| Peak-hour factor, PHF  | 0.88 | 0.88  | 0.88 | 0.93  | 0.93 | 0.93 | 0.86  | 0.86 | 0.86 | 0.85 | 0.85 | 0.85 |
| Growth Factor (vph)    | 105% | 105%  | 105% | 105%  | 105% | 105% | 105%  | 105% | 105% | 105% | 105% | 105% |
| Adj. Flow (vph)        | 30   | 903   | 126  | 53    | 615  | 3    | 71    | 2    | 66   | 42   | 4    | 56   |
| RTOR Reduction (vph)   | 0    | 0     | 66   | 0     | 0    | 0    | 0     | 50   | 0    | 0    | 0    | 51   |
| Lane Group Flow (vph)  | 30   | 903   | 60   | 53    | 618  | 0    | 71    | 18   | 0    | 0    | 46   | 5    |
| Heavy Vehicles (%)     | 4%   | 2%    | 3%   | 9%    | 2%   | 0%   | 5%    | 50%  | 2%   | 0%   | 0%   | 2%   |
| Turn Type              | Prot | NA    | Perm | Prot  | NA   |      | pm+pt | NA   |      | Perm | NA   | Perm |
| Protected Phases       | 1    | 6     |      | 5     | 2    |      | 3     | 8    |      |      | 4    |      |
| Permitted Phases       |      |       | 6    |       |      |      | 8     |      |      | 4    |      | 4    |
| Actuated Green, G (s)  | 2.8  | 37.7  | 37.7 | 5.0   | 39.9 |      | 19.0  | 19.0 |      |      | 7.5  | 7.5  |
| Effective Green, g (s) | 2.8  | 37.7  | 37.7 | 5.0   | 39.9 |      | 19.0  | 19.0 |      |      | 7.5  | 7.5  |
| Actuated g/C Ratio     | 0.04 | 0.48  | 0.48 | 0.06  | 0.51 |      | 0.24  | 0.24 |      |      | 0.10 | 0.10 |
| Clearance Time (s)     | 5.5  | 6.0   | 6.0  | 5.5   | 6.0  |      | 5.5   | 5.5  |      |      | 5.5  | 5.5  |
| Vehicle Extension (s)  | 3.5  | 4.0   | 4.0  | 3.5   | 4.0  |      | 3.0   | 4.0  |      |      | 4.0  | 4.0  |
| Lane Grp Cap (vph)     | 61   | 1695  | 751  | 105   | 1793 |      | 256   | 379  |      |      | 125  | 150  |
| v/s Ratio Prot         | 0.02 | c0.26 |      | c0.03 | 0.17 |      | c0.02 | 0.01 |      |      |      |      |
| v/s Ratio Perm         |      |       | 0.04 |       |      |      | c0.05 |      |      |      | 0.03 | 0.00 |
| v/c Ratio              | 0.49 | 0.53  | 0.08 | 0.50  | 0.34 |      | 0.28  | 0.05 |      |      | 0.37 | 0.04 |
| Uniform Delay, d1      | 37.3 | 14.3  | 11.1 | 35.7  | 11.6 |      | 23.8  | 22.9 |      |      | 33.4 | 32.3 |
| Progression Factor     | 1.00 | 1.00  | 1.00 | 1.00  | 1.00 |      | 1.00  | 1.00 |      |      | 1.00 | 1.00 |
| Incremental Delay, d2  | 7.2  | 0.4   | 0.1  | 4.5   | 0.2  |      | 0.6   | 0.1  |      |      | 2.5  | 0.1  |
| Delay (s)              | 44.4 | 14.8  | 11.2 | 40.1  | 11.7 |      | 24.4  | 23.0 |      |      | 35.9 | 32.4 |
| Level of Service       | D    | B     | B    | D     | B    |      | C     | C    |      |      | D    | C    |
| Approach Delay (s)     |      | 15.2  |      |       | 14.0 |      |       | 23.7 |      |      | 34.0 |      |
| Approach LOS           |      | B     |      |       | B    |      |       | C    |      |      | C    |      |

### Intersection Summary

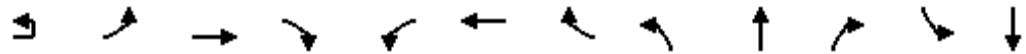
|                                   |       |                           |      |
|-----------------------------------|-------|---------------------------|------|
| HCM 2000 Control Delay            | 16.3  | HCM 2000 Level of Service | B    |
| HCM 2000 Volume to Capacity ratio | 0.49  |                           |      |
| Actuated Cycle Length (s)         | 78.7  | Sum of lost time (s)      | 22.5 |
| Intersection Capacity Utilization | 51.2% | ICU Level of Service      | A    |
| Analysis Period (min)             | 15    |                           |      |

c Critical Lane Group

# HCM Unsignalized Intersection Capacity Analysis

## 3: Cherry Ct/Driveway & Route 11

12/8/2015



| Movement                          | EBU         | EBL         | EBT         | EBR         | WBL         | WBT                  | WBR         | NBL         | NBT  | NBR  | SBL  | SBT  |
|-----------------------------------|-------------|-------------|-------------|-------------|-------------|----------------------|-------------|-------------|------|------|------|------|
| Lane Configurations               |             | ↔           | ↕           |             | ↕           | ↕                    |             |             | ↕    |      |      | ↕    |
| Traffic Volume (veh/h)            | 3           | 3           | 888         | 1           | 0           | 463                  | 0           | 3           | 0    | 3    | 0    | 0    |
| Future Volume (Veh/h)             | 3           | 3           | 888         | 1           | 0           | 463                  | 0           | 3           | 0    | 3    | 0    | 0    |
| Sign Control                      |             |             | Free        |             |             | Free                 |             |             | Stop |      |      | Stop |
| Grade                             |             |             | 0%          |             |             | 0%                   |             |             | 0%   |      |      | 0%   |
| Peak Hour Factor                  | 0.87        | 0.87        | 0.87        | 0.87        | 0.92        | 0.92                 | 0.92        | 0.30        | 0.30 | 0.30 | 0.38 | 0.38 |
| Hourly flow rate (vph)            | 0           | 4           | 1072        | 1           | 0           | 528                  | 0           | 11          | 0    | 11   | 0    | 0    |
| Pedestrians                       |             |             |             |             |             |                      |             |             |      |      |      |      |
| Lane Width (ft)                   |             |             |             |             |             |                      |             |             |      |      |      |      |
| Walking Speed (ft/s)              |             |             |             |             |             |                      |             |             |      |      |      |      |
| Percent Blockage                  |             |             |             |             |             |                      |             |             |      |      |      |      |
| Right turn flare (veh)            |             |             |             |             |             |                      |             |             |      |      |      |      |
| Median type                       |             |             | None        |             |             | TWLTL                |             |             |      |      |      |      |
| Median storage (veh)              |             |             |             |             |             | 2                    |             |             |      |      |      |      |
| Upstream signal (ft)              |             |             | 779         |             |             |                      |             |             |      |      |      |      |
| pX, platoon unblocked             | 0.00        |             |             |             | 0.96        |                      |             | 0.96        | 0.96 | 0.96 | 0.96 | 0.96 |
| vC, conflicting volume            | 0           | 528         |             |             | 1073        |                      |             | 1352        | 1608 | 536  | 1083 | 1609 |
| vC1, stage 1 conf vol             |             |             |             |             |             |                      |             | 1080        | 1080 |      | 528  | 528  |
| vC2, stage 2 conf vol             |             |             |             |             |             |                      |             | 272         | 528  |      | 555  | 1081 |
| vCu, unblocked vol                | 0           | 528         |             |             | 986         |                      |             | 1278        | 1546 | 426  | 997  | 1547 |
| tC, single (s)                    | 0.0         | 4.1         |             |             | 4.1         |                      |             | 7.5         | 6.5  | 6.9  | 7.5  | 6.5  |
| tC, 2 stage (s)                   |             |             |             |             |             |                      |             | 6.5         | 5.5  |      | 6.5  | 5.5  |
| tF (s)                            | 0.0         | 2.2         |             |             | 2.2         |                      |             | 3.5         | 4.0  | 3.3  | 3.5  | 4.0  |
| p0 queue free %                   | 0           | 100         |             |             | 100         |                      |             | 95          | 100  | 98   | 100  | 100  |
| cM capacity (veh/h)               | 0           | 1049        |             |             | 678         |                      |             | 243         | 277  | 558  | 398  | 277  |
| <b>Direction, Lane #</b>          | <b>EB 1</b> | <b>EB 2</b> | <b>EB 3</b> | <b>WB 1</b> | <b>WB 2</b> | <b>WB 3</b>          | <b>NB 1</b> | <b>SB 1</b> |      |      |      |      |
| Volume Total                      | 4           | 715         | 358         | 0           | 352         | 176                  | 22          | 8           |      |      |      |      |
| Volume Left                       | 4           | 0           | 0           | 0           | 0           | 0                    | 11          | 0           |      |      |      |      |
| Volume Right                      | 0           | 0           | 1           | 0           | 0           | 0                    | 11          | 8           |      |      |      |      |
| cSH                               | 1049        | 1700        | 1700        | 1700        | 1700        | 1700                 | 338         | 741         |      |      |      |      |
| Volume to Capacity                | 0.00        | 0.42        | 0.21        | 0.00        | 0.21        | 0.10                 | 0.07        | 0.01        |      |      |      |      |
| Queue Length 95th (ft)            | 0           | 0           | 0           | 0           | 0           | 0                    | 5           | 1           |      |      |      |      |
| Control Delay (s)                 | 8.4         | 0.0         | 0.0         | 0.0         | 0.0         | 0.0                  | 16.4        | 9.9         |      |      |      |      |
| Lane LOS                          | A           |             |             |             |             |                      | C           | A           |      |      |      |      |
| Approach Delay (s)                | 0.0         |             |             | 0.0         |             |                      | 16.4        | 9.9         |      |      |      |      |
| Approach LOS                      |             |             |             |             |             |                      | C           | A           |      |      |      |      |
| <b>Intersection Summary</b>       |             |             |             |             |             |                      |             |             |      |      |      |      |
| Average Delay                     |             |             | 0.3         |             |             |                      |             |             |      |      |      |      |
| Intersection Capacity Utilization |             |             | 35.8%       |             |             | ICU Level of Service |             | A           |      |      |      |      |
| Analysis Period (min)             |             |             | 15          |             |             |                      |             |             |      |      |      |      |

# HCM Unsignalized Intersection Capacity Analysis

## 3: Cherry Ct/Driveway & Route 11

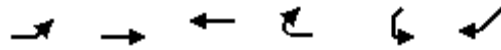
12/8/2015

| Movement               | SBR  |
|------------------------|------|
| Lane Configurations    |      |
| Traffic Volume (veh/h) | 3    |
| Future Volume (Veh/h)  | 3    |
| Sign Control           |      |
| Grade                  |      |
| Peak Hour Factor       | 0.38 |
| Hourly flow rate (vph) | 8    |
| Pedestrians            |      |
| Lane Width (ft)        |      |
| Walking Speed (ft/s)   |      |
| Percent Blockage       |      |
| Right turn flare (veh) |      |
| Median type            |      |
| Median storage veh)    |      |
| Upstream signal (ft)   |      |
| pX, platoon unblocked  |      |
| vC, conflicting volume | 264  |
| vC1, stage 1 conf vol  |      |
| vC2, stage 2 conf vol  |      |
| vCu, unblocked vol     | 264  |
| tC, single (s)         | 6.9  |
| tC, 2 stage (s)        |      |
| tF (s)                 | 3.3  |
| p0 queue free %        | 99   |
| cM capacity (veh/h)    | 741  |
| Direction, Lane #      |      |

# HCM Unsignalized Intersection Capacity Analysis

## 4: Route 11 & Belspring Rd

12/8/2015



| Movement                          | EBL  | EBT   | WBT   | WBR  | SWL                  | SWR  |
|-----------------------------------|------|-------|-------|------|----------------------|------|
| Lane Configurations               | ↶    | ↷     | ↷     |      | ↶                    |      |
| Traffic Volume (veh/h)            | 2    | 889   | 460   | 187  | 12                   | 3    |
| Future Volume (Veh/h)             | 2    | 889   | 460   | 187  | 12                   | 3    |
| Sign Control                      |      | Free  | Free  |      | Stop                 |      |
| Grade                             |      | 0%    | 0%    |      | 0%                   |      |
| Peak Hour Factor                  | 0.90 | 0.90  | 0.95  | 0.95 | 0.75                 | 0.75 |
| Hourly flow rate (vph)            | 2    | 1037  | 508   | 207  | 17                   | 4    |
| Pedestrians                       |      |       |       |      |                      |      |
| Lane Width (ft)                   |      |       |       |      |                      |      |
| Walking Speed (ft/s)              |      |       |       |      |                      |      |
| Percent Blockage                  |      |       |       |      |                      |      |
| Right turn flare (veh)            |      |       |       |      |                      |      |
| Median type                       |      |       |       |      |                      |      |
|                                   |      | TWLTL | TWLTL |      |                      |      |
| Median storage (veh)              |      | 2     | 2     |      |                      |      |
| Upstream signal (ft)              |      | 1075  | 1303  |      |                      |      |
| pX, platoon unblocked             | 0.95 |       |       |      | 0.95                 | 0.95 |
| vC, conflicting volume            | 715  |       |       |      | 1134                 | 358  |
| vC1, stage 1 conf vol             |      |       |       |      | 612                  |      |
| vC2, stage 2 conf vol             |      |       |       |      | 522                  |      |
| vCu, unblocked vol                | 587  |       |       |      | 1003                 | 210  |
| tC, single (s)                    | 4.1  |       |       |      | 6.8                  | 6.9  |
| tC, 2 stage (s)                   |      |       |       |      | 5.8                  |      |
| tF (s)                            | 2.2  |       |       |      | 3.5                  | 3.3  |
| p0 queue free %                   | 100  |       |       |      | 96                   | 99   |
| cM capacity (veh/h)               | 945  |       |       |      | 444                  | 760  |
| Direction, Lane #                 | EB 1 | EB 2  | EB 3  | WB 1 | WB 2                 | SW 1 |
| Volume Total                      | 2    | 518   | 518   | 339  | 376                  | 21   |
| Volume Left                       | 2    | 0     | 0     | 0    | 0                    | 17   |
| Volume Right                      | 0    | 0     | 0     | 0    | 207                  | 4    |
| cSH                               | 945  | 1700  | 1700  | 1700 | 1700                 | 482  |
| Volume to Capacity                | 0.00 | 0.30  | 0.30  | 0.20 | 0.22                 | 0.04 |
| Queue Length 95th (ft)            | 0    | 0     | 0     | 0    | 0                    | 3    |
| Control Delay (s)                 | 8.8  | 0.0   | 0.0   | 0.0  | 0.0                  | 12.8 |
| Lane LOS                          | A    |       |       |      |                      | B    |
| Approach Delay (s)                | 0.0  |       |       | 0.0  |                      | 12.8 |
| Approach LOS                      |      |       |       |      |                      | B    |
| Intersection Summary              |      |       |       |      |                      |      |
| Average Delay                     |      |       | 0.2   |      |                      |      |
| Intersection Capacity Utilization |      |       | 35.8% |      | ICU Level of Service | A    |
| Analysis Period (min)             |      |       | 15    |      |                      |      |

# HCM Unsignalized Intersection Capacity Analysis

## 5: Driveway/Nicewander Way & Route 11

12/8/2015



| Movement               | EBL  | EBT   | EBR  | WBL  | WBT   | WBR  | NBL  | NBT  | NBR  | SBL  | SBT  | SBR  |
|------------------------|------|-------|------|------|-------|------|------|------|------|------|------|------|
| Lane Configurations    |      |       |      |      |       |      |      |      |      |      |      |      |
| Traffic Volume (veh/h) | 4    | 885   | 6    | 1    | 639   | 1    | 0    | 0    | 1    | 7    | 0    | 8    |
| Future Volume (Veh/h)  | 4    | 885   | 6    | 1    | 639   | 1    | 0    | 0    | 1    | 7    | 0    | 8    |
| Sign Control           |      | Free  |      |      | Free  |      |      | Stop |      |      | Stop |      |
| Grade                  |      | 0%    |      |      | 0%    |      |      | 0%   |      |      | 0%   |      |
| Peak Hour Factor       | 0.90 | 0.90  | 0.90 | 0.95 | 0.95  | 0.95 | 0.25 | 0.25 | 0.25 | 0.63 | 0.63 | 0.63 |
| Hourly flow rate (vph) | 5    | 1033  | 7    | 1    | 706   | 1    | 0    | 0    | 4    | 12   | 0    | 13   |
| Pedestrians            |      |       |      |      |       |      |      |      |      |      |      |      |
| Lane Width (ft)        |      |       |      |      |       |      |      |      |      |      |      |      |
| Walking Speed (ft/s)   |      |       |      |      |       |      |      |      |      |      |      |      |
| Percent Blockage       |      |       |      |      |       |      |      |      |      |      |      |      |
| Right turn flare (veh) |      |       |      |      |       |      |      |      |      |      |      |      |
| Median type            |      | TWLTL |      |      | TWLTL |      |      |      |      |      |      |      |
| Median storage (veh)   |      | 2     |      |      | 2     |      |      |      |      |      |      |      |
| Upstream signal (ft)   |      |       |      |      | 715   |      |      |      |      |      |      |      |
| pX, platoon unblocked  | 0.91 |       |      |      |       |      | 0.91 | 0.91 |      | 0.91 | 0.91 | 0.91 |
| vC, conflicting volume | 707  |       |      | 1040 |       |      | 1414 | 1756 | 520  | 1239 | 1758 | 354  |
| vC1, stage 1 conf vol  |      |       |      |      |       |      | 1046 | 1046 |      | 708  | 708  |      |
| vC2, stage 2 conf vol  |      |       |      |      |       |      | 368  | 709  |      | 530  | 1050 |      |
| vCu, unblocked vol     | 488  |       |      | 1040 |       |      | 1263 | 1637 | 520  | 1071 | 1640 | 101  |
| tC, single (s)         | 4.1  |       |      | 4.1  |       |      | 7.5  | 6.5  | 8.9  | 7.5  | 6.5  | 6.9  |
| tC, 2 stage (s)        |      |       |      |      |       |      | 6.5  | 5.5  |      | 6.5  | 5.5  |      |
| tF (s)                 | 2.2  |       |      | 2.2  |       |      | 3.5  | 4.0  | 4.3  | 3.5  | 4.0  | 3.3  |
| p0 queue free %        | 99   |       |      | 100  |       |      | 100  | 100  | 99   | 97   | 100  | 98   |
| cM capacity (veh/h)    | 991  |       |      | 676  |       |      | 237  | 267  | 311  | 371  | 266  | 860  |

| Direction, Lane #      | EB 1 | EB 2 | EB 3 | WB 1 | WB 2 | WB 3 | NB 1 | SB 1 |
|------------------------|------|------|------|------|------|------|------|------|
| Volume Total           | 5    | 689  | 351  | 1    | 471  | 236  | 4    | 25   |
| Volume Left            | 5    | 0    | 0    | 1    | 0    | 0    | 0    | 12   |
| Volume Right           | 0    | 0    | 7    | 0    | 0    | 1    | 4    | 13   |
| cSH                    | 991  | 1700 | 1700 | 676  | 1700 | 1700 | 311  | 527  |
| Volume to Capacity     | 0.01 | 0.41 | 0.21 | 0.00 | 0.28 | 0.14 | 0.01 | 0.05 |
| Queue Length 95th (ft) | 0    | 0    | 0    | 0    | 0    | 0    | 1    | 4    |
| Control Delay (s)      | 8.7  | 0.0  | 0.0  | 10.3 | 0.0  | 0.0  | 16.7 | 12.2 |
| Lane LOS               | A    |      |      | B    |      |      | C    | B    |
| Approach Delay (s)     | 0.0  |      |      | 0.0  |      |      | 16.7 | 12.2 |
| Approach LOS           |      |      |      |      |      |      | C    | B    |

### Intersection Summary

|                                   |       |
|-----------------------------------|-------|
| Average Delay                     | 0.2   |
| Intersection Capacity Utilization | 40.1% |
| ICU Level of Service              | A     |
| Analysis Period (min)             | 15    |

# HCM Unsignalized Intersection Capacity Analysis

## 8: Wheatland & Route 11

12/8/2015



| Movement                          | EBT   | EBR  | WBL   | WBT                  | NBL  | NBR  |
|-----------------------------------|-------|------|-------|----------------------|------|------|
| Lane Configurations               | ↑↑    |      | ↙     | ↑↑                   | ↘    |      |
| Traffic Volume (veh/h)            | 830   | 10   | 42    | 644                  | 6    | 56   |
| Future Volume (Veh/h)             | 830   | 10   | 42    | 644                  | 6    | 56   |
| Sign Control                      | Free  |      |       | Free                 | Stop |      |
| Grade                             | 0%    |      |       | 0%                   | 0%   |      |
| Peak Hour Factor                  | 0.88  | 0.88 | 0.92  | 0.92                 | 0.74 | 0.74 |
| Hourly flow rate (vph)            | 990   | 12   | 48    | 735                  | 9    | 79   |
| <b>Pedestrians</b>                |       |      |       |                      |      |      |
| Lane Width (ft)                   |       |      |       |                      |      |      |
| Walking Speed (ft/s)              |       |      |       |                      |      |      |
| Percent Blockage                  |       |      |       |                      |      |      |
| Right turn flare (veh)            |       |      |       |                      |      |      |
| Median type                       | TWLTL |      | TWLTL |                      |      |      |
| Median storage (veh)              | 2     |      | 2     |                      |      |      |
| Upstream signal (ft)              | 335   |      |       |                      |      |      |
| pX, platoon unblocked             |       |      | 0.82  |                      | 0.82 | 0.82 |
| vC, conflicting volume            |       |      | 1002  |                      | 1460 | 501  |
| vC1, stage 1 conf vol             |       |      |       |                      | 996  |      |
| vC2, stage 2 conf vol             |       |      |       |                      | 464  |      |
| vCu, unblocked vol                |       |      | 567   |                      | 1124 | 0    |
| tC, single (s)                    |       |      | 4.1   |                      | 7.1  | 6.9  |
| tC, 2 stage (s)                   |       |      |       |                      | 6.1  |      |
| tF (s)                            |       |      | 2.2   |                      | 3.7  | 3.3  |
| p0 queue free %                   |       |      | 94    |                      | 97   | 91   |
| cM capacity (veh/h)               |       |      | 822   |                      | 339  | 890  |
| Direction, Lane #                 | EB 1  | EB 2 | WB 1  | WB 2                 | WB 3 | NB 1 |
| Volume Total                      | 660   | 342  | 48    | 368                  | 368  | 88   |
| Volume Left                       | 0     | 0    | 48    | 0                    | 0    | 9    |
| Volume Right                      | 0     | 12   | 0     | 0                    | 0    | 79   |
| cSH                               | 1700  | 1700 | 822   | 1700                 | 1700 | 763  |
| Volume to Capacity                | 0.39  | 0.20 | 0.06  | 0.22                 | 0.22 | 0.12 |
| Queue Length 95th (ft)            | 0     | 0    | 5     | 0                    | 0    | 10   |
| Control Delay (s)                 | 0.0   | 0.0  | 9.6   | 0.0                  | 0.0  | 10.3 |
| Lane LOS                          | A     |      |       | B                    |      |      |
| Approach Delay (s)                | 0.0   |      | 0.6   |                      |      | 10.3 |
| Approach LOS                      |       |      |       | B                    |      |      |
| <b>Intersection Summary</b>       |       |      |       |                      |      |      |
| Average Delay                     |       |      | 0.7   |                      |      |      |
| Intersection Capacity Utilization |       |      | 41.7% | ICU Level of Service |      | A    |
| Analysis Period (min)             |       |      | 15    |                      |      |      |

# HCM Unsignalized Intersection Capacity Analysis

## 9: University Park/Roca's & Route 11

12/8/2015



| Movement                          | EBL         | EBT         | EBR         | WBL         | WBT                  | WBR         | NBL         | NBT         | NBR  | SBL  | SBT  | SBR  |  |
|-----------------------------------|-------------|-------------|-------------|-------------|----------------------|-------------|-------------|-------------|------|------|------|------|--|
| Lane Configurations               |             |             |             |             |                      |             |             |             |      |      |      |      |  |
| Traffic Volume (veh/h)            | 1           | 881         | 9           | 9           | 695                  | 0           | 1           | 0           | 1    | 0    | 0    | 0    |  |
| Future Volume (Veh/h)             | 1           | 881         | 9           | 9           | 695                  | 0           | 1           | 0           | 1    | 0    | 0    | 0    |  |
| Sign Control                      |             | Free        |             |             | Free                 |             |             | Stop        |      |      | Stop |      |  |
| Grade                             |             | 0%          |             |             | 0%                   |             |             | 0%          |      |      | 0%   |      |  |
| Peak Hour Factor                  | 0.90        | 0.90        | 0.90        | 0.98        | 0.98                 | 0.98        | 0.25        | 0.25        | 0.25 | 1.00 | 1.00 | 1.00 |  |
| Hourly flow rate (vph)            | 1           | 1028        | 11          | 10          | 745                  | 0           | 4           | 0           | 4    | 0    | 0    | 0    |  |
| Pedestrians                       |             |             |             |             |                      |             |             |             |      |      |      |      |  |
| Lane Width (ft)                   |             |             |             |             |                      |             |             |             |      |      |      |      |  |
| Walking Speed (ft/s)              |             |             |             |             |                      |             |             |             |      |      |      |      |  |
| Percent Blockage                  |             |             |             |             |                      |             |             |             |      |      |      |      |  |
| Right turn flare (veh)            |             |             |             |             |                      |             |             |             |      |      |      |      |  |
| Median type                       | TWLTL       |             |             |             | None                 |             |             |             |      |      |      |      |  |
| Median storage veh                | 2           |             |             |             |                      |             |             |             |      |      |      |      |  |
| Upstream signal (ft)              | 792         |             |             |             |                      |             |             |             |      |      |      |      |  |
| pX, platoon unblocked             |             |             |             | 0.83        |                      |             |             | 0.83        | 0.83 | 0.83 | 0.83 | 0.83 |  |
| vC, conflicting volume            | 745         |             |             | 1039        |                      |             | 1428        | 1800        | 520  | 1285 | 1806 | 372  |  |
| vC1, stage 1 conf vol             |             |             |             |             |                      |             | 1036        | 1036        |      |      | 765  | 765  |  |
| vC2, stage 2 conf vol             |             |             |             |             |                      |             | 392         | 765         |      |      | 520  | 1041 |  |
| vCu, unblocked vol                | 745         |             |             | 635         |                      |             | 1104        | 1554        | 9    | 932  | 1560 | 372  |  |
| tC, single (s)                    | 6.1         |             |             | 4.1         |                      |             | 7.5         | 6.5         | 6.9  | 7.5  | 6.5  | 6.9  |  |
| tC, 2 stage (s)                   |             |             |             |             |                      |             | 6.5         | 5.5         |      |      | 6.5  | 5.5  |  |
| tF (s)                            | 3.2         |             |             | 2.2         |                      |             | 3.5         | 4.0         | 3.3  | 3.5  | 4.0  | 3.3  |  |
| p0 queue free %                   | 100         |             |             | 99          |                      |             | 99          | 100         | 100  | 100  | 100  | 100  |  |
| cM capacity (veh/h)               | 435         |             |             | 794         |                      |             | 325         | 292         | 893  | 343  | 287  | 631  |  |
| <b>Direction, Lane #</b>          | <b>EB 1</b> | <b>EB 2</b> | <b>EB 3</b> | <b>WB 1</b> | <b>WB 2</b>          | <b>WB 3</b> | <b>NB 1</b> | <b>SB 1</b> |      |      |      |      |  |
| Volume Total                      | 1           | 685         | 354         | 10          | 497                  | 248         | 8           | 0           |      |      |      |      |  |
| Volume Left                       | 1           | 0           | 0           | 10          | 0                    | 0           | 4           | 0           |      |      |      |      |  |
| Volume Right                      | 0           | 0           | 11          | 0           | 0                    | 0           | 4           | 0           |      |      |      |      |  |
| cSH                               | 435         | 1700        | 1700        | 794         | 1700                 | 1700        | 477         | 1700        |      |      |      |      |  |
| Volume to Capacity                | 0.00        | 0.40        | 0.21        | 0.01        | 0.29                 | 0.15        | 0.02        | 0.00        |      |      |      |      |  |
| Queue Length 95th (ft)            | 0           | 0           | 0           | 1           | 0                    | 0           | 1           | 0           |      |      |      |      |  |
| Control Delay (s)                 | 13.3        | 0.0         | 0.0         | 9.6         | 0.0                  | 0.0         | 12.7        | 0.0         |      |      |      |      |  |
| Lane LOS                          | B           |             |             | A           |                      |             | B           | A           |      |      |      |      |  |
| Approach Delay (s)                | 0.0         |             |             | 0.1         |                      |             | 12.7        | 0.0         |      |      |      |      |  |
| Approach LOS                      |             |             |             |             |                      |             |             | B           | A    |      |      |      |  |
| <b>Intersection Summary</b>       |             |             |             |             |                      |             |             |             |      |      |      |      |  |
| Average Delay                     |             |             |             | 0.1         |                      |             |             |             |      |      |      |      |  |
| Intersection Capacity Utilization |             |             |             | 35.9%       | ICU Level of Service |             |             |             | A    |      |      |      |  |
| Analysis Period (min)             |             |             |             | 15          |                      |             |             |             |      |      |      |      |  |



# HCM Unsignalized Intersection Capacity Analysis

## 10: Route 11 & Nesslerod

12/8/2015



| Movement                          | EBL  | EBT    | WBT    | WBR  | SBL                  | SBR  |
|-----------------------------------|------|--------|--------|------|----------------------|------|
| Lane Configurations               |      |        |        |      |                      |      |
| Traffic Volume (veh/h)            | 1    | 881    | 704    | 0    | 1                    | 0    |
| Future Volume (Veh/h)             | 1    | 881    | 704    | 0    | 1                    | 0    |
| Sign Control                      |      | Free   | Free   |      | Stop                 |      |
| Grade                             |      | 0%     | 0%     |      | 0%                   |      |
| Peak Hour Factor                  | 0.90 | 0.90   | 0.98   | 0.98 | 0.25                 | 0.25 |
| Hourly flow rate (vph)            | 1    | 1028   | 754    | 0    | 4                    | 0    |
| Pedestrians                       |      |        |        |      |                      |      |
| Lane Width (ft)                   |      |        |        |      |                      |      |
| Walking Speed (ft/s)              |      |        |        |      |                      |      |
| Percent Blockage                  |      |        |        |      |                      |      |
| Right turn flare (veh)            |      |        |        |      |                      |      |
| Median type                       |      | Raised | Raised |      |                      |      |
| Median storage (veh)              |      | 1      | 1      |      |                      |      |
| Upstream signal (ft)              |      | 1287   |        |      |                      |      |
| pX, platoon unblocked             |      |        |        |      | 0.85                 |      |
| vC, conflicting volume            | 754  |        |        |      | 1270                 | 377  |
| vC1, stage 1 conf vol             |      |        |        |      | 754                  |      |
| vC2, stage 2 conf vol             |      |        |        |      | 516                  |      |
| vCu, unblocked vol                | 754  |        |        |      | 954                  | 377  |
| tC, single (s)                    | 4.1  |        |        |      | 6.8                  | 6.9  |
| tC, 2 stage (s)                   |      |        |        |      | 5.8                  |      |
| tF (s)                            | 2.2  |        |        |      | 3.5                  | 3.3  |
| p0 queue free %                   | 100  |        |        |      | 99                   | 100  |
| cM capacity (veh/h)               | 865  |        |        |      | 342                  | 626  |
| Direction, Lane #                 | EB 1 | EB 2   | EB 3   | WB 1 | WB 2                 | SB 1 |
| Volume Total                      | 1    | 514    | 514    | 503  | 251                  | 4    |
| Volume Left                       | 1    | 0      | 0      | 0    | 0                    | 4    |
| Volume Right                      | 0    | 0      | 0      | 0    | 0                    | 0    |
| cSH                               | 865  | 1700   | 1700   | 1700 | 1700                 | 342  |
| Volume to Capacity                | 0.00 | 0.30   | 0.30   | 0.30 | 0.15                 | 0.01 |
| Queue Length 95th (ft)            | 0    | 0      | 0      | 0    | 0                    | 1    |
| Control Delay (s)                 | 9.2  | 0.0    | 0.0    | 0.0  | 0.0                  | 15.6 |
| Lane LOS                          | A    |        |        |      |                      | C    |
| Approach Delay (s)                | 0.0  |        |        | 0.0  |                      | 15.6 |
| Approach LOS                      |      |        |        |      |                      | C    |
| Intersection Summary              |      |        |        |      |                      |      |
| Average Delay                     |      |        | 0.0    |      |                      |      |
| Intersection Capacity Utilization |      |        | 35.6%  |      | ICU Level of Service | A    |
| Analysis Period (min)             |      |        | 15     |      |                      |      |

# HCM Unsignalized Intersection Capacity Analysis

## 11: Hazel Hollow Rd & Route 11

12/8/2015

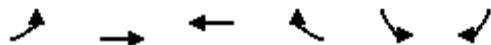


| Movement                          | EBT    | EBR  | WBL    | WBT                  | NBL  | NBR  |      |
|-----------------------------------|--------|------|--------|----------------------|------|------|------|
| Lane Configurations               | ↑↑     | ↑    | ↓      | ↑↑                   | ↓    |      |      |
| Traffic Volume (veh/h)            | 878    | 4    | 12     | 683                  | 21   | 28   |      |
| Future Volume (Veh/h)             | 878    | 4    | 12     | 683                  | 21   | 28   |      |
| Sign Control                      | Free   |      |        | Free                 | Stop |      |      |
| Grade                             | 0%     |      |        | 0%                   | 0%   |      |      |
| Peak Hour Factor                  | 0.90   | 0.90 | 0.98   | 0.98                 | 0.82 | 0.82 |      |
| Hourly flow rate (vph)            | 1024   | 5    | 13     | 732                  | 27   | 36   |      |
| Pedestrians                       |        |      |        |                      |      |      |      |
| Lane Width (ft)                   |        |      |        |                      |      |      |      |
| Walking Speed (ft/s)              |        |      |        |                      |      |      |      |
| Percent Blockage                  |        |      |        |                      |      |      |      |
| Right turn flare (veh)            |        |      |        |                      |      |      |      |
| Median type                       | Raised |      | Raised |                      |      |      |      |
| Median storage veh                | 1      |      | 1      |                      |      |      |      |
| Upstream signal (ft)              |        |      |        |                      |      |      |      |
| pX, platoon unblocked             |        |      |        |                      |      |      |      |
| vC, conflicting volume            |        |      | 1029   |                      | 1416 | 512  |      |
| vC1, stage 1 conf vol             |        |      |        |                      | 1024 |      |      |
| vC2, stage 2 conf vol             |        |      |        |                      | 392  |      |      |
| vCu, unblocked vol                |        |      | 1029   |                      | 1416 | 512  |      |
| tC, single (s)                    |        |      | 4.4    |                      | 6.8  | 6.9  |      |
| tC, 2 stage (s)                   |        |      |        |                      | 5.8  |      |      |
| tF (s)                            |        |      | 2.4    |                      | 3.5  | 3.3  |      |
| p0 queue free %                   |        |      | 98     |                      | 89   | 93   |      |
| cM capacity (veh/h)               |        |      | 588    |                      | 240  | 512  |      |
| Direction, Lane #                 | EB 1   | EB 2 | EB 3   | WB 1                 | WB 2 | WB 3 | NB 1 |
| Volume Total                      | 512    | 512  | 5      | 13                   | 366  | 366  | 63   |
| Volume Left                       | 0      | 0    | 0      | 13                   | 0    | 0    | 27   |
| Volume Right                      | 0      | 0    | 5      | 0                    | 0    | 0    | 36   |
| cSH                               | 1700   | 1700 | 1700   | 588                  | 1700 | 1700 | 345  |
| Volume to Capacity                | 0.30   | 0.30 | 0.00   | 0.02                 | 0.22 | 0.22 | 0.18 |
| Queue Length 95th (ft)            | 0      | 0    | 0      | 2                    | 0    | 0    | 16   |
| Control Delay (s)                 | 0.0    | 0.0  | 0.0    | 11.3                 | 0.0  | 0.0  | 17.7 |
| Lane LOS                          |        |      |        | B                    | C    |      |      |
| Approach Delay (s)                | 0.0    |      |        | 0.2                  |      |      | 17.7 |
| Approach LOS                      |        |      |        |                      |      |      | C    |
| Intersection Summary              |        |      |        |                      |      |      |      |
| Average Delay                     |        |      | 0.7    |                      |      |      |      |
| Intersection Capacity Utilization |        |      | 35.5%  | ICU Level of Service |      | A    |      |
| Analysis Period (min)             |        |      | 15     |                      |      |      |      |

# HCM Signalized Intersection Capacity Analysis

## 2: Route 11 & Route 114

12/8/2015



| Movement               | EBL  | EBT  | WBT  | WBR   | SBL   | SBR  |
|------------------------|------|------|------|-------|-------|------|
| Lane Configurations    |      |      |      |       |       |      |
| Traffic Volume (vph)   | 214  | 292  | 256  | 488   | 660   | 179  |
| Future Volume (vph)    | 214  | 292  | 256  | 488   | 660   | 179  |
| Ideal Flow (vphpl)     | 1900 | 1900 | 1900 | 1900  | 1900  | 1900 |
| Total Lost time (s)    | 5.5  | 6.0  | 6.0  | 4.0   | 5.5   | 4.0  |
| Lane Util. Factor      | 0.97 | 0.95 | 0.95 | 1.00  | 0.97  | 1.00 |
| Frt                    | 1.00 | 1.00 | 1.00 | 0.85  | 1.00  | 0.85 |
| Flt Protected          | 0.95 | 1.00 | 1.00 | 1.00  | 0.95  | 1.00 |
| Satd. Flow (prot)      | 3335 | 3505 | 3505 | 1583  | 3433  | 1509 |
| Flt Permitted          | 0.95 | 1.00 | 1.00 | 1.00  | 0.95  | 1.00 |
| Satd. Flow (perm)      | 3335 | 3505 | 3505 | 1583  | 3433  | 1509 |
| Peak-hour factor, PHF  | 0.81 | 0.81 | 0.95 | 0.95  | 0.86  | 0.86 |
| Growth Factor (vph)    | 105% | 105% | 105% | 105%  | 105%  | 105% |
| Adj. Flow (vph)        | 277  | 379  | 283  | 539   | 806   | 219  |
| RTOR Reduction (vph)   | 0    | 0    | 0    | 0     | 0     | 0    |
| Lane Group Flow (vph)  | 277  | 379  | 283  | 539   | 806   | 219  |
| Heavy Vehicles (%)     | 5%   | 3%   | 3%   | 2%    | 2%    | 7%   |
| Turn Type              | Prot | NA   | NA   | Free  | Prot  | Free |
| Protected Phases       | 1    | 6    | 2    |       | 4     |      |
| Permitted Phases       |      |      |      | Free  |       | Free |
| Actuated Green, G (s)  | 12.8 | 33.6 | 15.3 | 70.1  | 25.0  | 70.1 |
| Effective Green, g (s) | 12.8 | 33.6 | 15.3 | 70.1  | 25.0  | 70.1 |
| Actuated g/C Ratio     | 0.18 | 0.48 | 0.22 | 1.00  | 0.36  | 1.00 |
| Clearance Time (s)     | 5.5  | 6.0  | 6.0  |       | 5.5   |      |
| Vehicle Extension (s)  | 4.0  | 4.5  | 4.5  |       | 4.5   |      |
| Lane Grp Cap (vph)     | 608  | 1680 | 765  | 1583  | 1224  | 1509 |
| v/s Ratio Prot         | 0.08 | 0.11 | 0.08 |       | c0.23 |      |
| v/s Ratio Perm         |      |      |      | c0.34 |       | 0.15 |
| v/c Ratio              | 0.46 | 0.23 | 0.37 | 0.34  | 0.66  | 0.15 |
| Uniform Delay, d1      | 25.5 | 10.7 | 23.3 | 0.0   | 19.0  | 0.0  |
| Progression Factor     | 1.00 | 1.00 | 1.00 | 1.00  | 1.00  | 1.00 |
| Incremental Delay, d2  | 0.7  | 0.1  | 0.5  | 0.6   | 1.6   | 0.2  |
| Delay (s)              | 26.3 | 10.8 | 23.8 | 0.6   | 20.5  | 0.2  |
| Level of Service       | C    | B    | C    | A     | C     | A    |
| Approach Delay (s)     |      | 17.3 | 8.6  |       | 16.2  |      |
| Approach LOS           |      | B    | A    |       | B     |      |

### Intersection Summary

|                                   |       |                           |      |
|-----------------------------------|-------|---------------------------|------|
| HCM 2000 Control Delay            | 14.0  | HCM 2000 Level of Service | B    |
| HCM 2000 Volume to Capacity ratio | 0.56  |                           |      |
| Actuated Cycle Length (s)         | 70.1  | Sum of lost time (s)      | 17.0 |
| Intersection Capacity Utilization | 52.0% | ICU Level of Service      | A    |
| Analysis Period (min)             | 15    |                           |      |

c Critical Lane Group

# HCM Signalized Intersection Capacity Analysis

## 6: Kroger/The Rocks & Route 11

12/8/2015



| Movement               | EBL  | EBT   | EBR  | WBL   | WBT   | WBR  | NBL   | NBT  | NBR  | SBL  | SBT  | SBR  |
|------------------------|------|-------|------|-------|-------|------|-------|------|------|------|------|------|
| Lane Configurations    |      |       |      |       |       |      |       |      |      |      |      |      |
| Traffic Volume (vph)   | 32   | 821   | 129  | 84    | 765   | 1    | 101   | 5    | 109  | 36   | 10   | 74   |
| Future Volume (vph)    | 32   | 821   | 129  | 84    | 765   | 1    | 101   | 5    | 109  | 36   | 10   | 74   |
| Ideal Flow (vphpl)     | 1900 | 1900  | 1900 | 1900  | 1900  | 1900 | 1900  | 1900 | 1900 | 1900 | 1900 | 1900 |
| Total Lost time (s)    | 5.5  | 6.0   | 6.0  | 5.5   | 6.0   |      | 5.5   | 5.5  |      |      | 5.5  | 5.5  |
| Lane Util. Factor      | 1.00 | 0.95  | 1.00 | 1.00  | 0.95  |      | 1.00  | 1.00 |      |      | 1.00 | 1.00 |
| Frt                    | 1.00 | 1.00  | 0.85 | 1.00  | 1.00  |      | 1.00  | 0.86 |      |      | 1.00 | 0.85 |
| Flt Protected          | 0.95 | 1.00  | 1.00 | 0.95  | 1.00  |      | 0.95  | 1.00 |      |      | 0.96 | 1.00 |
| Satd. Flow (prot)      | 1805 | 3539  | 1583 | 1736  | 3535  |      | 1787  | 1569 |      |      | 1829 | 1599 |
| Flt Permitted          | 0.95 | 1.00  | 1.00 | 0.95  | 1.00  |      | 0.48  | 1.00 |      |      | 0.69 | 1.00 |
| Satd. Flow (perm)      | 1805 | 3539  | 1583 | 1736  | 3535  |      | 902   | 1569 |      |      | 1312 | 1599 |
| Peak-hour factor, PHF  | 0.95 | 0.95  | 0.95 | 0.90  | 0.90  | 0.90 | 0.94  | 0.94 | 0.94 | 0.71 | 0.71 | 0.71 |
| Growth Factor (vph)    | 105% | 105%  | 105% | 105%  | 105%  | 105% | 105%  | 105% | 105% | 105% | 105% | 105% |
| Adj. Flow (vph)        | 35   | 907   | 143  | 98    | 892   | 1    | 113   | 6    | 122  | 53   | 15   | 109  |
| RTOR Reduction (vph)   | 0    | 0     | 83   | 0     | 0     | 0    | 0     | 88   | 0    | 0    | 0    | 94   |
| Lane Group Flow (vph)  | 35   | 907   | 60   | 98    | 894   | 0    | 113   | 40   | 0    | 0    | 68   | 15   |
| Heavy Vehicles (%)     | 0%   | 2%    | 2%   | 4%    | 2%    | 100% | 1%    | 0%   | 4%   | 0%   | 0%   | 1%   |
| Turn Type              | Prot | NA    | Perm | Prot  | NA    |      | pm+pt | NA   |      | Perm | NA   | Perm |
| Protected Phases       | 1    | 6     |      | 5     | 2     |      | 3     | 8    |      |      | 4    |      |
| Permitted Phases       |      |       | 6    |       |       |      | 8     |      |      | 4    |      | 4    |
| Actuated Green, G (s)  | 4.5  | 35.7  | 35.7 | 8.7   | 39.9  |      | 23.3  | 23.3 |      |      | 11.3 | 11.3 |
| Effective Green, g (s) | 4.5  | 35.7  | 35.7 | 8.7   | 39.9  |      | 23.3  | 23.3 |      |      | 11.3 | 11.3 |
| Actuated g/C Ratio     | 0.05 | 0.42  | 0.42 | 0.10  | 0.47  |      | 0.28  | 0.28 |      |      | 0.13 | 0.13 |
| Clearance Time (s)     | 5.5  | 6.0   | 6.0  | 5.5   | 6.0   |      | 5.5   | 5.5  |      |      | 5.5  | 5.5  |
| Vehicle Extension (s)  | 3.5  | 4.0   | 4.0  | 3.5   | 4.0   |      | 3.0   | 4.0  |      |      | 4.0  | 4.0  |
| Lane Grp Cap (vph)     | 95   | 1491  | 667  | 178   | 1665  |      | 316   | 431  |      |      | 175  | 213  |
| v/s Ratio Prot         | 0.02 | c0.26 |      | c0.06 | c0.25 |      | c0.03 | 0.03 |      |      |      |      |
| v/s Ratio Perm         |      |       | 0.04 |       |       |      | c0.07 |      |      |      | 0.05 | 0.01 |
| v/c Ratio              | 0.37 | 0.61  | 0.09 | 0.55  | 0.54  |      | 0.36  | 0.09 |      |      | 0.39 | 0.07 |
| Uniform Delay, d1      | 38.7 | 19.1  | 14.7 | 36.1  | 15.9  |      | 24.0  | 22.8 |      |      | 33.5 | 32.1 |
| Progression Factor     | 1.00 | 1.00  | 1.00 | 1.00  | 1.00  |      | 1.00  | 1.00 |      |      | 1.00 | 1.00 |
| Incremental Delay, d2  | 2.8  | 0.8   | 0.1  | 4.0   | 0.4   |      | 0.7   | 0.1  |      |      | 1.9  | 0.2  |
| Delay (s)              | 41.6 | 19.9  | 14.8 | 40.1  | 16.3  |      | 24.6  | 23.0 |      |      | 35.5 | 32.3 |
| Level of Service       | D    | B     | B    | D     | B     |      | C     | C    |      |      | D    | C    |
| Approach Delay (s)     |      | 19.9  |      |       | 18.6  |      |       | 23.8 |      |      | 33.5 |      |
| Approach LOS           |      | B     |      |       | B     |      |       | C    |      |      | C    |      |

### Intersection Summary

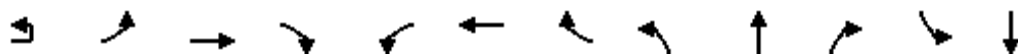
|                                   |       |                           |      |
|-----------------------------------|-------|---------------------------|------|
| HCM 2000 Control Delay            | 20.7  | HCM 2000 Level of Service | C    |
| HCM 2000 Volume to Capacity ratio | 0.56  |                           |      |
| Actuated Cycle Length (s)         | 84.7  | Sum of lost time (s)      | 22.5 |
| Intersection Capacity Utilization | 55.5% | ICU Level of Service      | B    |
| Analysis Period (min)             | 15    |                           |      |

c Critical Lane Group

# HCM Unsignalized Intersection Capacity Analysis

## 3: Cherry Ct/Driveway & Route 11

12/8/2015



| Movement               | EBU  | EBL  | EBT  | EBR  | WBL  | WBT   | WBR  | NBL  | NBT  | NBR  | SBL  | SBT  |
|------------------------|------|------|------|------|------|-------|------|------|------|------|------|------|
| Lane Configurations    |      | ↖    | ↕    |      | ↖    | ↕     |      |      | ↕    |      |      | ↕    |
| Traffic Volume (veh/h) | 1    | 5    | 943  | 3    | 1    | 742   | 1    | 3    | 0    | 1    | 2    | 0    |
| Future Volume (Veh/h)  | 1    | 5    | 943  | 3    | 1    | 742   | 1    | 3    | 0    | 1    | 2    | 0    |
| Sign Control           |      |      | Free |      |      | Free  |      |      | Stop |      |      | Stop |
| Grade                  |      |      | 0%   |      |      | 0%    |      |      | 0%   |      |      | 0%   |
| Peak Hour Factor       | 0.93 | 0.93 | 0.93 | 0.93 | 0.93 | 0.93  | 0.93 | 0.50 | 0.50 | 0.50 | 0.59 | 0.59 |
| Hourly flow rate (vph) | 0    | 6    | 1065 | 3    | 1    | 838   | 1    | 6    | 0    | 2    | 4    | 0    |
| Pedestrians            |      |      |      |      |      |       |      |      |      |      |      |      |
| Lane Width (ft)        |      |      |      |      |      |       |      |      |      |      |      |      |
| Walking Speed (ft/s)   |      |      |      |      |      |       |      |      |      |      |      |      |
| Percent Blockage       |      |      |      |      |      |       |      |      |      |      |      |      |
| Right turn flare (veh) |      |      |      |      |      |       |      |      |      |      |      |      |
| Median type            |      |      | None |      |      | TWLTL |      |      |      |      |      |      |
| Median storage (veh)   |      |      |      |      |      | 2     |      |      |      |      |      |      |
| Upstream signal (ft)   |      |      | 779  |      |      |       |      |      |      |      |      |      |
| pX, platoon unblocked  | 0.00 |      |      |      | 0.99 |       |      | 0.99 | 0.99 | 0.99 | 0.99 | 0.99 |
| vC, conflicting volume | 0    | 839  |      |      | 1068 |       |      | 1530 | 1920 | 534  | 1387 | 1920 |
| vC1, stage 1 conf vol  |      |      |      |      |      |       |      | 1078 | 1078 |      | 840  | 840  |
| vC2, stage 2 conf vol  |      |      |      |      |      |       |      | 451  | 841  |      | 546  | 1080 |
| vCu, unblocked vol     | 0    | 839  |      |      | 1046 |       |      | 1513 | 1907 | 506  | 1369 | 1908 |
| tC, single (s)         | 0.0  | 4.1  |      |      | 4.1  |       |      | 7.5  | 6.5  | 6.9  | 7.5  | 6.5  |
| tC, 2 stage (s)        |      |      |      |      |      |       |      | 6.5  | 5.5  |      | 6.5  | 5.5  |
| tF (s)                 | 0.0  | 2.2  |      |      | 2.2  |       |      | 3.5  | 4.0  | 3.3  | 3.5  | 4.0  |
| p0 queue free %        | 0    | 99   |      |      | 100  |       |      | 97   | 100  | 100  | 99   | 100  |
| cM capacity (veh/h)    | 0    | 804  |      |      | 665  |       |      | 220  | 237  | 511  | 284  | 238  |

| Direction, Lane #      | EB 1 | EB 2 | EB 3 | WB 1 | WB 2 | WB 3 | NB 1 | SB 1 |
|------------------------|------|------|------|------|------|------|------|------|
| Volume Total           | 6    | 710  | 358  | 1    | 559  | 280  | 8    | 34   |
| Volume Left            | 6    | 0    | 0    | 1    | 0    | 0    | 6    | 4    |
| Volume Right           | 0    | 0    | 3    | 0    | 0    | 1    | 2    | 30   |
| cSH                    | 804  | 1700 | 1700 | 665  | 1700 | 1700 | 257  | 511  |
| Volume to Capacity     | 0.01 | 0.42 | 0.21 | 0.00 | 0.33 | 0.16 | 0.03 | 0.07 |
| Queue Length 95th (ft) | 1    | 0    | 0    | 0    | 0    | 0    | 2    | 5    |
| Control Delay (s)      | 9.5  | 0.0  | 0.0  | 10.4 | 0.0  | 0.0  | 19.5 | 12.5 |
| Lane LOS               | A    |      |      | B    |      |      | C    | B    |
| Approach Delay (s)     | 0.1  |      |      | 0.0  |      |      | 19.5 | 12.5 |
| Approach LOS           |      |      |      |      |      |      | C    | B    |

### Intersection Summary

|                                   |       |
|-----------------------------------|-------|
| Average Delay                     | 0.3   |
| Intersection Capacity Utilization | 37.5% |
| ICU Level of Service              | A     |
| Analysis Period (min)             | 15    |

# HCM Unsignalized Intersection Capacity Analysis

## 3: Cherry Ct/Driveway & Route 11

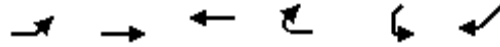
12/8/2015

| Movement               | SBR  |
|------------------------|------|
| Lane Configurations    |      |
| Traffic Volume (veh/h) | 17   |
| Future Volume (Veh/h)  | 17   |
| Sign Control           |      |
| Grade                  |      |
| Peak Hour Factor       | 0.59 |
| Hourly flow rate (vph) | 30   |
| Pedestrians            |      |
| Lane Width (ft)        |      |
| Walking Speed (ft/s)   |      |
| Percent Blockage       |      |
| Right turn flare (veh) |      |
| Median type            |      |
| Median storage (veh)   |      |
| Upstream signal (ft)   |      |
| pX, platoon unblocked  |      |
| vC, conflicting volume | 420  |
| vC1, stage 1 conf vol  |      |
| vC2, stage 2 conf vol  |      |
| vCu, unblocked vol     | 420  |
| tC, single (s)         | 7.0  |
| tC, 2 stage (s)        |      |
| tF (s)                 | 3.4  |
| p0 queue free %        | 95   |
| cM capacity (veh/h)    | 571  |
| Direction, Lane #      |      |

# HCM Unsignalized Intersection Capacity Analysis

## 4: Route 11 & Belspring Rd

12/8/2015



| Movement                          | EBL  | EBT   | WBT   | WBR  | SWL                  | SWR  |
|-----------------------------------|------|-------|-------|------|----------------------|------|
| Lane Configurations               | ↶    | ↷     | ↷     |      | ↶                    |      |
| Traffic Volume (veh/h)            | 7    | 939   | 743   | 178  | 17                   | 1    |
| Future Volume (Veh/h)             | 7    | 939   | 743   | 178  | 17                   | 1    |
| Sign Control                      |      | Free  | Free  |      | Stop                 |      |
| Grade                             |      | 0%    | 0%    |      | 0%                   |      |
| Peak Hour Factor                  | 0.93 | 0.93  | 0.93  | 0.93 | 0.65                 | 0.65 |
| Hourly flow rate (vph)            | 8    | 1060  | 839   | 201  | 27                   | 2    |
| Pedestrians                       |      |       |       |      |                      |      |
| Lane Width (ft)                   |      |       |       |      |                      |      |
| Walking Speed (ft/s)              |      |       |       |      |                      |      |
| Percent Blockage                  |      |       |       |      |                      |      |
| Right turn flare (veh)            |      |       |       |      |                      |      |
| Median type                       |      |       |       |      |                      |      |
|                                   |      | TWLTL | TWLTL |      |                      |      |
| Median storage (veh)              |      | 2     | 2     |      |                      |      |
| Upstream signal (ft)              |      | 1075  | 1303  |      |                      |      |
| pX, platoon unblocked             | 0.85 |       |       |      | 0.85                 | 0.85 |
| vC, conflicting volume            | 1040 |       |       |      | 1486                 | 520  |
| vC1, stage 1 conf vol             |      |       |       |      | 940                  |      |
| vC2, stage 2 conf vol             |      |       |       |      | 546                  |      |
| vCu, unblocked vol                | 707  |       |       |      | 1228                 | 99   |
| tC, single (s)                    | 4.1  |       |       |      | 6.9                  | 6.9  |
| tC, 2 stage (s)                   |      |       |       |      | 5.9                  |      |
| tF (s)                            | 2.2  |       |       |      | 3.6                  | 3.3  |
| p0 queue free %                   | 99   |       |       |      | 92                   | 100  |
| cM capacity (veh/h)               | 770  |       |       |      | 352                  | 807  |
| Direction, Lane #                 | EB 1 | EB 2  | EB 3  | WB 1 | WB 2                 | SW 1 |
| Volume Total                      | 8    | 530   | 530   | 559  | 481                  | 29   |
| Volume Left                       | 8    | 0     | 0     | 0    | 0                    | 27   |
| Volume Right                      | 0    | 0     | 0     | 0    | 201                  | 2    |
| cSH                               | 770  | 1700  | 1700  | 1700 | 1700                 | 366  |
| Volume to Capacity                | 0.01 | 0.31  | 0.31  | 0.33 | 0.28                 | 0.08 |
| Queue Length 95th (ft)            | 1    | 0     | 0     | 0    | 0                    | 6    |
| Control Delay (s)                 | 9.7  | 0.0   | 0.0   | 0.0  | 0.0                  | 15.7 |
| Lane LOS                          | A    |       |       |      |                      | C    |
| Approach Delay (s)                | 0.1  |       |       | 0.0  |                      | 15.7 |
| Approach LOS                      |      |       |       |      |                      | C    |
| Intersection Summary              |      |       |       |      |                      |      |
| Average Delay                     |      |       | 0.2   |      |                      |      |
| Intersection Capacity Utilization |      |       | 37.5% |      | ICU Level of Service | A    |
| Analysis Period (min)             |      |       | 15    |      |                      |      |

# HCM Unsignalized Intersection Capacity Analysis

## 5: Driveway/Nicewander Way & Route 11

12/8/2015



| Movement               | EBL  | EBT   | EBR  | WBL  | WBT   | WBR  | NBL  | NBT  | NBR  | SBL  | SBT  | SBR  |
|------------------------|------|-------|------|------|-------|------|------|------|------|------|------|------|
| Lane Configurations    |      |       |      |      |       |      |      |      |      |      |      |      |
| Traffic Volume (veh/h) | 4    | 955   | 32   | 27   | 919   | 2    | 31   | 0    | 28   | 2    | 0    | 2    |
| Future Volume (Veh/h)  | 4    | 955   | 32   | 27   | 919   | 2    | 31   | 0    | 28   | 2    | 0    | 2    |
| Sign Control           |      | Free  |      |      | Free  |      |      | Stop |      |      | Stop |      |
| Grade                  |      | 0%    |      |      | 0%    |      |      | 0%   |      |      | 0%   |      |
| Peak Hour Factor       | 0.93 | 0.93  | 0.93 | 0.92 | 0.92  | 0.92 | 0.74 | 0.74 | 0.74 | 0.50 | 0.50 | 0.50 |
| Hourly flow rate (vph) | 5    | 1078  | 36   | 31   | 1049  | 2    | 44   | 0    | 40   | 4    | 0    | 4    |
| Pedestrians            |      |       |      |      |       |      |      |      |      |      |      |      |
| Lane Width (ft)        |      |       |      |      |       |      |      |      |      |      |      |      |
| Walking Speed (ft/s)   |      |       |      |      |       |      |      |      |      |      |      |      |
| Percent Blockage       |      |       |      |      |       |      |      |      |      |      |      |      |
| Right turn flare (veh) |      |       |      |      |       |      |      |      |      |      |      |      |
| Median type            |      | TWLTL |      |      | TWLTL |      |      |      |      |      |      |      |
| Median storage veh     |      | 2     |      |      | 2     |      |      |      |      |      |      |      |
| Upstream signal (ft)   |      |       |      |      | 715   |      |      |      |      |      |      |      |
| pX, platoon unblocked  | 0.83 |       |      |      |       |      | 0.83 | 0.83 |      | 0.83 | 0.83 | 0.83 |
| vC, conflicting volume | 1051 |       |      | 1114 |       |      | 1696 | 2219 | 557  | 1701 | 2236 | 526  |
| vC1, stage 1 conf vol  |      |       |      |      |       |      | 1106 | 1106 |      | 1112 | 1112 |      |
| vC2, stage 2 conf vol  |      |       |      |      |       |      | 590  | 1113 |      | 589  | 1124 |      |
| vCu, unblocked vol     | 647  |       |      | 1114 |       |      | 1426 | 2057 | 557  | 1432 | 2078 | 13   |
| tC, single (s)         | 4.1  |       |      | 4.2  |       |      | 7.6  | 6.5  | 7.0  | 7.5  | 6.5  | 7.9  |
| tC, 2 stage (s)        |      |       |      |      |       |      | 6.6  | 5.5  |      | 6.5  | 5.5  |      |
| tF (s)                 | 2.2  |       |      | 2.2  |       |      | 3.5  | 4.0  | 3.3  | 3.5  | 4.0  | 3.8  |
| p0 queue free %        | 99   |       |      | 95   |       |      | 79   | 100  | 92   | 98   | 100  | 99   |
| cM capacity (veh/h)    | 785  |       |      | 611  |       |      | 208  | 215  | 471  | 245  | 199  | 768  |

| Direction, Lane #      | EB 1 | EB 2 | EB 3 | WB 1 | WB 2 | WB 3 | NB 1 | SB 1 |
|------------------------|------|------|------|------|------|------|------|------|
| Volume Total           | 5    | 719  | 395  | 31   | 699  | 352  | 84   | 8    |
| Volume Left            | 5    | 0    | 0    | 31   | 0    | 0    | 44   | 4    |
| Volume Right           | 0    | 0    | 36   | 0    | 0    | 2    | 40   | 4    |
| cSH                    | 785  | 1700 | 1700 | 611  | 1700 | 1700 | 284  | 371  |
| Volume to Capacity     | 0.01 | 0.42 | 0.23 | 0.05 | 0.41 | 0.21 | 0.30 | 0.02 |
| Queue Length 95th (ft) | 0    | 0    | 0    | 4    | 0    | 0    | 30   | 2    |
| Control Delay (s)      | 9.6  | 0.0  | 0.0  | 11.2 | 0.0  | 0.0  | 22.9 | 14.9 |
| Lane LOS               | A    |      |      | B    |      |      | C    | B    |
| Approach Delay (s)     | 0.0  |      |      | 0.3  |      |      | 22.9 | 14.9 |
| Approach LOS           |      |      |      |      |      |      | C    | B    |

### Intersection Summary

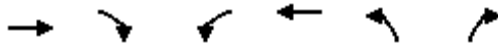
|                                   |       |
|-----------------------------------|-------|
| Average Delay                     | 1.1   |
| Intersection Capacity Utilization | 40.0% |
| ICU Level of Service              | A     |
| Analysis Period (min)             | 15    |



# HCM Unsignalized Intersection Capacity Analysis

## 8: Wheatland & Route 11

12/8/2015



| Movement                          | EBT   | EBR  | WBL   | WBT                  | NBL  | NBR  |
|-----------------------------------|-------|------|-------|----------------------|------|------|
| Lane Configurations               | ↑↑    |      | ↵     | ↑↑                   | ↵    |      |
| Traffic Volume (veh/h)            | 946   | 10   | 47    | 893                  | 7    | 63   |
| Future Volume (Veh/h)             | 946   | 10   | 47    | 893                  | 7    | 63   |
| Sign Control                      | Free  |      |       | Free                 | Stop |      |
| Grade                             | 0%    |      |       | 0%                   | 0%   |      |
| Peak Hour Factor                  | 0.95  | 0.95 | 0.90  | 0.90                 | 0.68 | 0.68 |
| Hourly flow rate (vph)            | 1046  | 11   | 55    | 1042                 | 11   | 97   |
| Pedestrians                       |       |      |       |                      |      |      |
| Lane Width (ft)                   |       |      |       |                      |      |      |
| Walking Speed (ft/s)              |       |      |       |                      |      |      |
| Percent Blockage                  |       |      |       |                      |      |      |
| Right turn flare (veh)            |       |      |       |                      |      |      |
| Median type                       | TWLTL |      | TWLTL |                      |      |      |
| Median storage veh)               | 2     |      | 2     |                      |      |      |
| Upstream signal (ft)              | 335   |      |       |                      |      |      |
| pX, platoon unblocked             |       |      | 0.80  |                      | 0.80 | 0.80 |
| vC, conflicting volume            |       |      | 1057  |                      | 1682 | 528  |
| vC1, stage 1 conf vol             |       |      |       |                      | 1052 |      |
| vC2, stage 2 conf vol             |       |      |       |                      | 631  |      |
| vCu, unblocked vol                |       |      | 575   |                      | 1356 | 0    |
| tC, single (s)                    |       |      | 4.1   |                      | 7.1  | 6.9  |
| tC, 2 stage (s)                   |       |      |       |                      | 6.1  |      |
| tF (s)                            |       |      | 2.2   |                      | 3.6  | 3.3  |
| p0 queue free %                   |       |      | 93    |                      | 96   | 89   |
| cM capacity (veh/h)               |       |      | 797   |                      | 300  | 874  |
| Direction, Lane #                 | EB 1  | EB 2 | WB 1  | WB 2                 | WB 3 | NB 1 |
| Volume Total                      | 697   | 360  | 55    | 521                  | 521  | 108  |
| Volume Left                       | 0     | 0    | 55    | 0                    | 0    | 11   |
| Volume Right                      | 0     | 11   | 0     | 0                    | 0    | 97   |
| cSH                               | 1700  | 1700 | 797   | 1700                 | 1700 | 731  |
| Volume to Capacity                | 0.41  | 0.21 | 0.07  | 0.31                 | 0.31 | 0.15 |
| Queue Length 95th (ft)            | 0     | 0    | 6     | 0                    | 0    | 13   |
| Control Delay (s)                 | 0.0   | 0.0  | 9.9   | 0.0                  | 0.0  | 10.8 |
| Lane LOS                          | A     |      |       | B                    |      |      |
| Approach Delay (s)                | 0.0   |      | 0.5   |                      |      | 10.8 |
| Approach LOS                      |       |      |       | B                    |      |      |
| <b>Intersection Summary</b>       |       |      |       |                      |      |      |
| Average Delay                     |       |      | 0.8   |                      |      |      |
| Intersection Capacity Utilization |       |      | 45.6% | ICU Level of Service |      | A    |
| Analysis Period (min)             |       |      | 15    |                      |      |      |

# HCM Unsignalized Intersection Capacity Analysis

## 9: University Park/Roca's & Route 11

12/8/2015



| Movement               | EBL  | EBT   | EBR  | WBL  | WBT  | WBR  | NBL  | NBT  | NBR  | SBL  | SBT  | SBR  |
|------------------------|------|-------|------|------|------|------|------|------|------|------|------|------|
| Lane Configurations    | ↗    | ↕     |      | ↖    | ↕    |      |      | ↕    |      |      | ↕    | ↘    |
| Traffic Volume (veh/h) | 4    | 973   | 6    | 5    | 961  | 0    | 9    | 0    | 8    | 0    | 0    | 2    |
| Future Volume (Veh/h)  | 4    | 973   | 6    | 5    | 961  | 0    | 9    | 0    | 8    | 0    | 0    | 2    |
| Sign Control           |      | Free  |      |      | Free |      |      | Stop |      |      | Stop |      |
| Grade                  |      | 0%    |      |      | 0%   |      |      | 0%   |      |      | 0%   |      |
| Peak Hour Factor       | 0.96 | 0.96  | 0.96 | 0.84 | 0.84 | 0.84 | 0.63 | 0.63 | 0.63 | 0.50 | 0.50 | 0.50 |
| Hourly flow rate (vph) | 4    | 1064  | 7    | 6    | 1201 | 0    | 15   | 0    | 13   | 0    | 0    | 4    |
| Pedestrians            |      |       |      |      |      |      |      |      |      |      |      |      |
| Lane Width (ft)        |      |       |      |      |      |      |      |      |      |      |      |      |
| Walking Speed (ft/s)   |      |       |      |      |      |      |      |      |      |      |      |      |
| Percent Blockage       |      |       |      |      |      |      |      |      |      |      |      |      |
| Right turn flare (veh) |      |       |      |      |      |      |      |      |      |      |      |      |
| Median type            |      | TWLTL |      |      | None |      |      |      |      |      |      |      |
| Median storage (veh)   |      | 2     |      |      |      |      |      |      |      |      |      |      |
| Upstream signal (ft)   |      | 792   |      |      |      |      |      |      |      |      |      |      |
| pX, platoon unblocked  |      |       |      | 0.81 |      |      | 0.81 | 0.81 | 0.81 | 0.81 | 0.81 |      |
| vC, conflicting volume | 1201 |       |      | 1071 |      |      | 1692 | 2288 | 536  | 1766 | 2292 | 600  |
| vC1, stage 1 conf vol  |      |       |      |      |      |      | 1076 | 1076 |      | 1213 | 1213 |      |
| vC2, stage 2 conf vol  |      |       |      |      |      |      | 616  | 1213 |      | 553  | 1079 |      |
| vCu, unblocked vol     | 1201 |       |      | 631  |      |      | 1394 | 2126 | 0    | 1485 | 2131 | 600  |
| tC, single (s)         | 4.1  |       |      | 4.9  |      |      | 7.5  | 6.5  | 7.2  | 7.5  | 6.5  | 6.9  |
| tC, 2 stage (s)        |      |       |      |      |      |      | 6.5  | 5.5  |      | 6.5  | 5.5  |      |
| tF (s)                 | 2.2  |       |      | 2.6  |      |      | 3.5  | 4.0  | 3.4  | 3.5  | 4.0  | 3.3  |
| p0 queue free %        | 99   |       |      | 99   |      |      | 95   | 100  | 98   | 100  | 100  | 99   |
| cM capacity (veh/h)    | 588  |       |      | 595  |      |      | 279  | 206  | 855  | 187  | 207  | 449  |

| Direction, Lane #      | EB 1 | EB 2 | EB 3 | WB 1 | WB 2 | WB 3 | NB 1 | SB 1 |
|------------------------|------|------|------|------|------|------|------|------|
| Volume Total           | 4    | 709  | 362  | 6    | 801  | 400  | 28   | 4    |
| Volume Left            | 4    | 0    | 0    | 6    | 0    | 0    | 15   | 0    |
| Volume Right           | 0    | 0    | 7    | 0    | 0    | 0    | 13   | 4    |
| cSH                    | 588  | 1700 | 1700 | 595  | 1700 | 1700 | 406  | 449  |
| Volume to Capacity     | 0.01 | 0.42 | 0.21 | 0.01 | 0.47 | 0.24 | 0.07 | 0.01 |
| Queue Length 95th (ft) | 1    | 0    | 0    | 1    | 0    | 0    | 6    | 1    |
| Control Delay (s)      | 11.2 | 0.0  | 0.0  | 11.1 | 0.0  | 0.0  | 14.5 | 13.1 |
| Lane LOS               | B    |      |      | B    |      |      | B    | B    |
| Approach Delay (s)     | 0.0  |      |      | 0.1  |      |      | 14.5 | 13.1 |
| Approach LOS           |      |      |      |      |      |      | B    | B    |

### Intersection Summary

|                                   |       |
|-----------------------------------|-------|
| Average Delay                     | 0.2   |
| Intersection Capacity Utilization | 42.8% |
| ICU Level of Service              | A     |
| Analysis Period (min)             | 15    |

# HCM Unsignalized Intersection Capacity Analysis

## 10: Route 11 & Nesslerod

12/8/2015

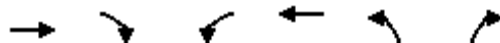


| Movement                          | EBL  | EBT    | WBT    | WBR  | SBL                  | SBR  |
|-----------------------------------|------|--------|--------|------|----------------------|------|
| Lane Configurations               |      |        |        |      |                      |      |
| Traffic Volume (veh/h)            | 3    | 978    | 985    | 1    | 0                    | 1    |
| Future Volume (Veh/h)             | 3    | 978    | 985    | 1    | 0                    | 1    |
| Sign Control                      |      | Free   | Free   |      | Stop                 |      |
| Grade                             |      | 0%     | 0%     |      | 0%                   |      |
| Peak Hour Factor                  | 0.96 | 0.96   | 0.84   | 0.84 | 0.25                 | 0.25 |
| Hourly flow rate (vph)            | 3    | 1070   | 1231   | 1    | 0                    | 4    |
| Pedestrians                       |      |        |        |      |                      |      |
| Lane Width (ft)                   |      |        |        |      |                      |      |
| Walking Speed (ft/s)              |      |        |        |      |                      |      |
| Percent Blockage                  |      |        |        |      |                      |      |
| Right turn flare (veh)            |      |        |        |      |                      |      |
| Median type                       |      | Raised | Raised |      |                      |      |
| Median storage (veh)              |      | 1      | 1      |      |                      |      |
| Upstream signal (ft)              |      | 1287   |        |      |                      |      |
| pX, platoon unblocked             |      |        |        |      | 0.83                 |      |
| vC, conflicting volume            | 1232 |        |        |      | 1772                 | 616  |
| vC1, stage 1 conf vol             |      |        |        |      | 1232                 |      |
| vC2, stage 2 conf vol             |      |        |        |      | 541                  |      |
| vCu, unblocked vol                | 1232 |        |        |      | 1522                 | 616  |
| tC, single (s)                    | 4.1  |        |        |      | 6.8                  | 6.9  |
| tC, 2 stage (s)                   |      |        |        |      | 5.8                  |      |
| tF (s)                            | 2.2  |        |        |      | 3.5                  | 3.3  |
| p0 queue free %                   | 99   |        |        |      | 100                  | 99   |
| cM capacity (veh/h)               | 573  |        |        |      | 198                  | 438  |
| Direction, Lane #                 | EB 1 | EB 2   | EB 3   | WB 1 | WB 2                 | SB 1 |
| Volume Total                      | 3    | 535    | 535    | 821  | 411                  | 4    |
| Volume Left                       | 3    | 0      | 0      | 0    | 0                    | 0    |
| Volume Right                      | 0    | 0      | 0      | 0    | 1                    | 4    |
| cSH                               | 573  | 1700   | 1700   | 1700 | 1700                 | 438  |
| Volume to Capacity                | 0.01 | 0.31   | 0.31   | 0.48 | 0.24                 | 0.01 |
| Queue Length 95th (ft)            | 0    | 0      | 0      | 0    | 0                    | 1    |
| Control Delay (s)                 | 11.3 | 0.0    | 0.0    | 0.0  | 0.0                  | 13.3 |
| Lane LOS                          | B    |        |        |      |                      | B    |
| Approach Delay (s)                | 0.0  |        |        | 0.0  |                      | 13.3 |
| Approach LOS                      |      |        |        |      |                      | B    |
| Intersection Summary              |      |        |        |      |                      |      |
| Average Delay                     |      |        | 0.0    |      |                      |      |
| Intersection Capacity Utilization |      |        | 38.6%  |      | ICU Level of Service | A    |
| Analysis Period (min)             |      |        | 15     |      |                      |      |

# HCM Unsignalized Intersection Capacity Analysis

## 11: Hazel Hollow Rd & Route 11

12/8/2015



| Movement                          | EBT    | EBR  | WBL    | WBT                  | NBL  | NBR  |      |
|-----------------------------------|--------|------|--------|----------------------|------|------|------|
| Lane Configurations               | ↑↑     | ↑    | ↓      | ↑↑                   | ↓    | ↓    |      |
| Traffic Volume (veh/h)            | 958    | 20   | 21     | 970                  | 16   | 27   |      |
| Future Volume (Veh/h)             | 958    | 20   | 21     | 970                  | 16   | 27   |      |
| Sign Control                      | Free   |      |        | Free                 | Stop |      |      |
| Grade                             | 0%     |      |        | 0%                   | 0%   |      |      |
| Peak Hour Factor                  | 0.96   | 0.96 | 0.84   | 0.84                 | 0.63 | 0.63 |      |
| Hourly flow rate (vph)            | 1048   | 22   | 26     | 1213                 | 27   | 45   |      |
| Pedestrians                       |        |      |        |                      |      |      |      |
| Lane Width (ft)                   |        |      |        |                      |      |      |      |
| Walking Speed (ft/s)              |        |      |        |                      |      |      |      |
| Percent Blockage                  |        |      |        |                      |      |      |      |
| Right turn flare (veh)            |        |      |        |                      |      |      |      |
| Median type                       | Raised |      | Raised |                      |      |      |      |
| Median storage (veh)              | 1      |      | 1      |                      |      |      |      |
| Upstream signal (ft)              |        |      |        |                      |      |      |      |
| pX, platoon unblocked             |        |      |        |                      |      |      |      |
| vC, conflicting volume            |        |      | 1070   |                      | 1706 | 524  |      |
| vC1, stage 1 conf vol             |        |      |        |                      | 1048 |      |      |
| vC2, stage 2 conf vol             |        |      |        |                      | 658  |      |      |
| vCu, unblocked vol                |        |      | 1070   |                      | 1706 | 524  |      |
| tC, single (s)                    |        |      | 4.3    |                      | 6.9  | 6.9  |      |
| tC, 2 stage (s)                   |        |      |        |                      | 5.9  |      |      |
| tF (s)                            |        |      | 2.3    |                      | 3.6  | 3.3  |      |
| p0 queue free %                   |        |      | 96     |                      | 86   | 91   |      |
| cM capacity (veh/h)               |        |      | 602    |                      | 193  | 503  |      |
| Direction, Lane #                 | EB 1   | EB 2 | EB 3   | WB 1                 | WB 2 | WB 3 | NB 1 |
| Volume Total                      | 524    | 524  | 22     | 26                   | 606  | 606  | 72   |
| Volume Left                       | 0      | 0    | 0      | 26                   | 0    | 0    | 27   |
| Volume Right                      | 0      | 0    | 22     | 0                    | 0    | 0    | 45   |
| cSH                               | 1700   | 1700 | 1700   | 602                  | 1700 | 1700 | 314  |
| Volume to Capacity                | 0.31   | 0.31 | 0.01   | 0.04                 | 0.36 | 0.36 | 0.23 |
| Queue Length 95th (ft)            | 0      | 0    | 0      | 3                    | 0    | 0    | 22   |
| Control Delay (s)                 | 0.0    | 0.0  | 0.0    | 11.3                 | 0.0  | 0.0  | 19.9 |
| Lane LOS                          |        |      |        | B                    | C    |      |      |
| Approach Delay (s)                | 0.0    |      |        | 0.2                  |      |      | 19.9 |
| Approach LOS                      |        |      |        |                      |      |      | C    |
| Intersection Summary              |        |      |        |                      |      |      |      |
| Average Delay                     |        |      | 0.7    |                      |      |      |      |
| Intersection Capacity Utilization |        |      | 38.2%  | ICU Level of Service |      | A    |      |
| Analysis Period (min)             |        |      | 15     |                      |      |      |      |

# HCM Signalized Intersection Capacity Analysis

## 2: Route 11 & Route 114

12/8/2015



| Movement               | EBL   | EBT   | WBT   | WBR  | SBL   | SBR  |
|------------------------|-------|-------|-------|------|-------|------|
| Lane Configurations    |       |       |       |      |       |      |
| Traffic Volume (vph)   | 335   | 284   | 463   | 548  | 824   | 410  |
| Future Volume (vph)    | 335   | 284   | 463   | 548  | 824   | 410  |
| Ideal Flow (vphpl)     | 1900  | 1900  | 1900  | 1900 | 1900  | 1900 |
| Total Lost time (s)    | 5.5   | 6.0   | 6.0   | 4.0  | 5.5   | 4.0  |
| Lane Util. Factor      | 0.97  | *0.85 | *0.90 | 1.00 | *0.85 | 1.00 |
| Frt                    | 1.00  | 1.00  | 1.00  | 0.85 | 1.00  | 0.85 |
| Flt Protected          | 0.95  | 1.00  | 1.00  | 1.00 | 0.95  | 1.00 |
| Satd. Flow (prot)      | 3467  | 3167  | 3386  | 1599 | 3038  | 1583 |
| Flt Permitted          | 0.95  | 1.00  | 1.00  | 1.00 | 0.95  | 1.00 |
| Satd. Flow (perm)      | 3467  | 3167  | 3386  | 1599 | 3038  | 1583 |
| Peak-hour factor, PHF  | 0.96  | 0.96  | 0.90  | 0.90 | 0.90  | 0.90 |
| Growth Factor (vph)    | 105%  | 105%  | 105%  | 105% | 105%  | 105% |
| Adj. Flow (vph)        | 366   | 311   | 540   | 639  | 961   | 478  |
| RTOR Reduction (vph)   | 0     | 0     | 0     | 0    | 0     | 0    |
| Lane Group Flow (vph)  | 366   | 311   | 540   | 639  | 961   | 478  |
| Heavy Vehicles (%)     | 1%    | 2%    | 1%    | 1%   | 1%    | 2%   |
| Turn Type              | Prot  | NA    | NA    | Free | Prot  | Free |
| Protected Phases       | 1     | 6     | 2     |      | 4     |      |
| Permitted Phases       |       |       |       | Free |       | Free |
| Actuated Green, G (s)  | 16.9  | 46.4  | 24.0  | 95.6 | 37.7  | 95.6 |
| Effective Green, g (s) | 16.9  | 46.4  | 24.0  | 95.6 | 37.7  | 95.6 |
| Actuated g/C Ratio     | 0.18  | 0.49  | 0.25  | 1.00 | 0.39  | 1.00 |
| Clearance Time (s)     | 5.5   | 6.0   | 6.0   |      | 5.5   |      |
| Vehicle Extension (s)  | 4.0   | 4.5   | 4.5   |      | 4.5   |      |
| Lane Grp Cap (vph)     | 612   | 1537  | 850   | 1599 | 1198  | 1583 |
| v/s Ratio Prot         | c0.11 | 0.10  | c0.16 |      | c0.32 |      |
| v/s Ratio Perm         |       |       |       | 0.40 |       | 0.30 |
| v/c Ratio              | 0.60  | 0.20  | 0.64  | 0.40 | 0.80  | 0.30 |
| Uniform Delay, d1      | 36.2  | 14.0  | 31.9  | 0.0  | 25.6  | 0.0  |
| Progression Factor     | 1.00  | 1.00  | 1.00  | 1.00 | 1.00  | 1.00 |
| Incremental Delay, d2  | 1.8   | 0.1   | 2.0   | 0.7  | 4.4   | 0.5  |
| Delay (s)              | 38.1  | 14.2  | 33.9  | 0.7  | 30.1  | 0.5  |
| Level of Service       | D     | B     | C     | A    | C     | A    |
| Approach Delay (s)     |       | 27.1  | 15.9  |      | 20.2  |      |
| Approach LOS           |       | C     | B     |      | C     |      |

### Intersection Summary

|                                   |       |                           |      |
|-----------------------------------|-------|---------------------------|------|
| HCM 2000 Control Delay            | 20.1  | HCM 2000 Level of Service | C    |
| HCM 2000 Volume to Capacity ratio | 0.71  |                           |      |
| Actuated Cycle Length (s)         | 95.6  | Sum of lost time (s)      | 17.0 |
| Intersection Capacity Utilization | 62.3% | ICU Level of Service      | B    |
| Analysis Period (min)             | 15    |                           |      |

c Critical Lane Group

# HCM Signalized Intersection Capacity Analysis

## 6: Kroger/The Rocks & Route 11

12/8/2015



| Movement               | EBL  | EBT   | EBR  | WBL   | WBT   | WBR  | NBL   | NBT  | NBR  | SBL  | SBT  | SBR  |
|------------------------|------|-------|------|-------|-------|------|-------|------|------|------|------|------|
| Lane Configurations    |      |       |      |       |       |      |       |      |      |      |      |      |
| Traffic Volume (vph)   | 29   | 922   | 132  | 127   | 1187  | 6    | 152   | 6    | 159  | 36   | 7    | 65   |
| Future Volume (vph)    | 29   | 922   | 132  | 127   | 1187  | 6    | 152   | 6    | 159  | 36   | 7    | 65   |
| Ideal Flow (vphpl)     | 1900 | 1900  | 1900 | 1900  | 1900  | 1900 | 1900  | 1900 | 1900 | 1900 | 1900 | 1900 |
| Total Lost time (s)    | 5.5  | 6.0   | 6.0  | 5.5   | 6.0   |      | 5.5   | 5.5  |      |      | 5.5  | 5.5  |
| Lane Util. Factor      | 1.00 | *0.85 | 1.00 | 1.00  | *0.75 |      | 1.00  | 1.00 |      |      | 1.00 | 1.00 |
| Frt                    | 1.00 | 1.00  | 0.85 | 1.00  | 1.00  |      | 1.00  | 0.86 |      |      | 1.00 | 0.85 |
| Flt Protected          | 0.95 | 1.00  | 1.00 | 0.95  | 1.00  |      | 0.95  | 1.00 |      |      | 0.96 | 1.00 |
| Satd. Flow (prot)      | 1805 | 3198  | 1583 | 1770  | 2820  |      | 1770  | 1611 |      |      | 1779 | 1615 |
| Flt Permitted          | 0.95 | 1.00  | 1.00 | 0.95  | 1.00  |      | 0.45  | 1.00 |      |      | 0.63 | 1.00 |
| Satd. Flow (perm)      | 1805 | 3198  | 1583 | 1770  | 2820  |      | 831   | 1611 |      |      | 1162 | 1615 |
| Peak-hour factor, PHF  | 0.93 | 0.93  | 0.93 | 0.89  | 0.89  | 0.89 | 0.84  | 0.84 | 0.84 | 0.93 | 0.93 | 0.93 |
| Growth Factor (vph)    | 105% | 105%  | 105% | 105%  | 105%  | 105% | 105%  | 105% | 105% | 105% | 105% | 105% |
| Adj. Flow (vph)        | 33   | 1041  | 149  | 150   | 1400  | 7    | 190   | 8    | 199  | 41   | 8    | 73   |
| RTOR Reduction (vph)   | 0    | 0     | 64   | 0     | 0     | 0    | 0     | 154  | 0    | 0    | 0    | 67   |
| Lane Group Flow (vph)  | 33   | 1041  | 85   | 150   | 1407  | 0    | 190   | 53   | 0    | 0    | 49   | 6    |
| Heavy Vehicles (%)     | 0%   | 1%    | 2%   | 2%    | 1%    | 0%   | 2%    | 0%   | 1%   | 3%   | 0%   | 0%   |
| Turn Type              | Prot | NA    | Perm | Prot  | NA    |      | pm+pt | NA   |      | Perm | NA   | Perm |
| Protected Phases       | 1    | 6     |      | 5     | 2     |      | 3     | 8    |      |      | 4    |      |
| Permitted Phases       |      |       | 6    |       |       |      | 8     |      |      | 4    |      | 4    |
| Actuated Green, G (s)  | 4.9  | 51.4  | 51.4 | 14.3  | 60.8  |      | 24.0  | 24.0 |      |      | 8.8  | 8.8  |
| Effective Green, g (s) | 4.9  | 51.4  | 51.4 | 14.3  | 60.8  |      | 24.0  | 24.0 |      |      | 8.8  | 8.8  |
| Actuated g/C Ratio     | 0.05 | 0.48  | 0.48 | 0.13  | 0.57  |      | 0.22  | 0.22 |      |      | 0.08 | 0.08 |
| Clearance Time (s)     | 5.5  | 6.0   | 6.0  | 5.5   | 6.0   |      | 5.5   | 5.5  |      |      | 5.5  | 5.5  |
| Vehicle Extension (s)  | 3.5  | 4.0   | 4.0  | 3.5   | 4.0   |      | 3.0   | 4.0  |      |      | 4.0  | 4.0  |
| Lane Grp Cap (vph)     | 82   | 1540  | 762  | 237   | 1606  |      | 272   | 362  |      |      | 95   | 133  |
| v/s Ratio Prot         | 0.02 | 0.33  |      | c0.08 | c0.50 |      | c0.06 | 0.03 |      |      |      |      |
| v/s Ratio Perm         |      |       | 0.05 |       |       |      | c0.09 |      |      |      | 0.04 | 0.00 |
| v/c Ratio              | 0.40 | 0.68  | 0.11 | 0.63  | 0.88  |      | 0.70  | 0.15 |      |      | 0.52 | 0.05 |
| Uniform Delay, d1      | 49.5 | 21.3  | 15.1 | 43.7  | 19.7  |      | 36.2  | 33.1 |      |      | 46.9 | 45.1 |
| Progression Factor     | 1.00 | 1.00  | 1.00 | 1.00  | 1.00  |      | 1.00  | 1.00 |      |      | 1.00 | 1.00 |
| Incremental Delay, d2  | 3.8  | 1.3   | 0.1  | 5.7   | 5.9   |      | 7.6   | 0.3  |      |      | 6.2  | 0.2  |
| Delay (s)              | 53.3 | 22.6  | 15.2 | 49.4  | 25.6  |      | 43.8  | 33.4 |      |      | 53.1 | 45.3 |
| Level of Service       | D    | C     | B    | D     | C     |      | D     | C    |      |      | D    | D    |
| Approach Delay (s)     |      | 22.5  |      |       | 27.9  |      |       | 38.4 |      |      | 48.4 |      |
| Approach LOS           |      | C     |      |       | C     |      |       | D    |      |      | D    |      |

### Intersection Summary

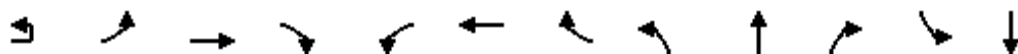
|                                   |       |                           |      |
|-----------------------------------|-------|---------------------------|------|
| HCM 2000 Control Delay            | 27.9  | HCM 2000 Level of Service | C    |
| HCM 2000 Volume to Capacity ratio | 0.88  |                           |      |
| Actuated Cycle Length (s)         | 106.7 | Sum of lost time (s)      | 22.5 |
| Intersection Capacity Utilization | 74.9% | ICU Level of Service      | D    |
| Analysis Period (min)             | 15    |                           |      |

c Critical Lane Group

# HCM Unsignalized Intersection Capacity Analysis

## 3: Cherry Ct/Driveway & Route 11

12/8/2015



| Movement               | EBU  | EBL  | EBT  | EBR  | WBL  | WBT   | WBR  | NBL  | NBT  | NBR  | SBL  | SBT  |
|------------------------|------|------|------|------|------|-------|------|------|------|------|------|------|
| Lane Configurations    |      |      |      |      |      |       |      |      |      |      |      |      |
| Traffic Volume (veh/h) | 2    | 12   | 1090 | 4    | 1    | 1040  | 0    | 3    | 0    | 0    | 5    | 0    |
| Future Volume (Veh/h)  | 2    | 12   | 1090 | 4    | 1    | 1040  | 0    | 3    | 0    | 0    | 5    | 0    |
| Sign Control           |      |      | Free |      |      | Free  |      |      | Stop |      |      | Stop |
| Grade                  |      |      | 0%   |      |      | 0%    |      |      | 0%   |      |      | 0%   |
| Peak Hour Factor       | 0.92 | 0.92 | 0.92 | 0.92 | 0.90 | 0.90  | 0.90 | 0.25 | 0.25 | 0.25 | 0.75 | 0.75 |
| Hourly flow rate (vph) | 0    | 14   | 1244 | 5    | 1    | 1213  | 0    | 13   | 0    | 0    | 7    | 0    |
| Pedestrians            |      |      |      |      |      |       |      |      |      |      |      |      |
| Lane Width (ft)        |      |      |      |      |      |       |      |      |      |      |      |      |
| Walking Speed (ft/s)   |      |      |      |      |      |       |      |      |      |      |      |      |
| Percent Blockage       |      |      |      |      |      |       |      |      |      |      |      |      |
| Right turn flare (veh) |      |      |      |      |      |       |      |      |      |      |      |      |
| Median type            |      |      | None |      |      | TWLTL |      |      |      |      |      |      |
| Median storage (veh)   |      |      |      |      |      | 2     |      |      |      |      |      |      |
| Upstream signal (ft)   |      |      | 779  |      |      |       |      |      |      |      |      |      |
| pX, platoon unblocked  | 0.00 |      |      |      | 0.98 |       |      | 0.98 | 0.98 | 0.98 | 0.98 | 0.98 |
| vC, conflicting volume | 0    | 1213 |      |      | 1249 |       |      | 1918 | 2490 | 624  | 1865 | 2492 |
| vC1, stage 1 conf vol  |      |      |      |      |      |       |      | 1274 | 1274 |      | 1215 | 1215 |
| vC2, stage 2 conf vol  |      |      |      |      |      |       |      | 644  | 1215 |      | 650  | 1277 |
| vCu, unblocked vol     | 0    | 1213 |      |      | 1212 |       |      | 1895 | 2479 | 574  | 1841 | 2481 |
| tC, single (s)         | 0.0  | 4.1  |      |      | 4.1  |       |      | 7.5  | 6.5  | 6.9  | 7.5  | 6.5  |
| tC, 2 stage (s)        |      |      |      |      |      |       |      | 6.5  | 5.5  |      | 6.5  | 5.5  |
| tF (s)                 | 0.0  | 2.2  |      |      | 2.2  |       |      | 3.5  | 4.0  | 3.3  | 3.5  | 4.0  |
| p0 queue free %        | 0    | 98   |      |      | 100  |       |      | 92   | 100  | 100  | 96   | 100  |
| cM capacity (veh/h)    | 0    | 582  |      |      | 571  |       |      | 162  | 166  | 457  | 177  | 169  |

| Direction, Lane #      | EB 1 | EB 2 | EB 3 | WB 1 | WB 2 | WB 3 | NB 1 | SB 1 |
|------------------------|------|------|------|------|------|------|------|------|
| Volume Total           | 14   | 829  | 420  | 1    | 809  | 404  | 13   | 42   |
| Volume Left            | 14   | 0    | 0    | 1    | 0    | 0    | 13   | 7    |
| Volume Right           | 0    | 0    | 5    | 0    | 0    | 0    | 0    | 35   |
| cSH                    | 582  | 1700 | 1700 | 571  | 1700 | 1700 | 162  | 355  |
| Volume to Capacity     | 0.02 | 0.49 | 0.25 | 0.00 | 0.48 | 0.24 | 0.08 | 0.12 |
| Queue Length 95th (ft) | 2    | 0    | 0    | 0    | 0    | 0    | 6    | 10   |
| Control Delay (s)      | 11.3 | 0.0  | 0.0  | 11.3 | 0.0  | 0.0  | 29.2 | 16.5 |
| Lane LOS               | B    |      |      | B    |      |      | D    | C    |
| Approach Delay (s)     | 0.1  |      |      | 0.0  |      |      | 29.2 | 16.5 |
| Approach LOS           |      |      |      |      |      |      | D    | C    |

### Intersection Summary

|                                   |       |
|-----------------------------------|-------|
| Average Delay                     | 0.5   |
| Intersection Capacity Utilization | 41.8% |
| ICU Level of Service              | A     |
| Analysis Period (min)             | 15    |

# HCM Unsignalized Intersection Capacity Analysis

## 3: Cherry Ct/Driveway & Route 11

12/8/2015

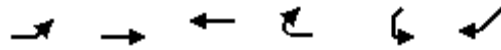
| Movement               | SBR  |
|------------------------|------|
| Lane Configurations    |      |
| Traffic Volume (veh/h) | 25   |
| Future Volume (Veh/h)  | 25   |
| Sign Control           |      |
| Grade                  |      |
| Peak Hour Factor       | 0.75 |
| Hourly flow rate (vph) | 35   |
| Pedestrians            |      |
| Lane Width (ft)        |      |
| Walking Speed (ft/s)   |      |
| Percent Blockage       |      |
| Right turn flare (veh) |      |
| Median type            |      |
| Median storage (veh)   |      |
| Upstream signal (ft)   |      |
| pX, platoon unblocked  |      |
| vC, conflicting volume | 606  |
| vC1, stage 1 conf vol  |      |
| vC2, stage 2 conf vol  |      |
| vCu, unblocked vol     | 606  |
| tC, single (s)         | 6.9  |
| tC, 2 stage (s)        |      |
| tF (s)                 | 3.3  |
| p0 queue free %        | 92   |
| cM capacity (veh/h)    | 445  |
| Direction, Lane #      |      |



# HCM Unsignalized Intersection Capacity Analysis

## 4: Route 11 & Belspring Rd

12/8/2015



| Movement                          | EBL  | EBT   | WBT   | WBR  | SWL                  | SWR  |
|-----------------------------------|------|-------|-------|------|----------------------|------|
| Lane Configurations               |      |       |       |      |                      |      |
| Traffic Volume (veh/h)            | 1    | 1097  | 1096  | 305  | 18                   | 5    |
| Future Volume (Veh/h)             | 1    | 1097  | 1096  | 305  | 18                   | 5    |
| Sign Control                      |      | Free  | Free  |      | Stop                 |      |
| Grade                             |      | 0%    | 0%    |      | 0%                   |      |
| Peak Hour Factor                  | 0.96 | 0.96  | 0.89  | 0.89 | 0.64                 | 0.64 |
| Hourly flow rate (vph)            | 1    | 1200  | 1293  | 360  | 30                   | 8    |
| Pedestrians                       |      |       |       |      |                      |      |
| Lane Width (ft)                   |      |       |       |      |                      |      |
| Walking Speed (ft/s)              |      |       |       |      |                      |      |
| Percent Blockage                  |      |       |       |      |                      |      |
| Right turn flare (veh)            |      |       |       |      |                      |      |
| Median type                       |      |       |       |      |                      |      |
|                                   |      | TWLTL | TWLTL |      |                      |      |
| Median storage (veh)              |      | 2     | 2     |      |                      |      |
| Upstream signal (ft)              |      | 1075  | 1303  |      |                      |      |
| pX, platoon unblocked             | 0.60 |       |       |      | 0.60                 | 0.60 |
| vC, conflicting volume            | 1653 |       |       |      | 2075                 | 826  |
| vC1, stage 1 conf vol             |      |       |       |      | 1473                 |      |
| vC2, stage 2 conf vol             |      |       |       |      | 602                  |      |
| vCu, unblocked vol                | 765  |       |       |      | 1465                 | 0    |
| tC, single (s)                    | 4.1  |       |       |      | 6.8                  | 6.9  |
| tC, 2 stage (s)                   |      |       |       |      | 5.8                  |      |
| tF (s)                            | 2.2  |       |       |      | 3.5                  | 3.3  |
| p0 queue free %                   | 100  |       |       |      | 90                   | 99   |
| cM capacity (veh/h)               | 517  |       |       |      | 296                  | 658  |
| Direction, Lane #                 | EB 1 | EB 2  | EB 3  | WB 1 | WB 2                 | SW 1 |
| Volume Total                      | 1    | 600   | 600   | 862  | 791                  | 38   |
| Volume Left                       | 1    | 0     | 0     | 0    | 0                    | 30   |
| Volume Right                      | 0    | 0     | 0     | 0    | 360                  | 8    |
| cSH                               | 517  | 1700  | 1700  | 1700 | 1700                 | 335  |
| Volume to Capacity                | 0.00 | 0.35  | 0.35  | 0.51 | 0.47                 | 0.11 |
| Queue Length 95th (ft)            | 0    | 0     | 0     | 0    | 0                    | 10   |
| Control Delay (s)                 | 12.0 | 0.0   | 0.0   | 0.0  | 0.0                  | 17.1 |
| Lane LOS                          | B    |       |       |      |                      | C    |
| Approach Delay (s)                | 0.0  |       |       | 0.0  |                      | 17.1 |
| Approach LOS                      |      |       |       |      |                      | C    |
| Intersection Summary              |      |       |       |      |                      |      |
| Average Delay                     |      |       | 0.2   |      |                      |      |
| Intersection Capacity Utilization |      |       | 52.0% |      | ICU Level of Service | A    |
| Analysis Period (min)             |      |       | 15    |      |                      |      |

# HCM Unsignalized Intersection Capacity Analysis

## 5: Driveway/Nicewander Way & Route 11

12/8/2015

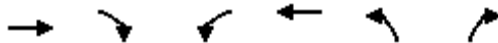


| Movement                          | EBL   | EBT  | EBR  | WBL                  | WBT  | WBR   | NBL  | NBT  | NBR  | SBL  | SBT  | SBR  |      |
|-----------------------------------|-------|------|------|----------------------|------|-------|------|------|------|------|------|------|------|
| Lane Configurations               |       |      |      |                      |      |       |      |      |      |      |      |      |      |
| Traffic Volume (veh/h)            | 4     | 1087 | 26   | 18                   | 1379 | 5     | 18   | 0    | 18   | 4    | 0    | 4    |      |
| Future Volume (Veh/h)             | 4     | 1087 | 26   | 18                   | 1379 | 5     | 18   | 0    | 18   | 4    | 0    | 4    |      |
| Sign Control                      |       | Free |      |                      | Free |       |      | Stop |      |      | Stop |      |      |
| Grade                             |       | 0%   |      |                      | 0%   |       |      | 0%   |      |      | 0%   |      |      |
| Peak Hour Factor                  | 0.96  | 0.96 | 0.96 | 0.89                 | 0.89 | 0.89  | 0.82 | 0.82 | 0.82 | 0.67 | 0.67 | 0.67 |      |
| Hourly flow rate (vph)            | 4     | 1189 | 28   | 21                   | 1627 | 6     | 23   | 0    | 23   | 6    | 0    | 6    |      |
| Pedestrians                       |       |      |      |                      |      |       |      |      |      |      |      |      |      |
| Lane Width (ft)                   |       |      |      |                      |      |       |      |      |      |      |      |      |      |
| Walking Speed (ft/s)              |       |      |      |                      |      |       |      |      |      |      |      |      |      |
| Percent Blockage                  |       |      |      |                      |      |       |      |      |      |      |      |      |      |
| Right turn flare (veh)            |       |      |      |                      |      |       |      |      |      |      |      |      |      |
| Median type                       |       |      |      |                      |      |       |      |      |      |      |      |      |      |
|                                   | TWLTL |      |      |                      |      | TWLTL |      |      |      |      |      |      |      |
| Median storage veh                | 2     |      |      |                      |      | 2     |      |      |      |      |      |      |      |
| Upstream signal (ft)              |       |      |      |                      |      | 715   |      |      |      |      |      |      |      |
| pX, platoon unblocked             | 0.59  |      |      |                      |      |       | 0.59 | 0.59 |      |      | 0.59 | 0.59 | 0.59 |
| vC, conflicting volume            | 1633  |      |      |                      |      |       | 1217 | 2072 | 2886 | 608  | 2298 | 2897 | 816  |
| vC1, stage 1 conf vol             |       |      |      |                      |      |       | 1211 | 1211 |      |      | 1672 | 1672 |      |
| vC2, stage 2 conf vol             |       |      |      |                      |      |       | 862  | 1675 |      |      | 626  | 1225 |      |
| vCu, unblocked vol                | 677   |      |      |                      |      |       | 1217 | 1424 | 2806 | 608  | 1806 | 2825 | 0    |
| tC, single (s)                    | 4.1   |      |      |                      |      |       | 4.1  | 7.5  | 6.5  | 6.9  | 7.5  | 6.5  | 6.9  |
| tC, 2 stage (s)                   |       |      |      |                      |      |       | 6.5  | 5.5  |      |      | 6.5  | 5.5  |      |
| tF (s)                            | 2.2   |      |      |                      |      |       | 2.2  | 3.5  | 4.0  | 3.3  | 3.5  | 4.0  | 3.3  |
| p0 queue free %                   | 99    |      |      |                      |      |       | 96   | 87   | 100  | 95   | 97   | 100  | 99   |
| cM capacity (veh/h)               | 544   |      |      |                      |      |       | 580  | 183  | 163  | 443  | 183  | 157  | 642  |
| <b>Direction, Lane #</b>          |       |      |      |                      |      |       |      |      |      |      |      |      |      |
|                                   | EB 1  | EB 2 | EB 3 | WB 1                 | WB 2 | WB 3  | NB 1 | SB 1 |      |      |      |      |      |
| Volume Total                      | 4     | 793  | 424  | 21                   | 1085 | 548   | 46   | 12   |      |      |      |      |      |
| Volume Left                       | 4     | 0    | 0    | 21                   | 0    | 0     | 23   | 6    |      |      |      |      |      |
| Volume Right                      | 0     | 0    | 28   | 0                    | 0    | 6     | 23   | 6    |      |      |      |      |      |
| cSH                               | 544   | 1700 | 1700 | 580                  | 1700 | 1700  | 259  | 284  |      |      |      |      |      |
| Volume to Capacity                | 0.01  | 0.47 | 0.25 | 0.04                 | 0.64 | 0.32  | 0.18 | 0.04 |      |      |      |      |      |
| Queue Length 95th (ft)            | 1     | 0    | 0    | 3                    | 0    | 0     | 16   | 3    |      |      |      |      |      |
| Control Delay (s)                 | 11.7  | 0.0  | 0.0  | 11.4                 | 0.0  | 0.0   | 21.9 | 18.2 |      |      |      |      |      |
| Lane LOS                          | B     |      |      | B                    |      |       | C    |      | C    |      |      |      |      |
| Approach Delay (s)                | 0.0   |      |      | 0.1                  |      |       | 21.9 |      | 18.2 |      |      |      |      |
| Approach LOS                      |       |      |      |                      |      |       | C    |      | C    |      |      |      |      |
| <b>Intersection Summary</b>       |       |      |      |                      |      |       |      |      |      |      |      |      |      |
| Average Delay                     | 0.5   |      |      |                      |      |       |      |      |      |      |      |      |      |
| Intersection Capacity Utilization | 50.2% |      |      | ICU Level of Service |      |       |      |      | A    |      |      |      |      |
| Analysis Period (min)             | 15    |      |      |                      |      |       |      |      |      |      |      |      |      |

# HCM Unsignalized Intersection Capacity Analysis

## 8: Wheatland & Route 11

12/8/2015



| Movement                          | EBT   | EBR  | WBL   | WBT                  | NBL  | NBR  |
|-----------------------------------|-------|------|-------|----------------------|------|------|
| Lane Configurations               | ↑↑    |      | ↵     | ↑↑                   | ↵    |      |
| Traffic Volume (veh/h)            | 1098  | 9    | 57    | 1340                 | 11   | 55   |
| Future Volume (Veh/h)             | 1098  | 9    | 57    | 1340                 | 11   | 55   |
| Sign Control                      | Free  |      |       | Free                 | Stop |      |
| Grade                             | 0%    |      |       | 0%                   | 0%   |      |
| Peak Hour Factor                  | 0.92  | 0.92 | 0.89  | 0.89                 | 0.75 | 0.75 |
| Hourly flow rate (vph)            | 1253  | 10   | 67    | 1581                 | 15   | 77   |
| Pedestrians                       |       |      |       |                      |      |      |
| Lane Width (ft)                   |       |      |       |                      |      |      |
| Walking Speed (ft/s)              |       |      |       |                      |      |      |
| Percent Blockage                  |       |      |       |                      |      |      |
| Right turn flare (veh)            |       |      |       |                      |      |      |
| Median type                       | TWLTL |      | TWLTL |                      |      |      |
| Median storage veh)               | 2     |      | 2     |                      |      |      |
| Upstream signal (ft)              | 335   |      |       |                      |      |      |
| pX, platoon unblocked             |       |      | 0.75  |                      | 0.75 | 0.75 |
| vC, conflicting volume            |       |      | 1263  |                      | 2182 | 632  |
| vC1, stage 1 conf vol             |       |      |       |                      | 1258 |      |
| vC2, stage 2 conf vol             |       |      |       |                      | 924  |      |
| vCu, unblocked vol                |       |      | 696   |                      | 1916 | 0    |
| tC, single (s)                    |       |      | 4.1   |                      | 7.2  | 6.9  |
| tC, 2 stage (s)                   |       |      |       |                      | 6.2  |      |
| tF (s)                            |       |      | 2.2   |                      | 3.7  | 3.3  |
| p0 queue free %                   |       |      | 90    |                      | 93   | 91   |
| cM capacity (veh/h)               |       |      | 675   |                      | 204  | 818  |
| Direction, Lane #                 | EB 1  | EB 2 | WB 1  | WB 2                 | WB 3 | NB 1 |
| Volume Total                      | 835   | 428  | 67    | 790                  | 790  | 92   |
| Volume Left                       | 0     | 0    | 67    | 0                    | 0    | 15   |
| Volume Right                      | 0     | 10   | 0     | 0                    | 0    | 77   |
| cSH                               | 1700  | 1700 | 675   | 1700                 | 1700 | 548  |
| Volume to Capacity                | 0.49  | 0.25 | 0.10  | 0.47                 | 0.47 | 0.17 |
| Queue Length 95th (ft)            | 0     | 0    | 8     | 0                    | 0    | 15   |
| Control Delay (s)                 | 0.0   | 0.0  | 10.9  | 0.0                  | 0.0  | 12.9 |
| Lane LOS                          |       |      | B     |                      |      | B    |
| Approach Delay (s)                | 0.0   |      | 0.4   |                      |      | 12.9 |
| Approach LOS                      |       |      |       |                      |      | B    |
| Intersection Summary              |       |      |       |                      |      |      |
| Average Delay                     |       |      | 0.6   |                      |      |      |
| Intersection Capacity Utilization |       |      | 49.8% | ICU Level of Service | A    |      |
| Analysis Period (min)             |       |      | 15    |                      |      |      |

# HCM Unsignalized Intersection Capacity Analysis

## 9: University Park/Roca's & Route 11

12/8/2015



| Movement               | EBL  | EBT   | EBR  | WBL  | WBT  | WBR  | NBL  | NBT  | NBR  | SBL  | SBT  | SBR  |
|------------------------|------|-------|------|------|------|------|------|------|------|------|------|------|
| Lane Configurations    |      |       |      |      |      |      |      |      |      |      |      |      |
| Traffic Volume (veh/h) | 11   | 1146  | 4    | 4    | 1401 | 0    | 5    | 0    | 8    | 1    | 0    | 2    |
| Future Volume (Veh/h)  | 11   | 1146  | 4    | 4    | 1401 | 0    | 5    | 0    | 8    | 1    | 0    | 2    |
| Sign Control           |      | Free  |      |      | Free |      |      | Stop |      |      | Stop |      |
| Grade                  |      | 0%    |      |      | 0%   |      |      | 0%   |      |      | 0%   |      |
| Peak Hour Factor       | 0.88 | 0.88  | 0.88 | 0.92 | 0.92 | 0.92 | 0.54 | 0.54 | 0.54 | 0.75 | 0.75 | 0.75 |
| Hourly flow rate (vph) | 13   | 1367  | 5    | 5    | 1599 | 0    | 10   | 0    | 16   | 1    | 0    | 3    |
| Pedestrians            |      |       |      |      |      |      |      |      |      |      |      |      |
| Lane Width (ft)        |      |       |      |      |      |      |      |      |      |      |      |      |
| Walking Speed (ft/s)   |      |       |      |      |      |      |      |      |      |      |      |      |
| Percent Blockage       |      |       |      |      |      |      |      |      |      |      |      |      |
| Right turn flare (veh) |      |       |      |      |      |      |      |      |      |      |      |      |
| Median type            |      | TWLTL |      |      | None |      |      |      |      |      |      |      |
| Median storage (veh)   |      | 2     |      |      |      |      |      |      |      |      |      |      |
| Upstream signal (ft)   |      | 792   |      |      |      |      |      |      |      |      |      |      |
| pX, platoon unblocked  |      |       |      | 0.76 |      |      | 0.76 | 0.76 | 0.76 | 0.76 | 0.76 |      |
| vC, conflicting volume | 1599 |       |      | 1372 |      |      | 2208 | 3004 | 686  | 2334 | 3007 | 800  |
| vC1, stage 1 conf vol  |      |       |      |      |      |      | 1396 | 1396 |      | 1609 | 1609 |      |
| vC2, stage 2 conf vol  |      |       |      |      |      |      | 812  | 1609 |      | 726  | 1398 |      |
| vCu, unblocked vol     | 1599 |       |      | 860  |      |      | 1959 | 3006 | 0    | 2125 | 3009 | 800  |
| tC, single (s)         | 4.1  |       |      | 4.1  |      |      | 7.5  | 6.5  | 6.9  | 7.5  | 6.5  | 6.9  |
| tC, 2 stage (s)        |      |       |      |      |      |      | 6.5  | 5.5  |      | 6.5  | 5.5  |      |
| tF (s)                 | 2.2  |       |      | 2.2  |      |      | 3.5  | 4.0  | 3.3  | 3.5  | 4.0  | 3.3  |
| p0 queue free %        | 97   |       |      | 99   |      |      | 94   | 100  | 98   | 99   | 100  | 99   |
| cM capacity (veh/h)    | 415  |       |      | 601  |      |      | 180  | 125  | 830  | 107  | 132  | 332  |

| Direction, Lane #      | EB 1 | EB 2 | EB 3 | WB 1 | WB 2 | WB 3 | NB 1 | SB 1 |
|------------------------|------|------|------|------|------|------|------|------|
| Volume Total           | 13   | 911  | 461  | 5    | 1066 | 533  | 26   | 4    |
| Volume Left            | 13   | 0    | 0    | 5    | 0    | 0    | 10   | 1    |
| Volume Right           | 0    | 0    | 5    | 0    | 0    | 0    | 16   | 3    |
| cSH                    | 415  | 1700 | 1700 | 601  | 1700 | 1700 | 348  | 217  |
| Volume to Capacity     | 0.03 | 0.54 | 0.27 | 0.01 | 0.63 | 0.31 | 0.07 | 0.02 |
| Queue Length 95th (ft) | 2    | 0    | 0    | 1    | 0    | 0    | 6    | 1    |
| Control Delay (s)      | 14.0 | 0.0  | 0.0  | 11.0 | 0.0  | 0.0  | 16.2 | 21.9 |
| Lane LOS               | B    |      |      | B    |      |      | C    | C    |
| Approach Delay (s)     | 0.1  |      |      | 0.0  |      |      | 16.2 | 21.9 |
| Approach LOS           |      |      |      |      |      |      | C    | C    |

### Intersection Summary

|                                   |       |
|-----------------------------------|-------|
| Average Delay                     | 0.2   |
| Intersection Capacity Utilization | 50.7% |
| ICU Level of Service              | A     |
| Analysis Period (min)             | 15    |

# HCM Unsignalized Intersection Capacity Analysis

## 10: Route 11 & Nesslerod

12/8/2015

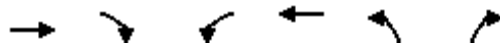


| Movement                          | EBL  | EBT    | WBT    | WBR  | SBL                  | SBR  |
|-----------------------------------|------|--------|--------|------|----------------------|------|
| Lane Configurations               |      |        |        |      |                      |      |
| Traffic Volume (veh/h)            | 8    | 1147   | 1413   | 1    | 2                    | 2    |
| Future Volume (Veh/h)             | 8    | 1147   | 1413   | 1    | 2                    | 2    |
| Sign Control                      |      | Free   | Free   |      | Stop                 |      |
| Grade                             |      | 0%     | 0%     |      | 0%                   |      |
| Peak Hour Factor                  | 0.88 | 0.88   | 0.92   | 0.92 | 0.50                 | 0.50 |
| Hourly flow rate (vph)            | 10   | 1369   | 1613   | 1    | 4                    | 4    |
| Pedestrians                       |      |        |        |      |                      |      |
| Lane Width (ft)                   |      |        |        |      |                      |      |
| Walking Speed (ft/s)              |      |        |        |      |                      |      |
| Percent Blockage                  |      |        |        |      |                      |      |
| Right turn flare (veh)            |      |        |        |      |                      |      |
| Median type                       |      | Raised | Raised |      |                      |      |
| Median storage veh                |      | 1      | 1      |      |                      |      |
| Upstream signal (ft)              |      | 1287   |        |      |                      |      |
| pX, platoon unblocked             |      |        |        |      | 0.77                 |      |
| vC, conflicting volume            | 1614 |        |        |      | 2318                 | 807  |
| vC1, stage 1 conf vol             |      |        |        |      | 1614                 |      |
| vC2, stage 2 conf vol             |      |        |        |      | 704                  |      |
| vCu, unblocked vol                | 1614 |        |        |      | 2115                 | 807  |
| tC, single (s)                    | 4.1  |        |        |      | 6.8                  | 6.9  |
| tC, 2 stage (s)                   |      |        |        |      | 5.8                  |      |
| tF (s)                            | 2.2  |        |        |      | 3.5                  | 3.3  |
| p0 queue free %                   | 98   |        |        |      | 97                   | 99   |
| cM capacity (veh/h)               | 410  |        |        |      | 123                  | 329  |
| Direction, Lane #                 | EB 1 | EB 2   | EB 3   | WB 1 | WB 2                 | SB 1 |
| Volume Total                      | 10   | 684    | 684    | 1075 | 539                  | 8    |
| Volume Left                       | 10   | 0      | 0      | 0    | 0                    | 4    |
| Volume Right                      | 0    | 0      | 0      | 0    | 1                    | 4    |
| cSH                               | 410  | 1700   | 1700   | 1700 | 1700                 | 179  |
| Volume to Capacity                | 0.02 | 0.40   | 0.40   | 0.63 | 0.32                 | 0.04 |
| Queue Length 95th (ft)            | 2    | 0      | 0      | 0    | 0                    | 3    |
| Control Delay (s)                 | 14.0 | 0.0    | 0.0    | 0.0  | 0.0                  | 26.0 |
| Lane LOS                          | B    |        |        |      |                      | D    |
| Approach Delay (s)                | 0.1  |        |        | 0.0  |                      | 26.0 |
| Approach LOS                      |      |        |        |      |                      | D    |
| Intersection Summary              |      |        |        |      |                      |      |
| Average Delay                     |      |        | 0.1    |      |                      |      |
| Intersection Capacity Utilization |      |        | 51.0%  |      | ICU Level of Service | A    |
| Analysis Period (min)             |      |        | 15     |      |                      |      |

# HCM Unsignalized Intersection Capacity Analysis

## 11: Hazel Hollow Rd & Route 11

12/8/2015



| Movement                          | EBT         | EBR         | WBL         | WBT                  | NBL         | NBR         |             |
|-----------------------------------|-------------|-------------|-------------|----------------------|-------------|-------------|-------------|
| Lane Configurations               | ↑↑          | ↑           | ↓           | ↑↑                   | ↓           | ↓           |             |
| Traffic Volume (veh/h)            | 1116        | 33          | 43          | 1398                 | 16          | 31          |             |
| Future Volume (Veh/h)             | 1116        | 33          | 43          | 1398                 | 16          | 31          |             |
| Sign Control                      | Free        |             |             | Free                 | Stop        |             |             |
| Grade                             | 0%          |             |             | 0%                   | 0%          |             |             |
| Peak Hour Factor                  | 0.88        | 0.88        | 0.92        | 0.92                 | 0.78        | 0.78        |             |
| Hourly flow rate (vph)            | 1332        | 39          | 49          | 1596                 | 22          | 42          |             |
| <b>Pedestrians</b>                |             |             |             |                      |             |             |             |
| Lane Width (ft)                   |             |             |             |                      |             |             |             |
| Walking Speed (ft/s)              |             |             |             |                      |             |             |             |
| Percent Blockage                  |             |             |             |                      |             |             |             |
| Right turn flare (veh)            |             |             |             |                      |             |             |             |
| Median type                       | Raised      |             | Raised      |                      |             |             |             |
| Median storage (veh)              | 1           |             | 1           |                      |             |             |             |
| Upstream signal (ft)              |             |             |             |                      |             |             |             |
| pX, platoon unblocked             |             |             |             |                      |             |             |             |
| vC, conflicting volume            |             |             | 1371        |                      | 2228        | 666         |             |
| vC1, stage 1 conf vol             |             |             |             |                      | 1332        |             |             |
| vC2, stage 2 conf vol             |             |             |             |                      | 896         |             |             |
| vCu, unblocked vol                |             |             | 1371        |                      | 2228        | 666         |             |
| tC, single (s)                    |             |             | 4.1         |                      | 6.8         | 7.0         |             |
| tC, 2 stage (s)                   |             |             |             |                      | 5.8         |             |             |
| tF (s)                            |             |             | 2.2         |                      | 3.5         | 3.3         |             |
| p0 queue free %                   |             |             | 90          |                      | 83          | 89          |             |
| cM capacity (veh/h)               |             |             | 507         |                      | 133         | 400         |             |
| <b>Direction, Lane #</b>          | <b>EB 1</b> | <b>EB 2</b> | <b>EB 3</b> | <b>WB 1</b>          | <b>WB 2</b> | <b>WB 3</b> | <b>NB 1</b> |
| Volume Total                      | 666         | 666         | 39          | 49                   | 798         | 798         | 64          |
| Volume Left                       | 0           | 0           | 0           | 49                   | 0           | 0           | 22          |
| Volume Right                      | 0           | 0           | 39          | 0                    | 0           | 0           | 42          |
| cSH                               | 1700        | 1700        | 1700        | 507                  | 1700        | 1700        | 237         |
| Volume to Capacity                | 0.39        | 0.39        | 0.02        | 0.10                 | 0.47        | 0.47        | 0.27        |
| Queue Length 95th (ft)            | 0           | 0           | 0           | 8                    | 0           | 0           | 26          |
| Control Delay (s)                 | 0.0         | 0.0         | 0.0         | 12.9                 | 0.0         | 0.0         | 25.8        |
| Lane LOS                          |             |             |             | B                    | D           |             |             |
| Approach Delay (s)                | 0.0         |             |             | 0.4                  | 25.8        |             |             |
| Approach LOS                      |             |             |             | D                    |             |             |             |
| <b>Intersection Summary</b>       |             |             |             |                      |             |             |             |
| Average Delay                     |             |             | 0.7         |                      |             |             |             |
| Intersection Capacity Utilization |             |             | 50.6%       | ICU Level of Service |             | A           |             |
| Analysis Period (min)             |             |             | 15          |                      |             |             |             |



## APPENDIX D

### Proposed Improvements Concept